

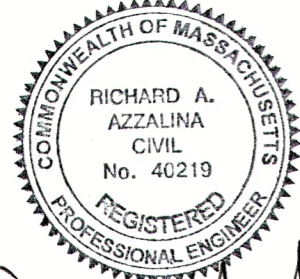


SALEM, MA

BRIDGE STREET RECONSTRUCTION
PROJECT

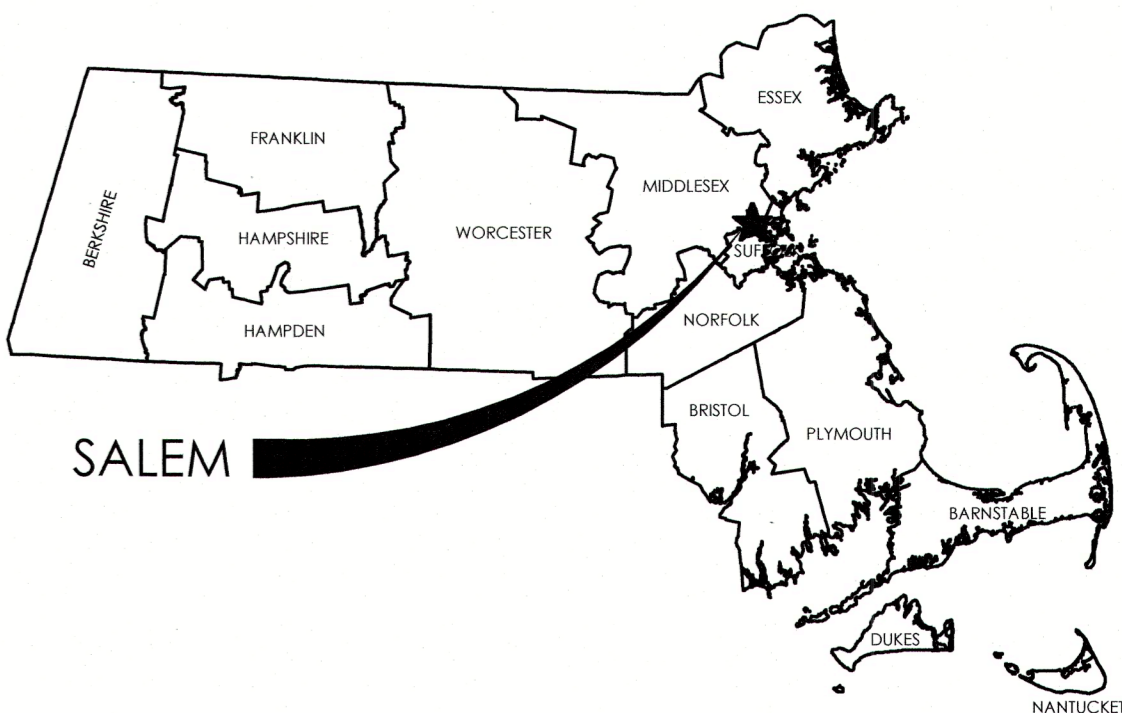
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Richard A. Azzalina
2-5-2026

THE CURRENT EDITION OF MASSACHUSETTS HIGHWAY DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES DATED 1988, AS AMENDED, THE OCTOBER 2017 CONSTRUCTION STANDARD DETAILS, THE 2015 OVERHEAD SIGNAL STRUCTURE AND FOUNDATION STANDARD DRAWINGS, MASSDOT TRAFFIC MANAGEMENT PLANS AND DETAIL DRAWINGS, THE LATEST MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS WITH MASSACHUSETTS AMENDMENTS, THE 1990 STANDARD DRAWINGS FOR SIGNS AND SUPPORTS, THE 1988 STANDARD DRAWINGS FOR TRAFFIC SIGNALS AND HIGHWAY LIGHTING, AND THE LATEST EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK, WILL GOVERN.

FEBRUARY 5, 2019



LOCATION MAP



VICINITY MAP
1" = 1000'

CITY MAYOR
KIMBERLEY DRISCOLL

CITY ENGINEER
DAVID KNOWLTON, P.E.

Revision	By	Appd.	YY.MM.DD

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LEGEND AND ABBREVIATIONS

EXISTING:	PROPOSED:	
		TWO FOOT CONTOUR
		TEN FOOT CONTOUR
		SPOT ELEVATION
		BITUMINOUS BERM
		GRANITE CURB
		CONCRETE CURB
		VEGETATION LINE
		GUARDRAIL
		METAL FENCE
		WOOD FENCE
		WALL
		CRUSHED STONE
		CONCRETE
		EXPOSED LEDGE
		SIGN
		MAILBOX
		HANDICAP RAMP
		BOLLARD
		STUMP
		DECIDUOUS TREE
		CONIFEROUS TREE
		COMPILED COMMUNICATIONS LINE
		TELEPHONE MANHOLE
		COMPILED GAS LINE
		VENT
		GAS DRIP
		GAS GATE
		OVERHEAD WIRES
		COMPILED ELECTRIC LINE
		TRAFFIC LIGHT
		STREET LIGHT
		ELECTRIC MANHOLE
		FIRE ALARM BOX
		UTILITY POLE
		UTILITY POLE WITH LIGHT
		GUY WIRE
		OBSERVED DRAIN LINE
		COMPILED DRAIN LINE
		DRAIN MANHOLE
		CATCH BASIN
		MONITORING WELL
		OBSERVED SEWER LINE
		COMPILED SEWER LINE
		SEWER MANHOLE
		COMPILED WATER LINE
		WATER GATE
		HYDRANT
		WATER SERVICE
		IRRIGATION CONTROL VALVE
		WATER MANHOLE
		MANHOLE
		HANDHOLE
		TERMINUS UNKNOWN
		ABOVE GROUND
		BITUMINOUS BERM
		BITUMINOUS
		CONCRETE CURB
		CAST IRON
		CHAINLINK FENCE
		CORRUGATED METAL PIPE
		CONCRETE
		EDGE OF PAVEMENT
		FIRE ALARM BOX
		GRANITE CURB
		HIGH DENSITY POLYETHYLENE
		MASS HIGHWAY
		NO PIPES VISIBLE
		PLASTIC
		POLYVINYL CHLORIDE

BASELINE	EOP
CURB	EXIST
CITY APPROX. LAYOUT LINE	EXC
SAWCUT	F&C
CATCH BASIN	F&G
DRAIN MANHOLE	FDN.
CHANGE IN TYPE (CB TO DMH)	FLDSTN
HYDRANT	FND
RAISED INTERSECTION PAVERS	GC
FLUSH GRANITE CURB	GD
ASPHALT PAVEMENT BREAKLINE	GG
DETECTABLE WARNING PANEL	GI
SIGN POST	GIP
ANNUAL AVERAGE DAILY TRAFFIC	GRAN
ABANDON	GRAV
ADJUST	GRD
ADJUST BY OTHERS	HLSD
ANGLE POINT	HDW
APPROXIMATE	HMA
BITUMINOUS	HOR
BOTTOM OF CURB	HYD
BITUMINOUS CONCRETE DRIVEWAY	I OR INV
BITUMINOUS CONCRETE SIDEWALK	L
BASELINE	LP
BUILDING	LST
BORDERING LAND SUBJECT TO FLOODING	LT
BY OTHERS	MAG
BOTTOM OF SLOPE	MAX
CATCH BASIN	MB
CATCH BASIN WITH CURB INLET	MH
CEMENT	MHR
CURB INLET	MIN
CAST IRON PIPE	N/F
CHAIN LINK FENCE	NIC
CENTERLINE	NO.
CITY LINE	NPV
CORRUGATED METAL PIPE	PC
CONCRETE PAD	PCC
CORRUGATED STEEL PIPE	P.G.L.
CONCRETE	PI
CONTINUOUS	PKT
CONSTRUCTION	POC
CONCRETE RETAINING WALL	POT
CONCRETE WALK	PRC
DESIGN HOURLY VOLUME	PROJ
DROP INLET	PROP
DIAMETER	PSI
DUCTILE IRON PIPE	PT
DRIVEWAY	DWY
ELEVATION	ELEV

EDGE OF PAVEMENT	PVC
EXISTING	PVI
EXCAVATION	PVT
FRAME AND COVER	PVMT
FRAME AND GRATE	R
FOUNDATION	R&D
FIELDSTONE	R&DBO
FOUND	RCP
GRANITE CURB	RD
GROUND	RDWY
GAS GATE	REM
GUTTER INLET	RET
GALVANIZED IRON PIPE	RET WALL
GRANITE	RFA
GRAVEL	ROW
GUARD	RR
HEADLIGHT SIGHT DISTANCE	R&R
REMOVE AND DISPOSE	R&RBO
REMOVE AND RESET BY OTHERS	R&S
REMOVE AND STACK	RT
RIGHT	SESD
SOUTH ESSEX SEWER DISTRICT	SHLDR
SHOULDER	SL
STOP LINE - 12" WIDTH	SMH
SEWER MANHOLE	SRW
STONE RETAINING WALL	ST
STREET	STA
STATION	STK
STAKE OR STOCKADE FENCE	SSD
STOPPING SIGHT DISTANCE	SW
SIDEWALK	T
TANGENT DISTANCE OF CURVE/	TAN
TANGENT	TEMP
TEMPORARY	TC
TOP OF CURB	TOS
TOP OF SLOPE	TYP
TYPICAL	UG
UNKNOWN GATE	UP
UTILITY POLE	VAR
VARIES	VERT
VERTICAL	VC
VERTICAL CURVE	VCP
VITREOUS CLAY PIPE	WCR
WHEELCHAIR RAMP	WG
WATER GATE	WIF
WROUGHT IRON FENCE	WIP
WROUGHT IRON PIPE	WM
WATER METER/WATER MAIN	WTL
WHITE TRANSVERSE LINE - 4' SPACE,	
3:1 SLOPE, 4' WIDTH	
CROSS SECTION	

X-SECT

GENERAL NOTES:

- HORIZONTAL DATUM: NORTH AMERICAN DATUM OF 1983 (NAD83)
- ALL TRAFFIC SIGNAL HARDWARE (INCLUDING MAST ARMS, POLE BOXES, PEDESTALS & CONTROLLER BOXES), PARKING METERS, LIGHT/UTILITY POLES, FENCES, STONE BOUNDS, GRANITE BOUNDS, AND OTHER PROPERTY MARKERS ARE TO BE RETAINED AND PROTECTED UNLESS OTHERWISE NOTED ON PLANS. PULLBOXES ARE TO BE RETAINED AND ADJUSTED TO GRADE.
- THE CONTRACTOR SHALL SAWCUT TO THE FULL PAVEMENT DEPTH AT LOCATIONS WHERE NEW FULL DEPTH PAVEMENT OR NEW SIDEWALK MEETS EXISTING PAVEMENT OR SIDEWALK, AT THE LOCATIONS SHOWN ON THE PLANS AND AS REQUIRED BY THE ENGINEER. PROPOSED WORK SHALL MATCH EXISTING LINE AND GRADE AT LIMITS OF WORK.
- ALL AREAS OUTSIDE OF THE LIMIT OF WORK DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
- EXISTING TREES WITHIN THE LIMIT OF WORK SHALL BE RETAINED AND PROTECTED UNLESS OTHERWISE NOTED OR REQUIRED BY ENGINEER. IN THE EVENT THAT A PROPOSED CATCH BASIN IS WITHIN 6' OF AN EXISTING TREE, THE INFORMATION SHALL BE FURNISHED TO THE ENGINEER FOR RESOLUTION PRIOR TO INSTALLATION OF THE CATCH BASIN.
- ALL GRANITE CURB, EDGING, AND CURB CORNERS THAT ARE TO BE REMOVED SHALL BE REMOVED AND STACKED OR DISCARDED AS REQUIRED BY THE ENGINEER. GRANITE CURB OR EDGING TO BE RESET SHALL NOT BE RANDOMLY MIXED WITH NEW CURB, BUT SHALL BE RESET SO AS TO GIVE A UNIFORM APPEARANCE.
- ALL PROPOSED GRANITE CURB SHALL BE TYPE VB, UNLESS OTHERWISE NOTED.
- ALL FLUSH CURB ENCOUNTERED AT DRIVEWAYS SHALL BE REMOVED AND RE-USED ELSEWHERE, DISPOSED, OR AS DIRECTED BY ENGINEER, UNLESS OTHERWISE NOTED.
- ALL PROPOSED WHEELCHAIR RAMPS ARE TO CONFORM TO LATEST AAB AND ADA WHEELCHAIR RAMP STANDARDS.

ALL UNDERGROUND UTILITY DATA REPRESENTS RECORD INFORMATION RECOVERED THROUGH RESEARCH WITHOUT SURFACE DEMARCATON NOR SUBSURFACE VERIFICATION.

- A MINIMUM 36" PATH OF TRAVEL (EXCLUDING CURB) SHALL BE MAINTAINED PAST ALL OBSTRUCTIONS (EXISTING, RELOCATED, AND PROPOSED) ON THE SIDEWALK INCLUDING, BUT NOT LIMITED TO, UTILITY POLES, HYDRANTS, SIGNS, SIGNALS, LIGHT POLES, TREES AND MAILBOXES. IF THERE IS A CONFLICT, THE INFORMATION SHALL BE FURNISHED TO THE ENGINEER FOR RESOLUTION PRIOR TO SIDEWALK RECONSTRUCTION.
- ALL EXISTING CURB STOPS, VALVE BOXES, CASTINGS AND HYDRANTS, IN AREAS TO BE REPAVED SHALL BE PROTECTED AND ADJUSTED TO LINE AND GRADE BY THE CONTRACTOR, UNLESS OTHERWISE DIRECTED BY ENGINEER.
- THE CONTRACTOR SHALL VERIFY ALL OUTLET GRADES OF DRAINAGE STRUCTURES PRIOR TO CONSTRUCTING ANY DRAINAGE MODIFICATIONS. CONTRACTOR TO CONFIRM LOCATION OF EXISTING UTILITIES WITH TEST PITS.
- WHERE AN EXISTING UTILITY IS FOUND TO CONFLICT WITH THE PROPOSED WORK, THE LOCATION, ELEVATION AND SIZE OF THE UTILITY SHALL BE ACCURATELY DETERMINED WITHOUT DELAY BY THE CONTRACTOR, AND THE INFORMATION FURNISHED TO THE ENGINEER FOR RESOLUTION OF THE CONFLICT.
- THE CONTRACTOR SHALL ADJUST ALL MUNICIPAL UTILITY FRAMES AND COVERS AS REQUIRED. CONTRACTOR TO ARRANGE FOR INSPECTION OF ALL WORK ON PRIVATE UTILITIES WITH THE APPROPRIATE UTILITY COMPANY. IN THE EVENT ANY UTILITY COMPANY DOES NOT ALLOW THE CONTRACTOR TO MAKE THE ADJUSTMENTS, THE CONTRACTOR SHALL COORDINATE HIS WORK TO ALLOW FOR THE ADJUSTMENTS TO BE MADE BY THE UTILITY OR ITS DESIGNEE. THE EXPECTED UTILITIES INCLUDE BUT ARE NOT LIMITED TO: GAS, ELECTRIC, TELEPHONE, CATV, AND FIRE.
- CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND APPROVALS REQUIRED TO PERFORM THE WORK, INCLUDING ALL PERMITS REQUIRED BY THE CITY OF SALEM DEPARTMENT OF PUBLIC WORKS AND/OR WATER DEPARTMENT.
- PRIOR TO THE START OF CONSTRUCTION, CONTRACTOR SHALL COORDINATE ANY UTILITY RELOCATIONS WITH THE CITY OF SALEM DEPARTMENT OF PUBLIC WORKS AND ALL RESPECTIVE UTILITY AGENCIES. LOCATIONS FOR RELOCATED UTILITIES ARE TO BE DETERMINED BY THE RESPECTIVE UTILITY AGENCY IN CONSULTATION WITH THE SALEM DEPARTMENT OF PUBLIC WORKS.
- CONTRACTOR SHALL FOLLOW CITY OF SALEM DEPARTMENT OF PUBLIC WORKS STANDARD DETAILS WHERE APPLICABLE. ALL WORK DONE UNDER THIS CONTRACT SHALL BE COMPLETED IN CONFORMANCE TO MASSDOT "STANDARD SPECIFICATIONS FOR HIGHWAYS & BRIDGES" AND ANY UPDATES OR SUPPLEMENTAL SPECIFICATIONS PERTAINING THERETO, AND SPECIFICATIONS INCLUDED IN THIS CONTRACT.
- WORK SHALL NOT EXTEND BEYOND EXISTING ROW UNLESS OTHERWISE NOTED ON PLANS OR AS REQUIRED BY ENGINEER.
- INLETS AND OUTLETS FROM DRAINAGE STRUCTURES REMOVED SHALL BE PLUGGED WITH BRICK MASONRY NOT LESS THAN 8" IN THICKNESS, CONSISTENT WITH MASSDOT STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES SECTION 201. PIPES LARGER THAN 24" SHALL BE BACKFILLED WITH FLOWABLE FILL TO THE SATISFACTION OF THE CITY OF SALEM.
- NO WORK SHALL TAKE PLACE ON PRIVATE PROPERTY WITHOUT A SIGNED RIGHT OF ENTRY AGREEMENT BETWEEN THE PROPERTY OWNER AND THE CITY.
- WHEN FITTING GRANITE CURB OVER EXISTING UTILITIES AND OTHER OBSTRUCTIONS, NOTCHING AND CUTTING OF CURB SHALL TAKE PLACE WITH NO ADDITIONAL COST TO CITY.
- CONTRACTOR SHALL PROVIDE A STAKED LAYOUT FOR THE ENGINEER TO REVIEW PRIOR TO COMMENCING SIDEWALK WORK. THE CONTRACTOR SHALL PROVIDE ALL CUT AND FILL HEIGHTS AT ALL BACK OF SIDEWALK, STEPS, WALLS, DRIVEWAYS, WALKWAYS, BUILDING ENTRANCES, UTILITY STRUCTURES, ETC. ALL DISCREPANCIES THAT WILL CAUSE THE SIDEWALK NOT TO BE COMPLIANT WILL BE BROUGHT TO THE ENGINEER'S ATTENTION.
- FOR ALL PAY REQUESTS, CONTRACTOR SHALL PROVIDE BACK UP STATING DATES INSTALLED, QUANTITY INSTALLED, CALCULATION ON QUANTITY, AND A MARKED UP DRAWING SHOWING LOCATION INSTALLED.

SURVEY NOTES:

- THE TOPOGRAPHY, SITE DETAIL & SURFACE IMPROVEMENTS DEPICTED HEREON WERE OBTAINED FROM AN INSTRUMENT SURVEY CONDUCTED ON THE GROUND BY MERIDIAN ASSOCIATES, INC. (MAI) BETWEEN DECEMBER 30, 2016 & JANUARY 25, 2017 AND SUPPLEMENTED WITH AUTOCAD FILES DEPICTING RECENT PRIVATE DEVELOPMENT SITES INCLUDING THE SALEM COMMUNITY LIFE CENTER, 162 FEDERAL STREET AND RIVER ROCK RESIDENCES.
- THE LOCATION OF ALL UNDERGROUND UTILITIES SHOWN ARE APPROXIMATE AND ARE BASED UPON A PARTIAL FIELD SURVEY AND COMPILATION OF AVAILABLE PLANS OF RECORD. MERIDIAN ASSOCIATES, INC. DOES NOT WARRANTY NOR GUARANTEE THE LOCATION OF ALL UTILITIES DEPICTED OR NOT DEPICTED. THE CONTRACTOR, PRIOR TO COMMENCEMENT OF CONSTRUCTION, SHALL VERIFY THE LOCATION OF ALL UTILITIES AND CONTACT DIG SAFE AT 1-888-344-7233. TRAFFIC ELECTRIC CONDUIT IS NOT DEPICTED HEREON. AT THE TIME OF FINAL RELEASE OF THIS DRAWING MERIDIAN HAD NOT RECEIVED CONDUIT RECORDS OF SEWER, DRAIN OR WATER FROM THE CITY OF SALEM.
- THE SUBJECT PREMISES IS LOCATED IN THE FOLLOWING FLOOD ZONES PER FEMA FLOOD INSURANCE RATE MAPS 25009C0418G & 25009C0419G DATED JULY 16, 2014:
-ZONE X (UNSHADED), AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN;
-ZONE X (SHADED), AREAS OF 0.2% ANNUAL CHANCE FLOODPLAIN; AREAS OF 1% ANNUAL CHANCE FLOOD WITH AVERAGE DEPTHS OF LESS THAN 1 FOOT OR WITH DRAINAGE AREAS LESS THAN 1 SQUARE MILE; AND AREAS PROTECTED BY LEVEES FROM 1% ANNUAL CHANCE FLOOD;
-ZONE AE, AREAS DETERMINED TO HAVE A BASE FLOOD ELEVATION (ELEV=10 NAVD 88)
- THIS PLAN DOES NOT SHOW ANY UNRECORDED OR UNWRITTEN EASEMENTS WHICH MAY EXIST. THIS DOES NOT CONSTITUTE A GUARANTEE THAT NO SUCH EASEMENTS EXIST.
- THE ELEVATIONS DEPICTED HEREON WERE BASED ON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88). STARTING BENCHMARK: COMMONWEALTH OF MASSACHUSETTS HIGHWAY DEPARTMENT BENCHMARK 805C. MARK IS A BRASS RIVET SET IN A CONCRETE CHANNEL WALL ON BRIDGE STREET AT FLINT STREET. ELEVATION=9.58 FEET.
- THIS PLAN IS NOT TO BE USED FOR THE RECONSTRUCTION OF RIGHT OF WAY OR EASEMENT LINES OR FOR TITLE INSURANCE PURPOSES. RIGHT OF WAY AND EASEMENT LINES DEPICTED HEREON ARE APPROXIMATE ONLY. MAI DID NOT PERFORM A BOUNDARY RETRACEMENT SURVEY. RIGHT OF WAY LINES ARE PROVIDED BASED UPON LIMITED LAYOUT RECORDS, LAND PLANS RECOVERED AT SOUTH ESSEX REGISTRY OF DEEDS AND APPARENT OCCUPATION. PROPERTY LINES ARE APPROXIMATE ONLY AND COMPILED FROM CITY OF SALEM GIS.



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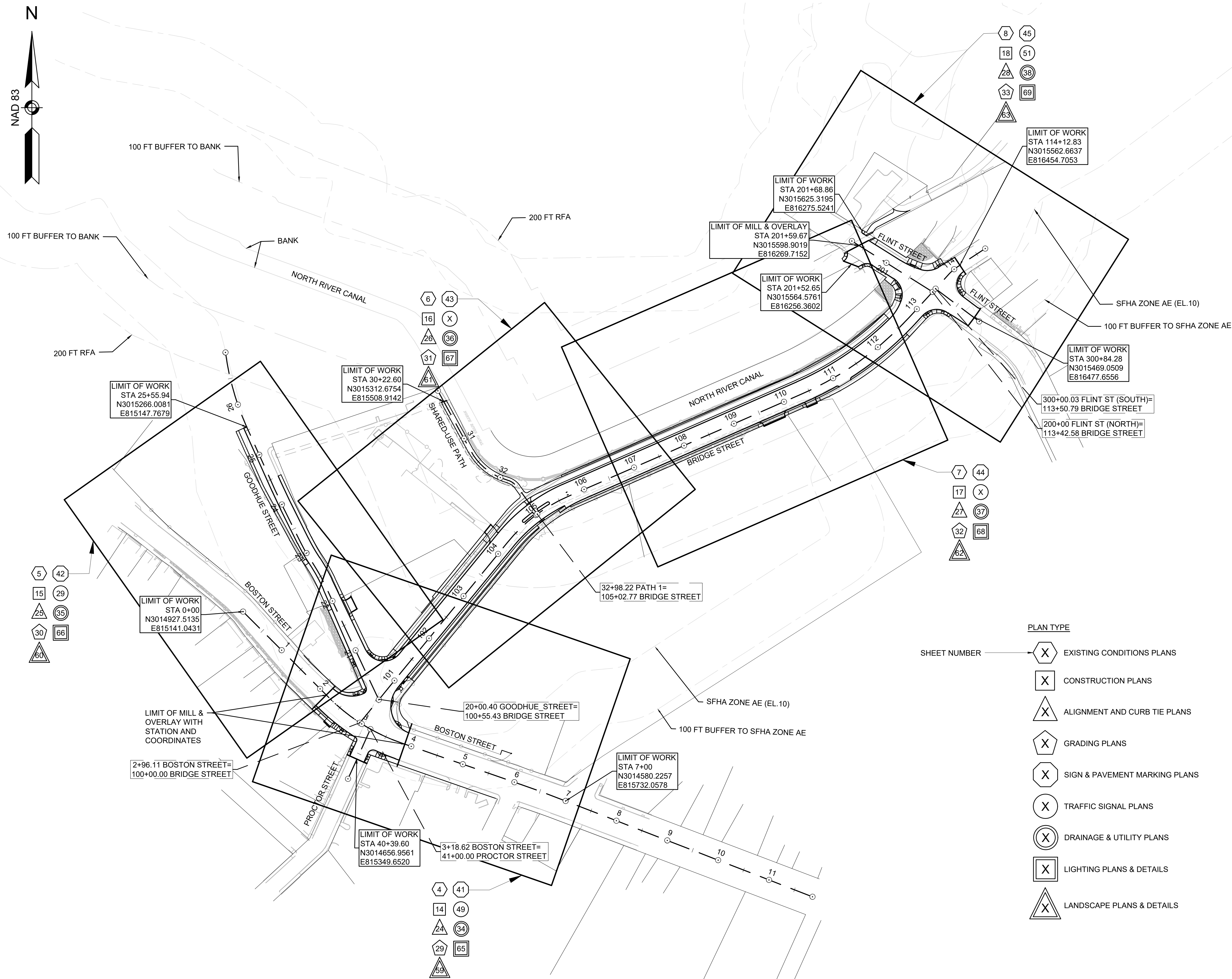


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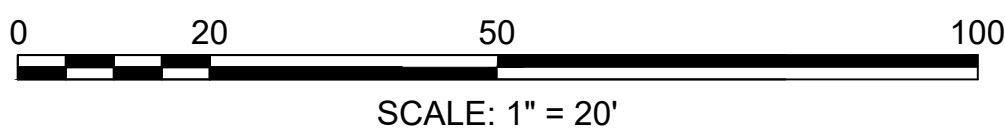
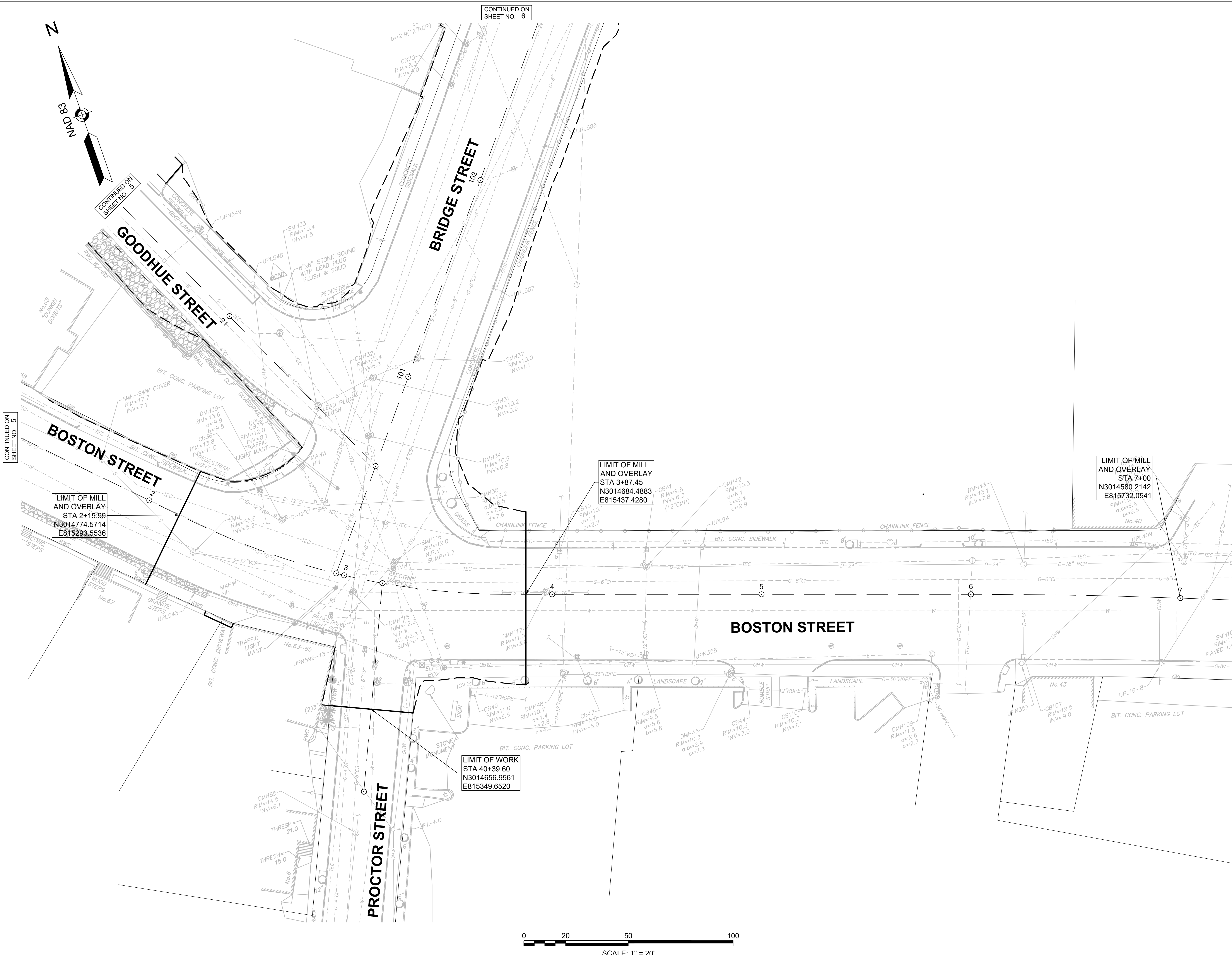
BRIDGE STREET RECONSTRUCTION PROJECT

Title
KEY PLAN

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Drawing No.	Sheet	Revision



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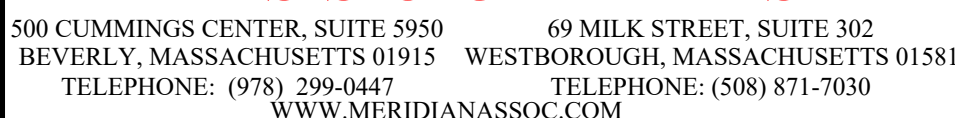


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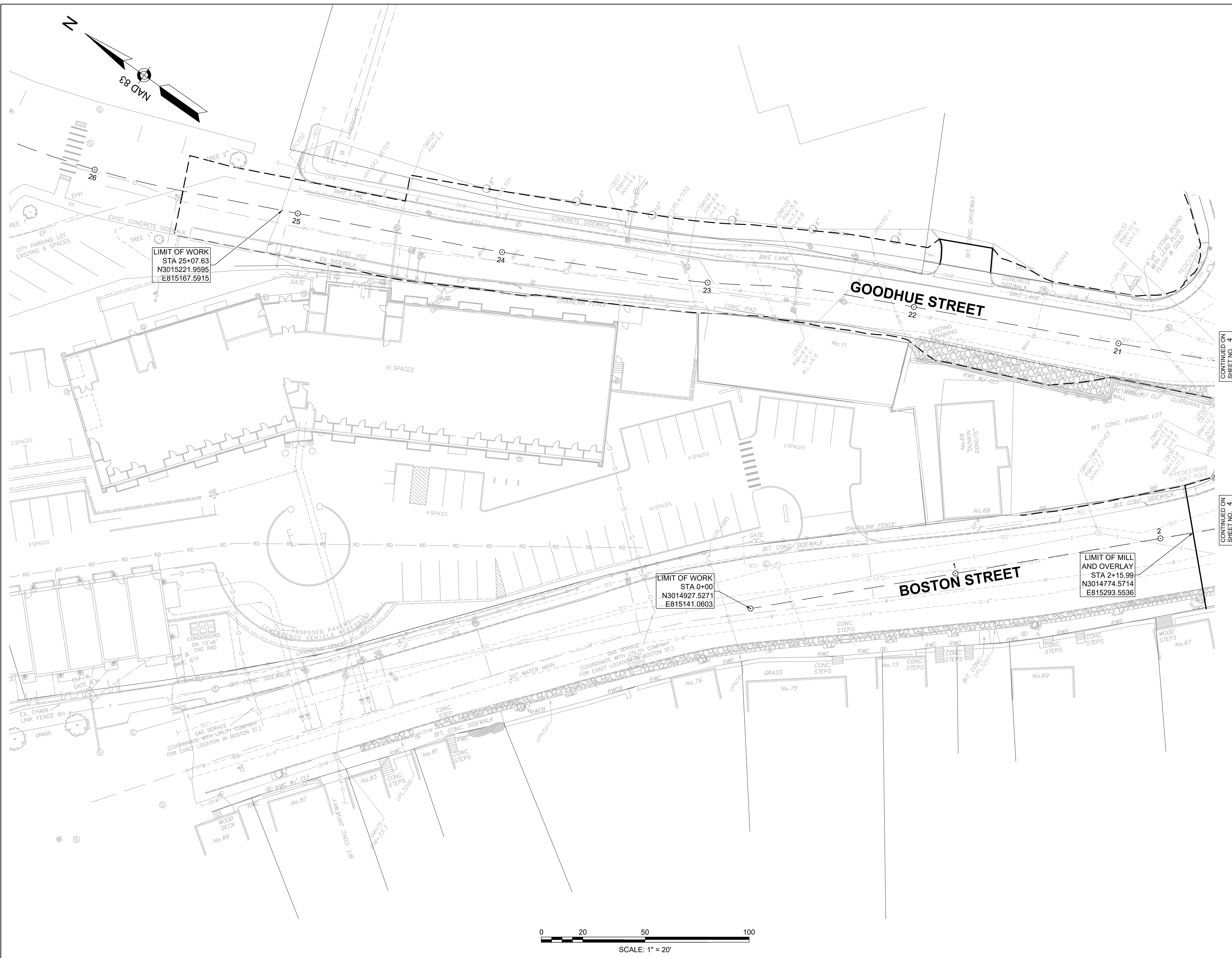
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PART 2 OF 5

179410455 1" = 20'

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5 of 113

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EXISTING CONDITIONS PLAN
PART 3 OF 5

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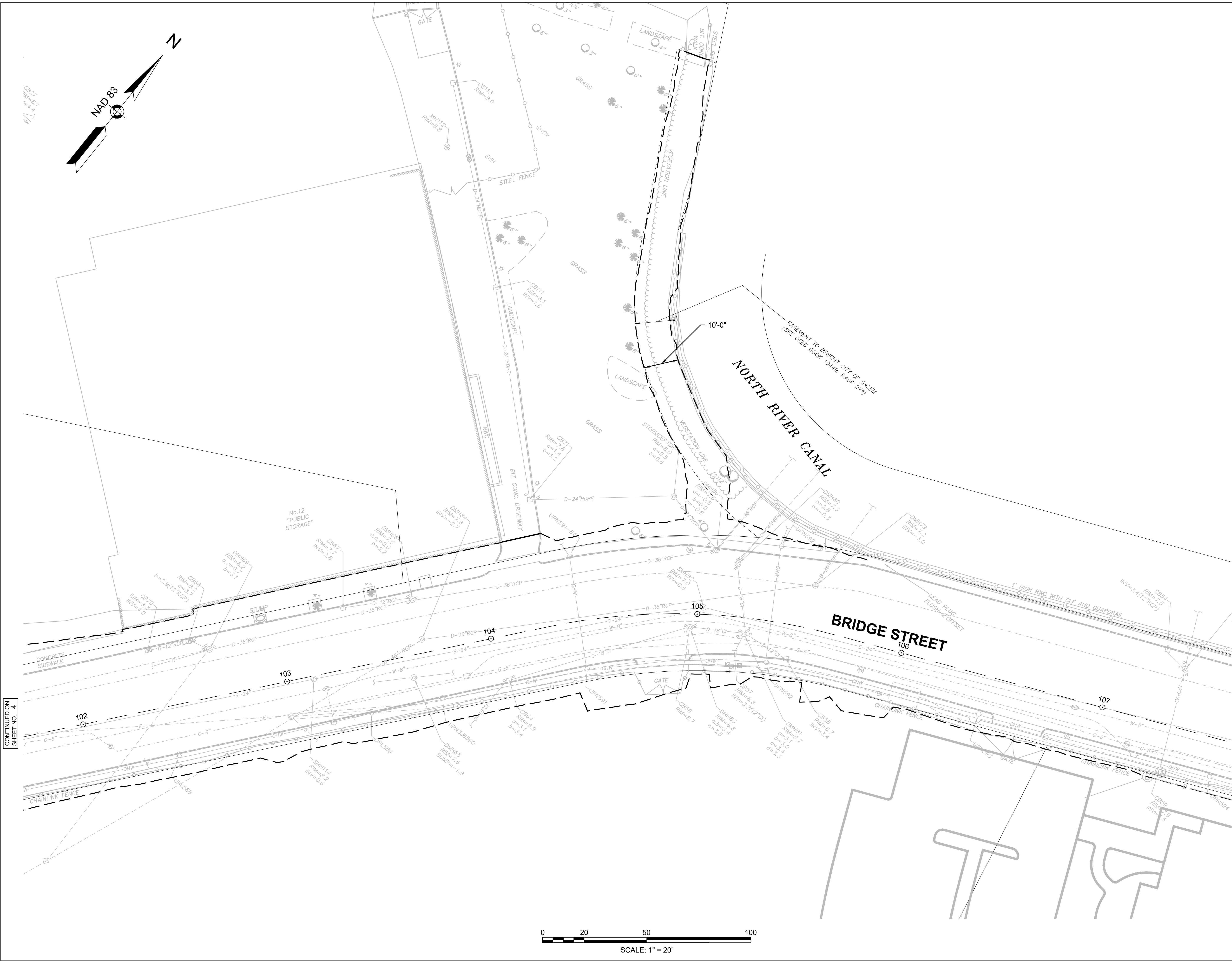
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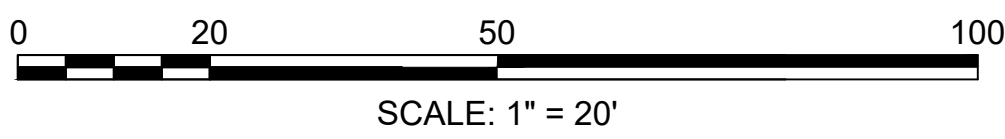
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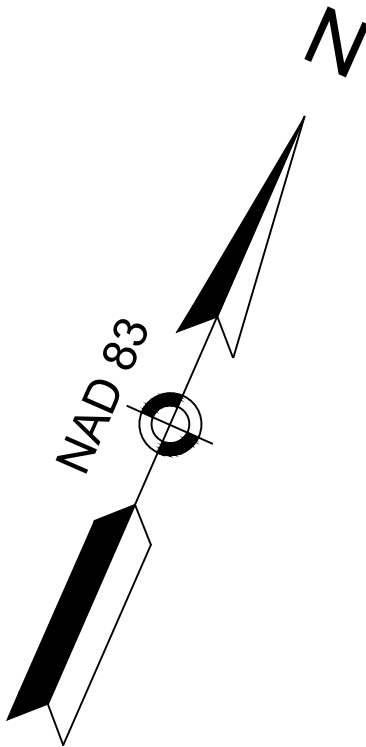


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SHEET NO. 7

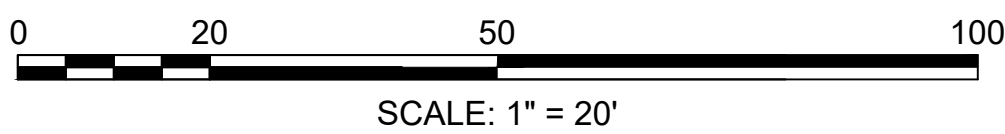


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NORTH RIVER CANAL

BRIDGE STREET





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Title
EXISTING CONDITIONS PLAN
PART 5 OF 5

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SCALE: 1" = 20'

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Legend
SEE SHEET 2
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10. *Journal of the American Medical Association*, 2000; 284: 2689-2695.



* 0.5% TOLERANCE FOR CONSTRUCTION
** SEE GRADING PLANS AND CROSS SECTIONS FOR SLOPES



1. MEET EXISTING ELEVATIONS AT BACK OF SIDEWALK WHERE POSSIBLE WHILE MAINTAINING ADA COMPLIANCE. EXISTING AND PROPOSED ELEVATIONS ARE SHOWN ON THE GRADING PLANS WHERE BACK OF SIDEWALK ELEVATIONS ARE NOT MET.



ASPHALT EMULSION FOR TACK COAT SHALL BE PLACED ON ALL MILLED SURFACES (APPLICATION RATE = 0.07 GAL/SY) AND BETWEEN ALL NEW PAVEMENT LAYERS (APPLICATION RATE = 0.05 GAL/SY).

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Title
TYPICAL SECTIONS BRIDGE STREET
PART 1 OF 2

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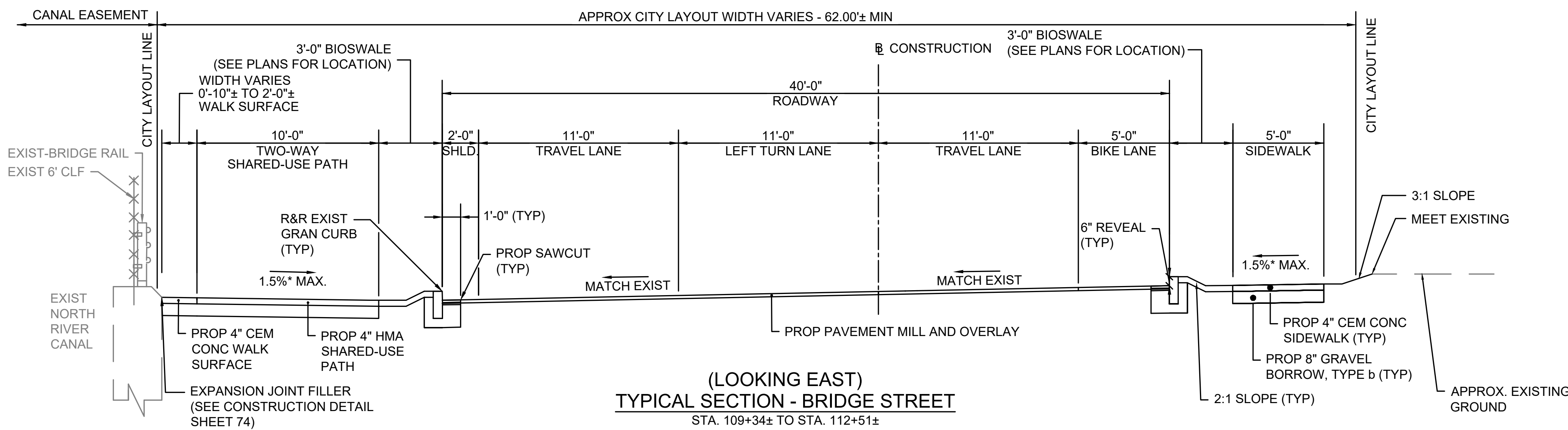
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Legend

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Notes

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* 0.5% TOLERANCE FOR CONSTRUCTION
** SEE GRADING PLANS AND CROSS SECTIONS FOR SLOPES



NOTES:

- MEET EXISTING ELEVATIONS AT BACK OF SIDEWALK WHERE POSSIBLE WHILE MAINTAINING ADA COMPLIANCE. EXISTING AND PROPOSED ELEVATIONS ARE SHOWN ON THE GRADING PLANS WHERE BACK OF SIDEWALK ELEVATIONS ARE NOT MET.

Revision	By	Appd.	YY.MM.DD
BID SET			
Issued	By	Appd.	YY.MM.DD
File Name:	dwg_typical_sections.dwg	Dwn.	Chkd.
		Dsgn.	YY.MM.DD

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CITY OF SALEM
Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title
TYPICAL SECTIONS BRIDGE STREET
PART 2 OF 2

179410455	1" = 4'-0"
Project No.	Scale
	10 of 113
Drawing No.	Sheet
	Revision

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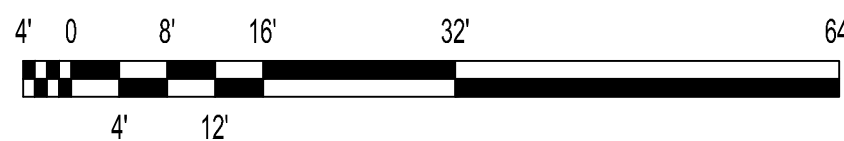
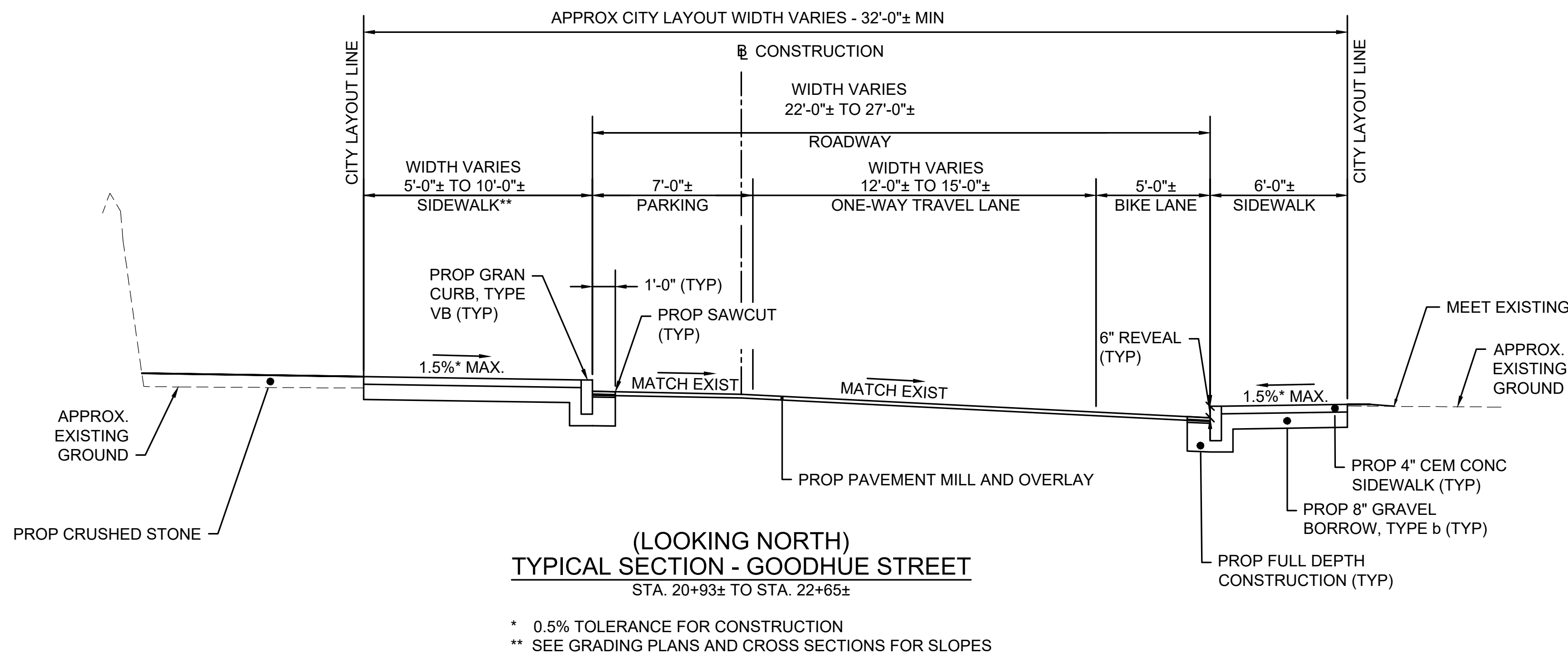
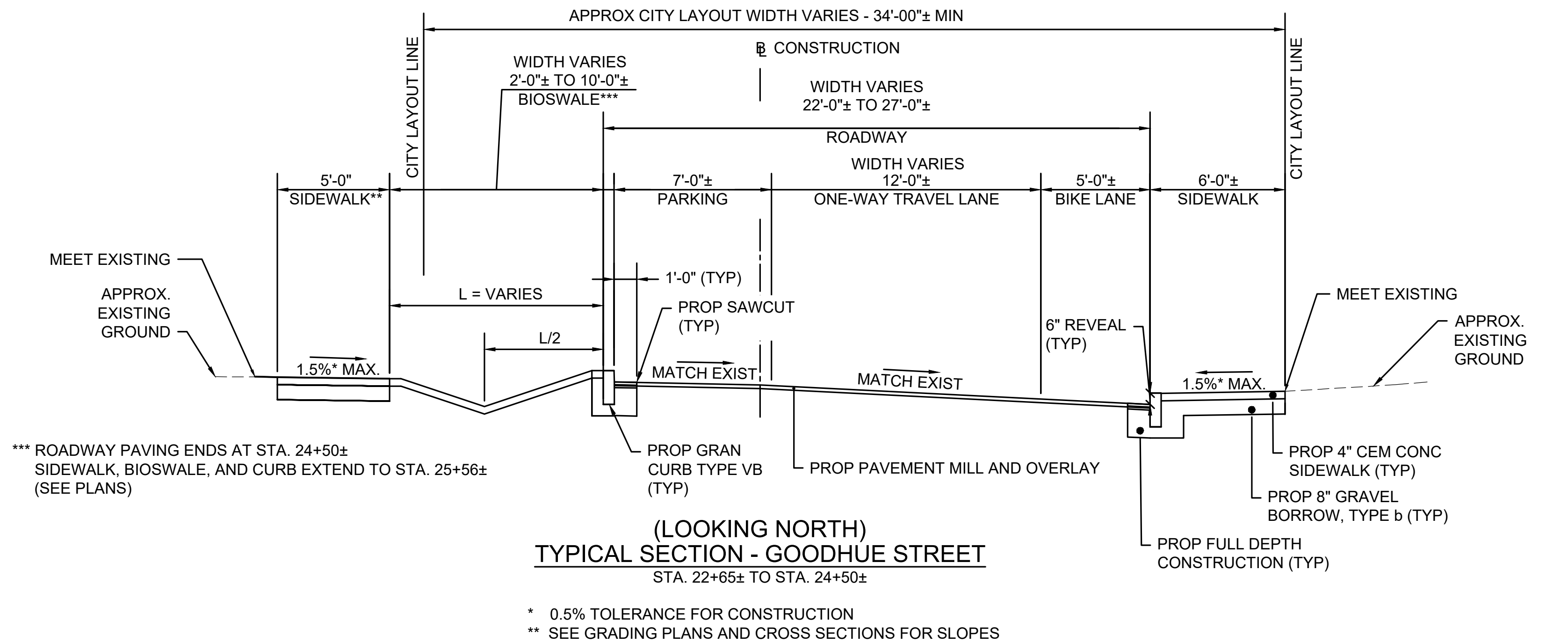
Consultants

Legend

SEE SHEET 2
SEE THIS SHEET

Notes

SEE SHEET 2



NOTES:

- MEET EXISTING ELEVATIONS AT BACK OF SIDEWALK WHERE POSSIBLE WHILE MAINTAINING ADA COMPLIANCE. EXISTING AND PROPOSED ELEVATIONS ARE SHOWN ON THE GRADING PLANS WHERE BACK OF SIDEWALK ELEVATIONS ARE NOT MET.

Revision	By	Appd.	YY.MM.DD
BID SET			
Issued	By	Appd.	YY.MM.DD
File Name:	dwg_typical_sections.dwg	Dwn.	Chkd.
		Dsgn.	YY.MM.DD

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CITY OF SALEM

Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title

TYPICAL SECTIONS
GOODHUE STREET

179410455	1" = 4'-0"
Project No.	Scale
	11 of 113
Drawing No.	Sheet
	Revision

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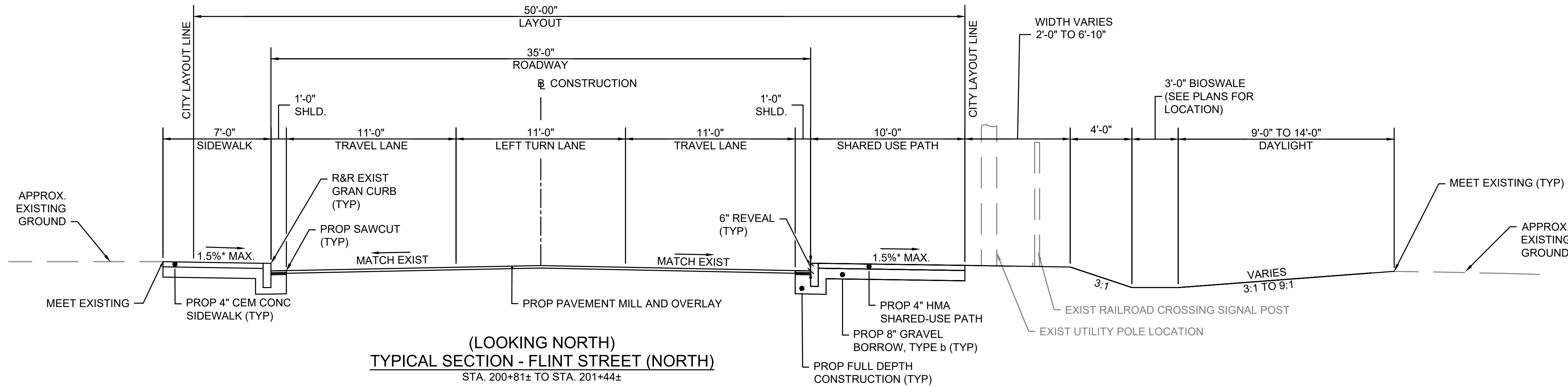
Consultants

Legend

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SEE THIS SHEET

Notes

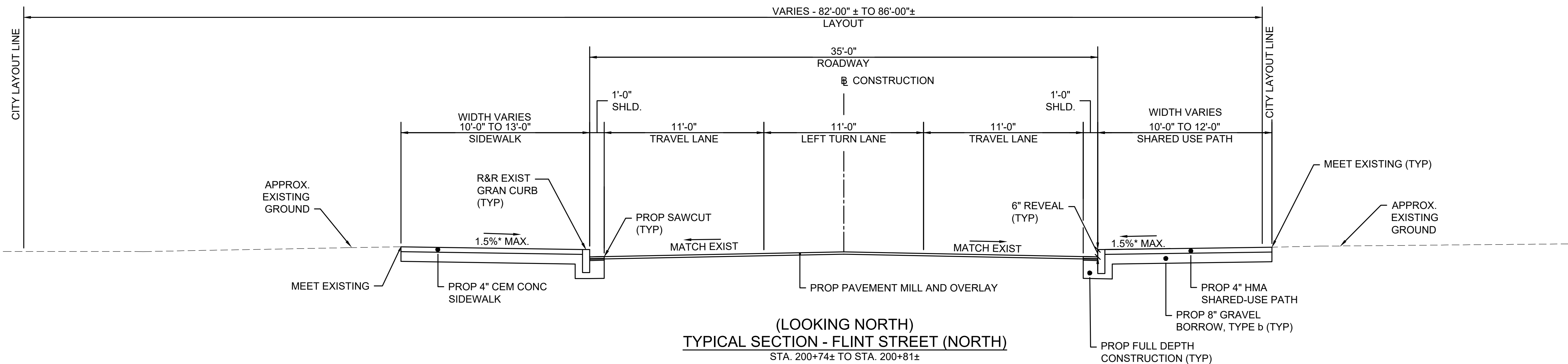
SEE SHEET 2



(LOOKING NORTH)
TYPICAL SECTION - FLINT STREET (NORTH)
STA. 200+81± TO STA. 201+44±

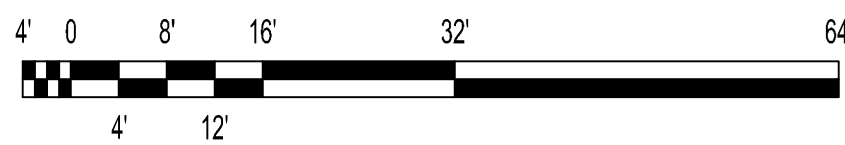
* 0.5% TOLERANCE FOR CONSTRUCTION
** SEE GRADING PLANS AND CROSS SECTIONS FOR SLOPES

NOTES:
- FROM STA 201+44 TO 201+50 SEE CONSTRUCTION PLANS
- SEE CONSTRUCTION PLANS FOR CONNECTION OF SHARED USE PATH TO EXISTING PATH



(LOOKING NORTH)
TYPICAL SECTION - FLINT STREET (NORTH)
STA. 200+74± TO STA. 200+81±

* 0.5% TOLERANCE FOR CONSTRUCTION
** SEE GRADING PLANS AND CROSS SECTIONS FOR SLOPES



SCALE: 1" = 4'-0"

NOTES:

- MEET EXISTING ELEVATIONS AT BACK OF SIDEWALK WHERE POSSIBLE WHILE MAINTAINING ADA COMPLIANCE. EXISTING AND PROPOSED ELEVATIONS ARE SHOWN ON THE GRADING PLANS WHERE BACK OF SIDEWALK ELEVATIONS ARE NOT MET.

Revision	By	Appd.	YY.MM.DD
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CITY OF SALEM
Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title

TYPICAL SECTIONS
FLINT STREET

179410455 1" = 4'-0"

Project No. Scale

12 of 113

Drawing No. Sheet Revision

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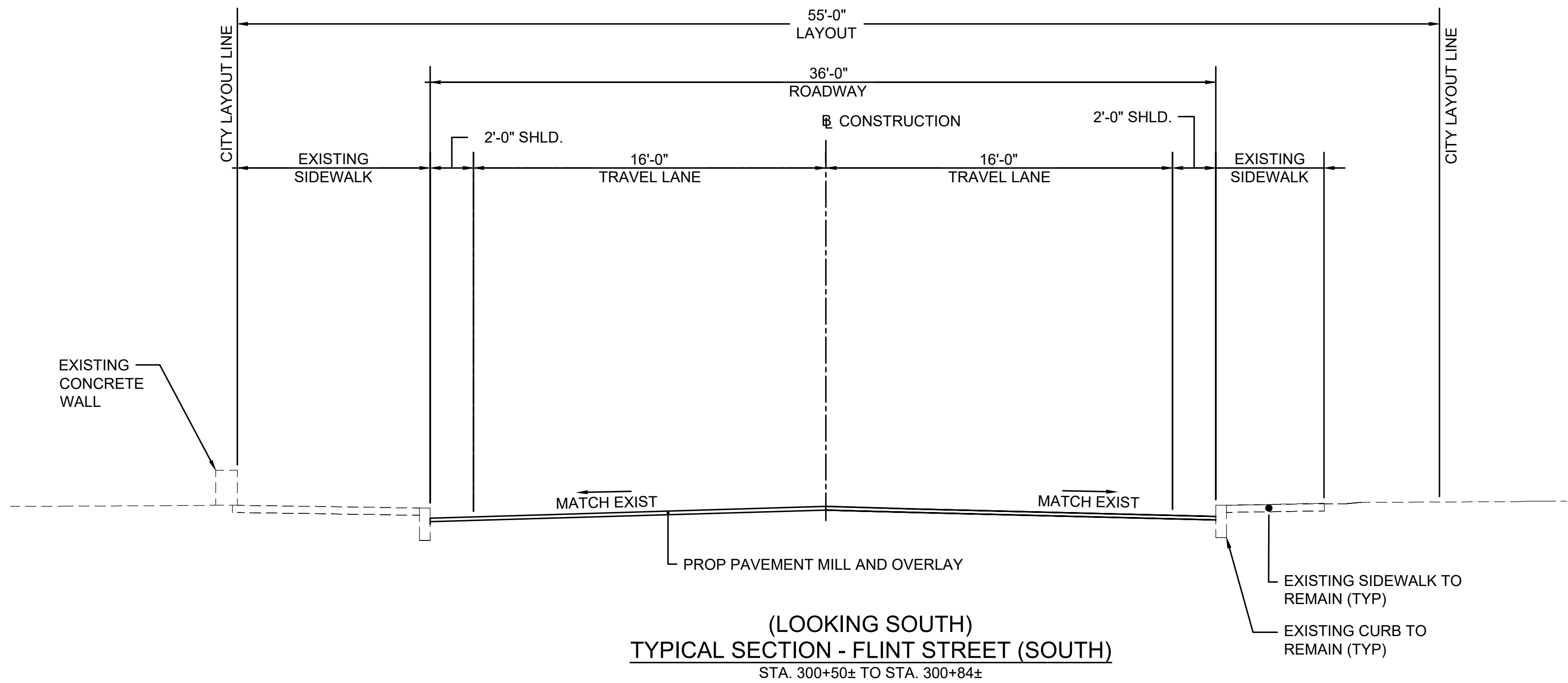
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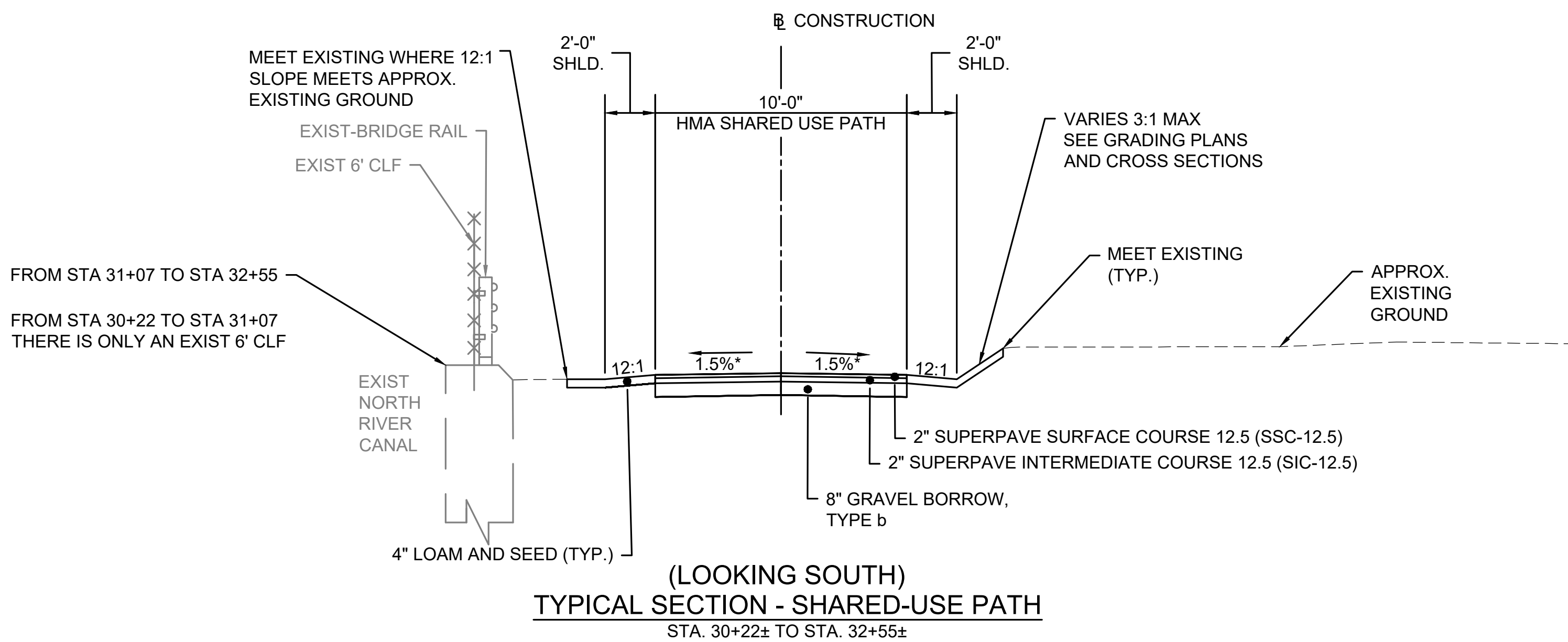
SEE SHEET 2
SEE THIS SHEET

Notes

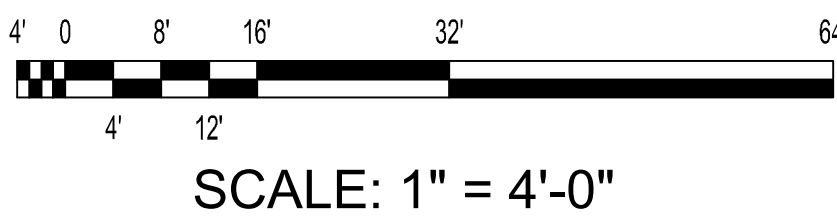
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- * 0.5% TOLERANCE FOR CONSTRUCTION
- ** SEE GRADING PLANS AND CROSS SECTIONS FOR SLOPES



- * 0.5% TOLERANCE FOR CONSTRUCTION
- ** SEE GRADING PLANS AND CROSS SECTIONS FOR SLOPES



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File Name:	dwg_typical_sections.dwg	Dwn.	Chkd.	Dsgn.	YY.MM.DD
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CITY OF SALEM
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BRIDGE STREET RECONSTRUCTION PROJECT

Title
TYPICAL SECTIONS FLINT STREET
AND SHARED-USE PATH

179410455	1" = 4'-0"
Project No.	Scale
	13 of 113
Drawing No.	Sheet
	Revision

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By Appd. YY.MM.DD

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BRIDGE STREET RECONSTRUCTION PROJECT

Title

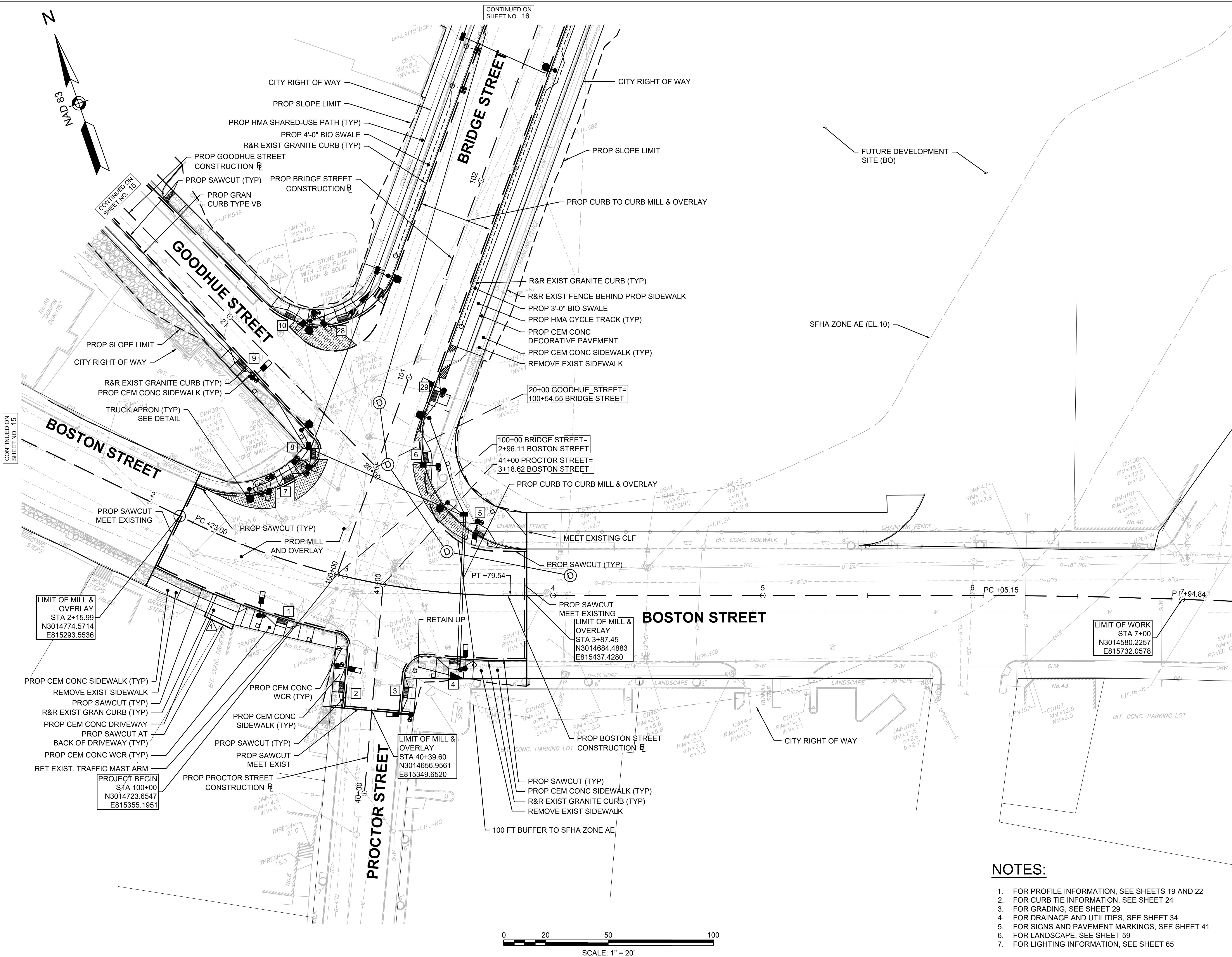
CONSTRUCTION PLAN
PART 1 OF 5

179410455 1" = 20'

Project No. Scale

14 of 113

Drawing No. Sheet Revision



NOTES:

1. FOR PROFILE INFORMATION, SEE SHEETS 19 AND 22
2. FOR CURB TIE INFORMATION, SEE SHEET 24
3. FOR GRADING, SEE SHEET 29
4. FOR DRAINAGE AND UTILITIES, SEE SHEET 34
5. FOR SIGNS AND PAVEMENT MARKINGS, SEE SHEET 41
6. FOR LANDSCAPE, SEE SHEET 59
7. FOR LIGHTING INFORMATION, SEE SHEET 65



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Notes
SEE SHEET 2

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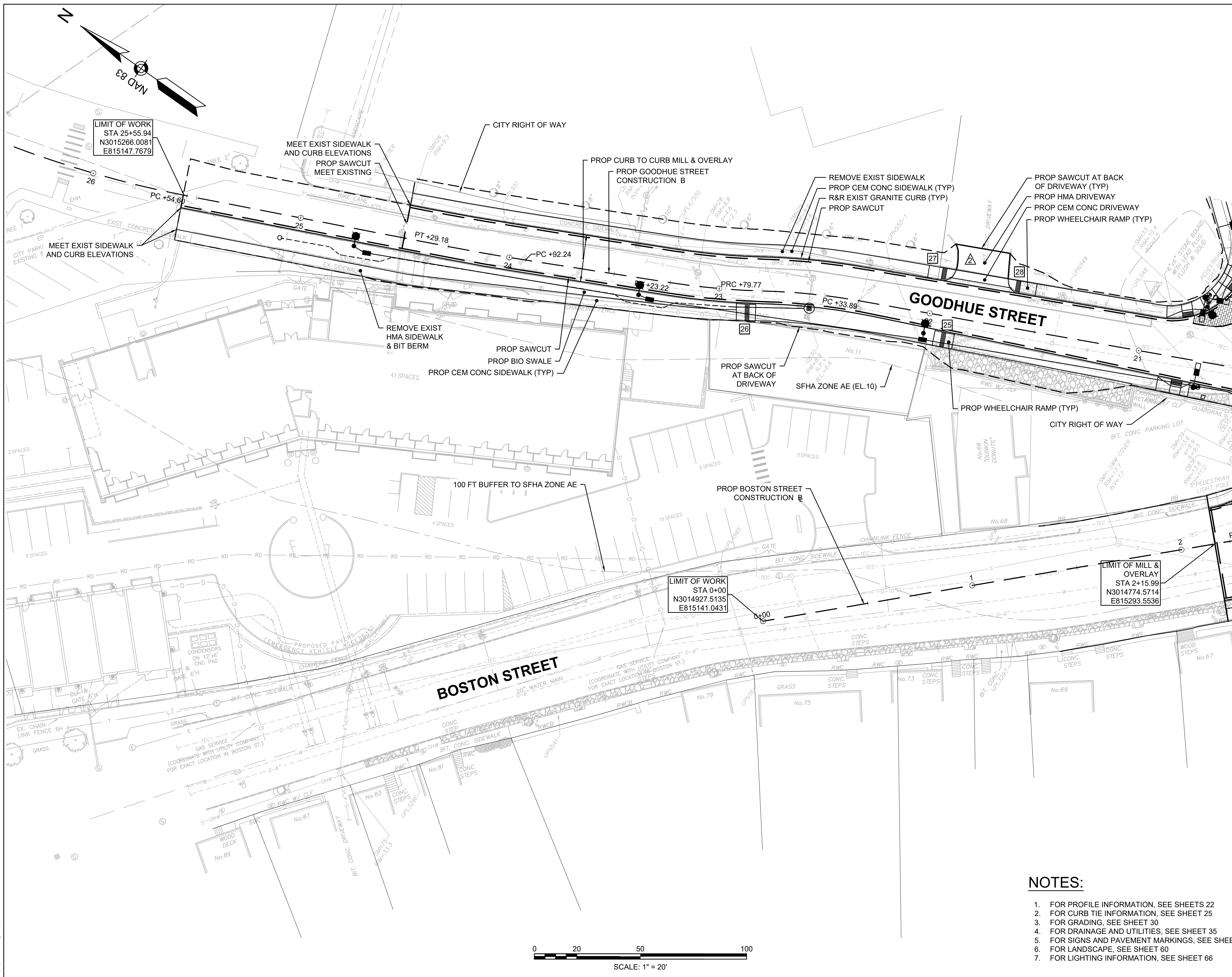
BRIDGE STREET RECONSTRUCTION PROJECT

Title
CONSTRUCTION PLAN
PART 2 OF 5

179410455	1" = 20'	
Project No.	Scale	
	15 of 113	
Drawing No.	Sheet	Revision

NOTES:

1. FOR PROFILE INFORMATION, SEE SHEETS 22
2. FOR CURB TIE INFORMATION, SEE SHEET 25
3. FOR GRADING, SEE SHEET 30
4. FOR DRAINAGE AND UTILITIES, SEE SHEET 35
5. FOR SIGNS AND PAVEMENT MARKINGS, SEE SHEET 42
6. FOR LANDSCAPE, SEE SHEET 60
7. FOR LIGHTING INFORMATION, SEE SHEET 66



0 20 50 100

SCALE: 1" = 20'

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2020/02/05 1:05 PM By: Sadtler, Chris



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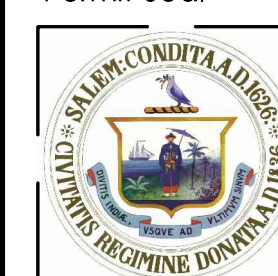
Notes
SEE SHEET 2

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BRIDGE STREET RECONSTRUCTION PROJECT

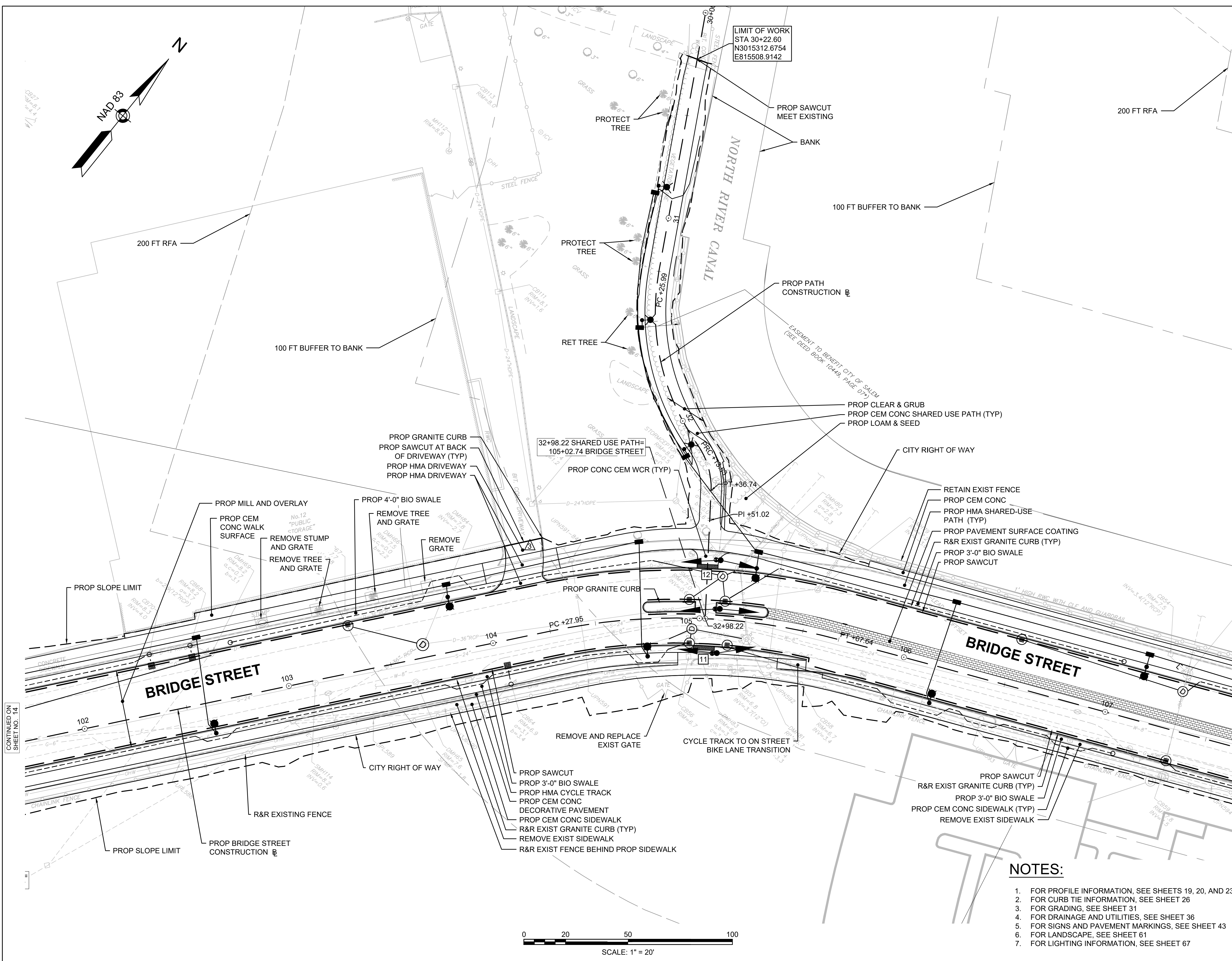
Title
CONSTRUCTION PLAN
PART 3 OF 5

179410455 1" = 20'

Project No.	Scale
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Project No. _____ Scale _____

Drawing No.	Sheet	Revision
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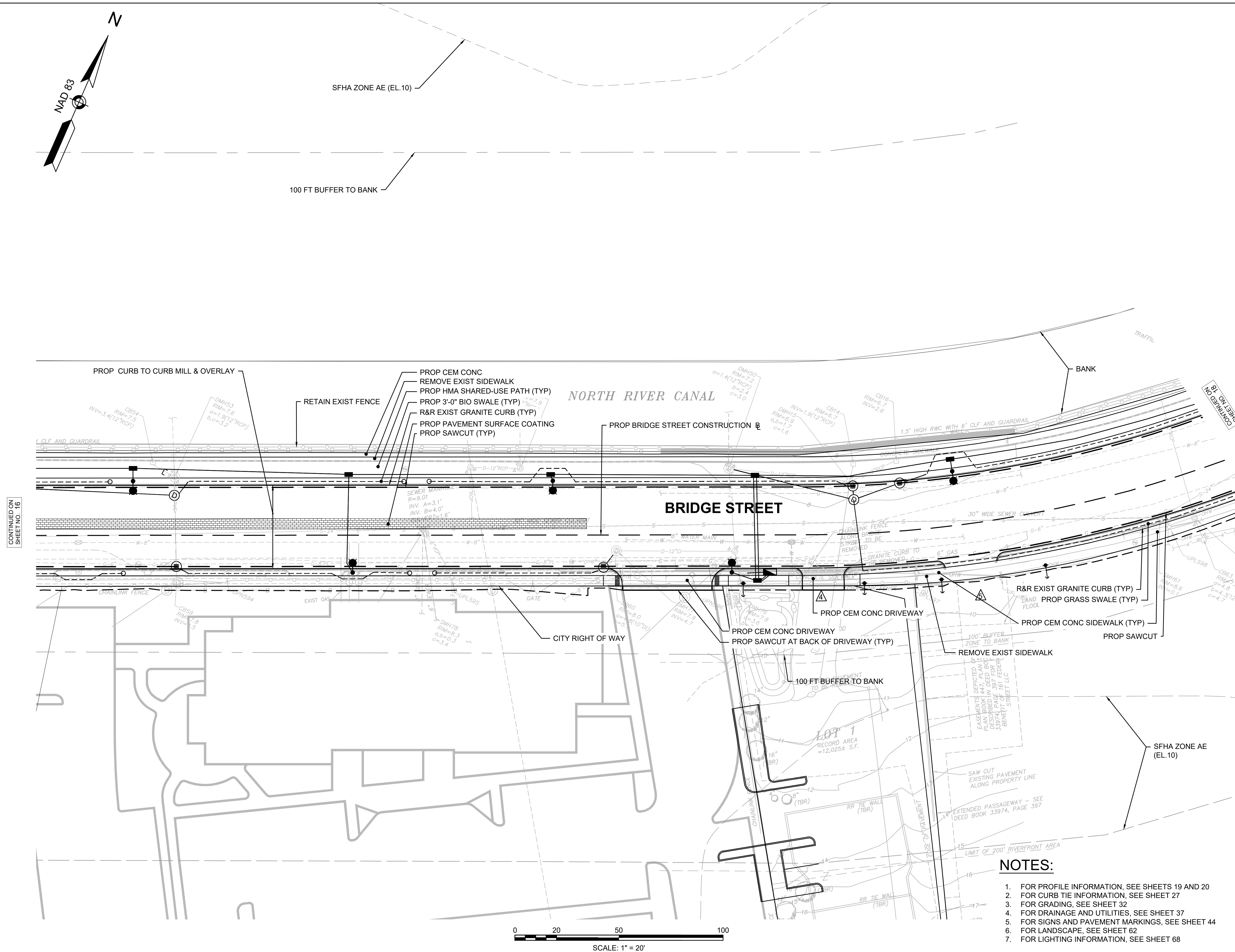


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BRIDGE STREET RECONSTRUCTION PROJECT

Title
CONSTRUCTION PLAN
PART 4 OF 5

179410455	1" = 20'	
Project No.	Scale	
	17 of 113	
Drawing No.	Sheet	Revision



NOTES:

1. FOR PROFILE INFORMATION, SEE SHEETS 19 AND 20
2. FOR CURB TIE INFORMATION, SEE SHEET 27
3. FOR GRADING, SEE SHEET 32
4. FOR DRAINAGE AND UTILITIES, SEE SHEET 37
5. FOR SIGNS AND PAVEMENT MARKINGS, SEE SHEET 44
6. FOR LANDSCAPE, SEE SHEET 62
7. FOR LIGHTING INFORMATION, SEE SHEET 68



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Notes
SEE SHEET 2

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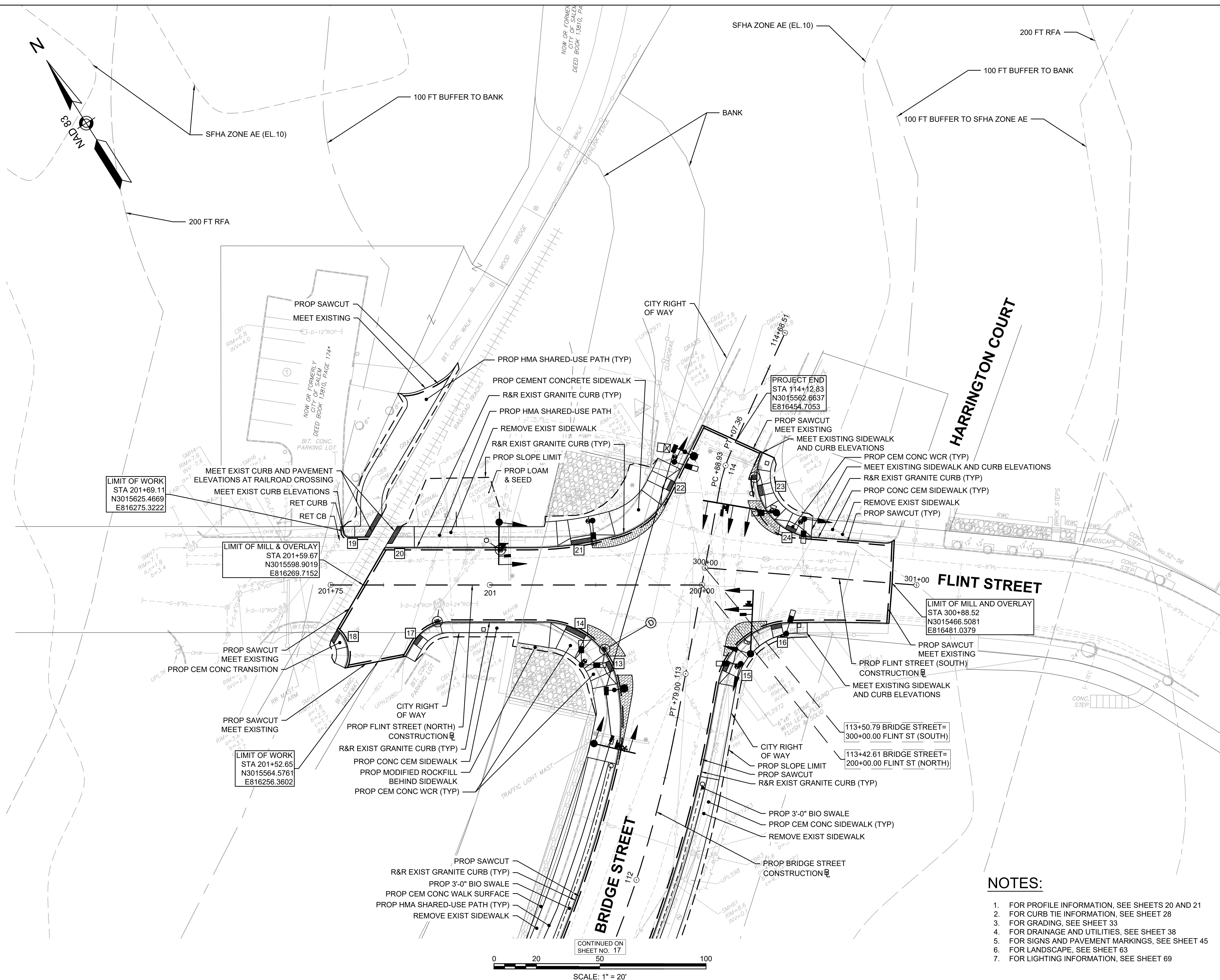
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BRIDGE STREET RECONSTRUCTION PROJECT

Title
CONSTRUCTION PLAN
PART 5 OF 5

179410455	1" = 20'	
Project No.	Scale	
	18 of 113	
Drawing No.	Sheet	Revision



NOTES:

1. FOR PROFILE INFORMATION, SEE SHEETS 20 AND 21
2. FOR CURB TIE INFORMATION, SEE SHEET 28
3. FOR GRADING, SEE SHEET 33
4. FOR DRAINAGE AND UTILITIES, SEE SHEET 38
5. FOR SIGNS AND PAVEMENT MARKINGS, SEE SHEET 45
6. FOR LANDSCAPE, SEE SHEET 63
7. FOR LIGHTING INFORMATION, SEE SHEET 69

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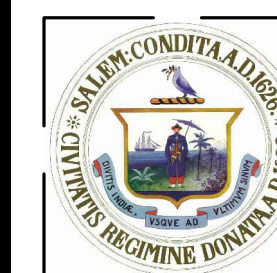
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Legend
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Notes
SEE SHEET 2

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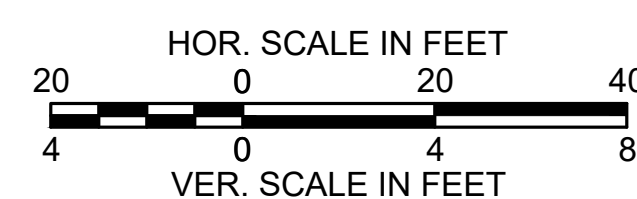
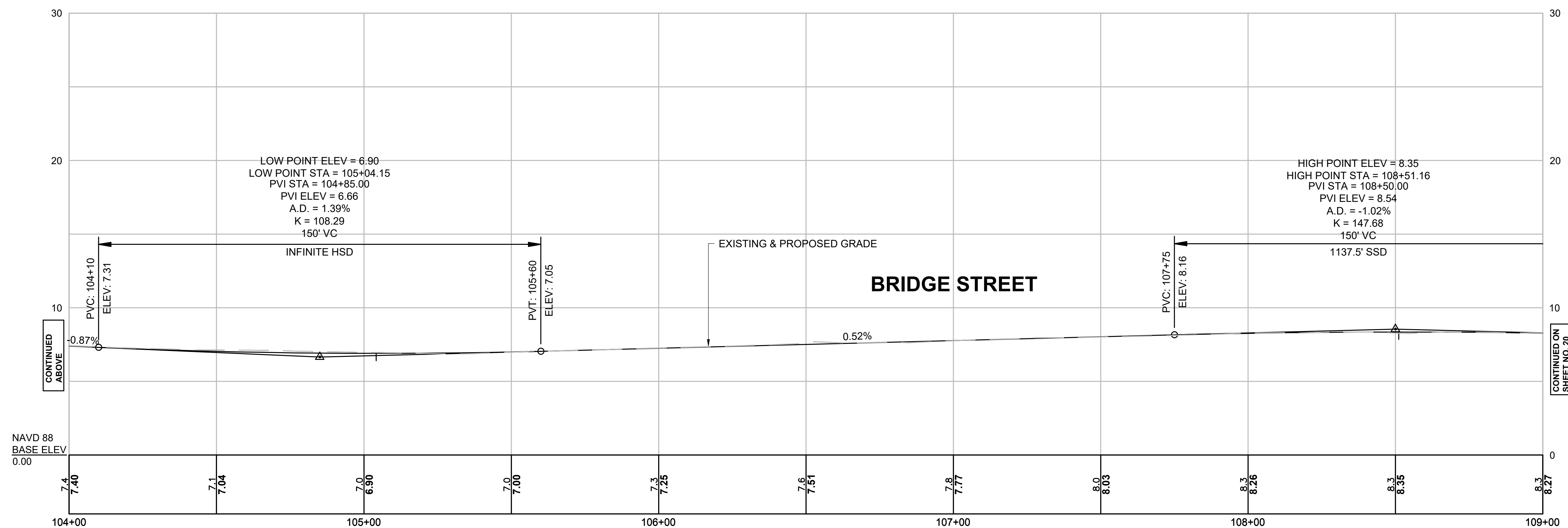
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BRIDGE STREET RECONSTRUCTION PROJECT

Title
PROFILE - BRIDGE STREET PART 1 OF 2

179410455	H:1" = 20' V:1"=4'
Project No.	Scale

Drawing No.	Sheet	Revision
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NOTES:
1. FOR CONSTRUCTION PLAN SEE SHEET 14, 16, AND 17.

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SEE THIS SHEET

Notes

SEE SHEET 2

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BRIDGE STREET RECONSTRUCTION PROJECT

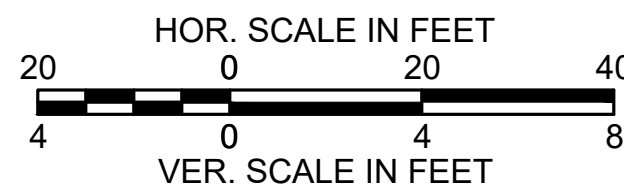
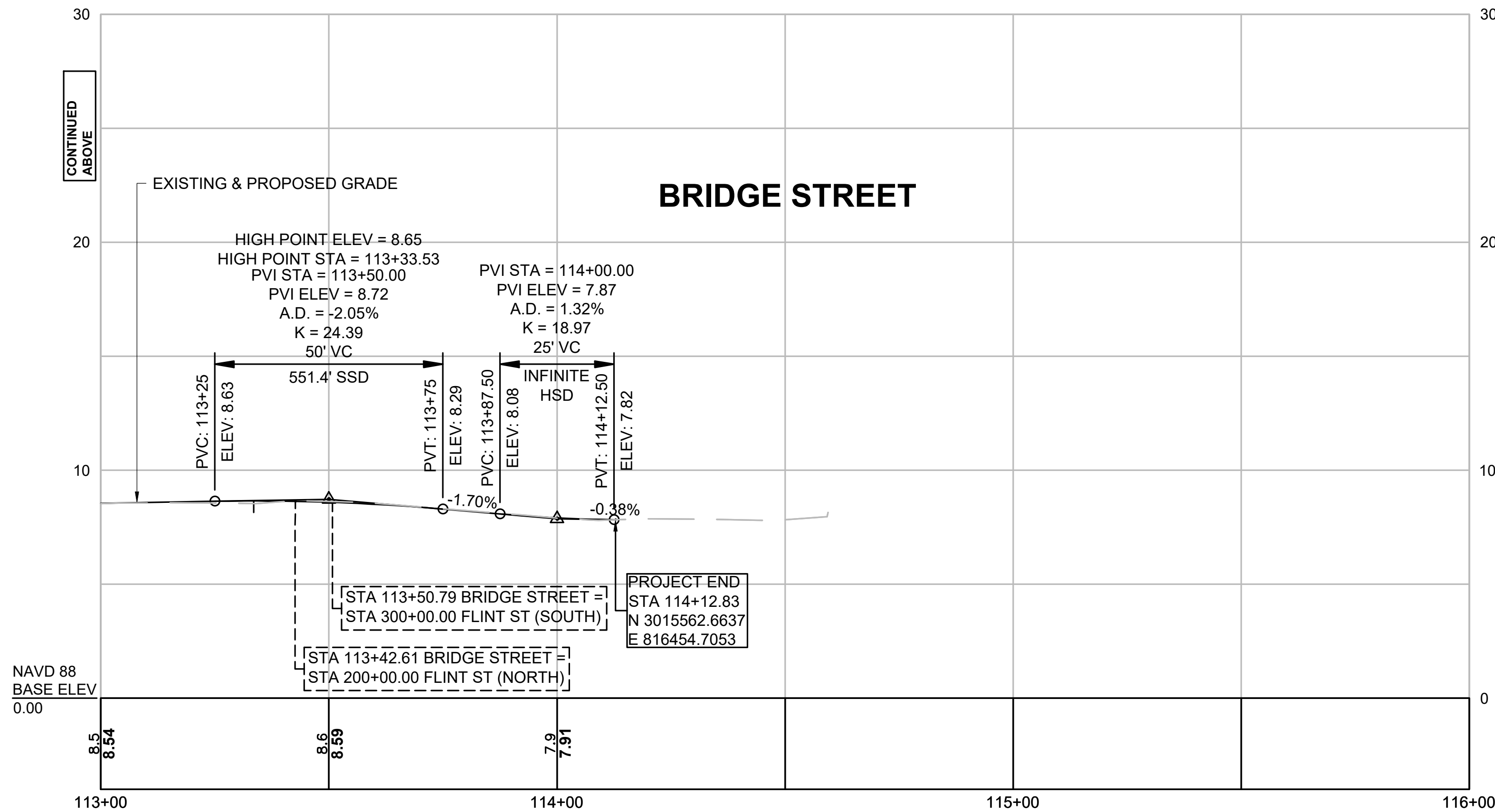
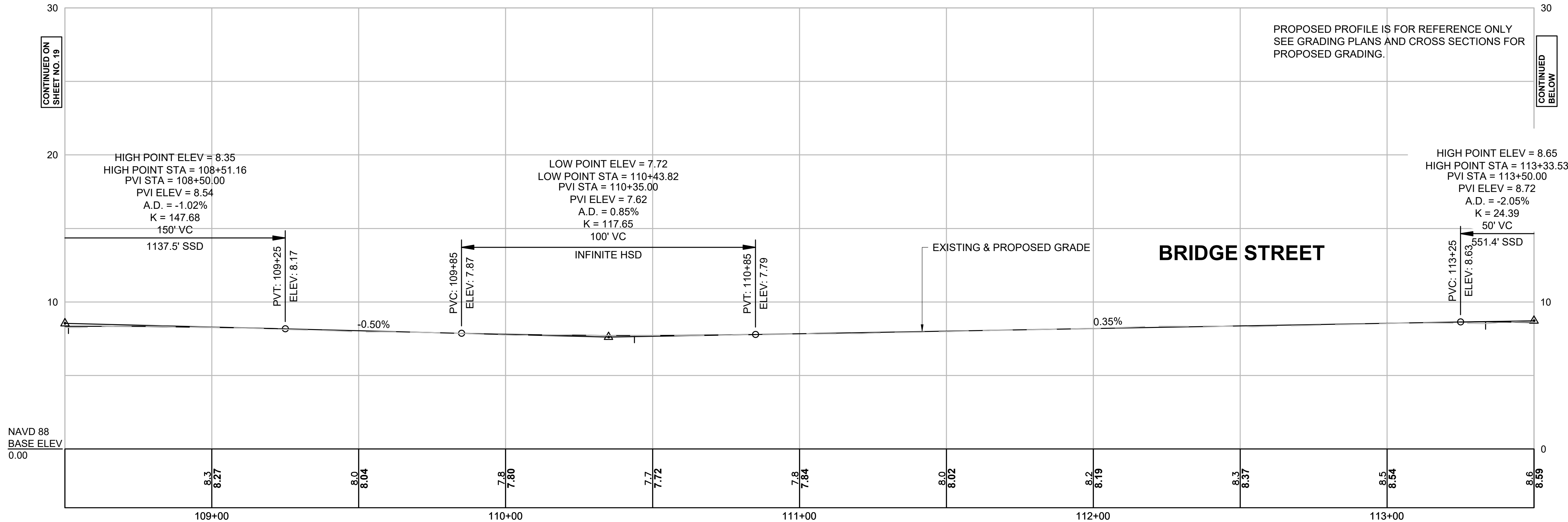
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PROFILE - BRIDGE STREET PART 2 OF 2

179410455 H:1" = 20' V:1"=4'

Project No. Scale

20 of 113

Drawing No. Sheet Revision



NOTES:
1. FOR CONSTRUCTION PLAN SEE SHEET 17 AND 18.

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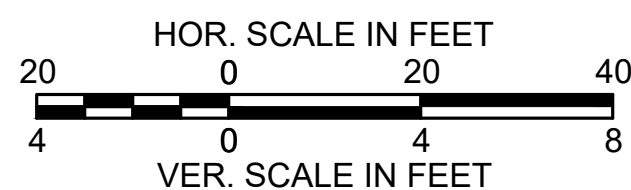
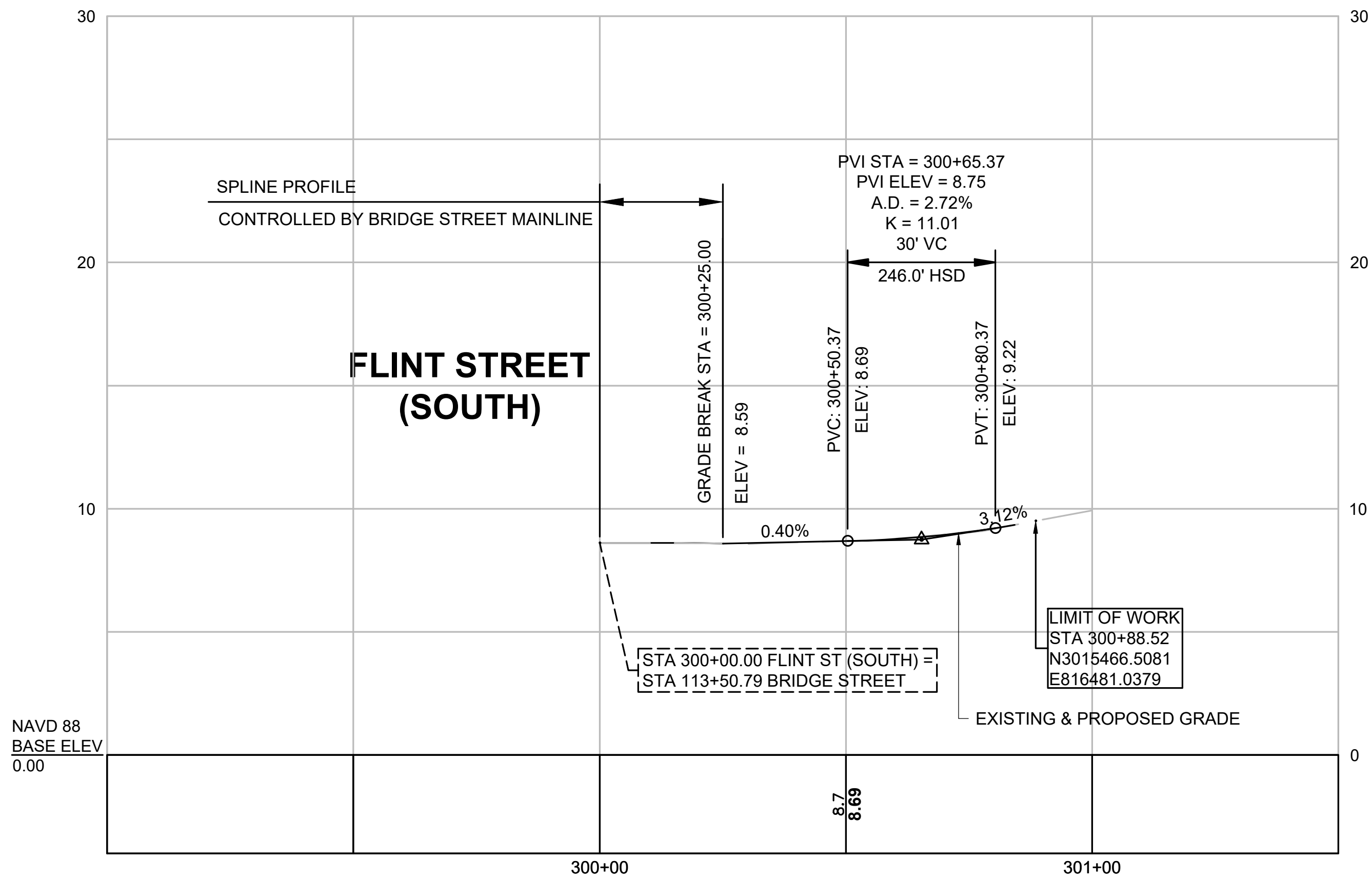
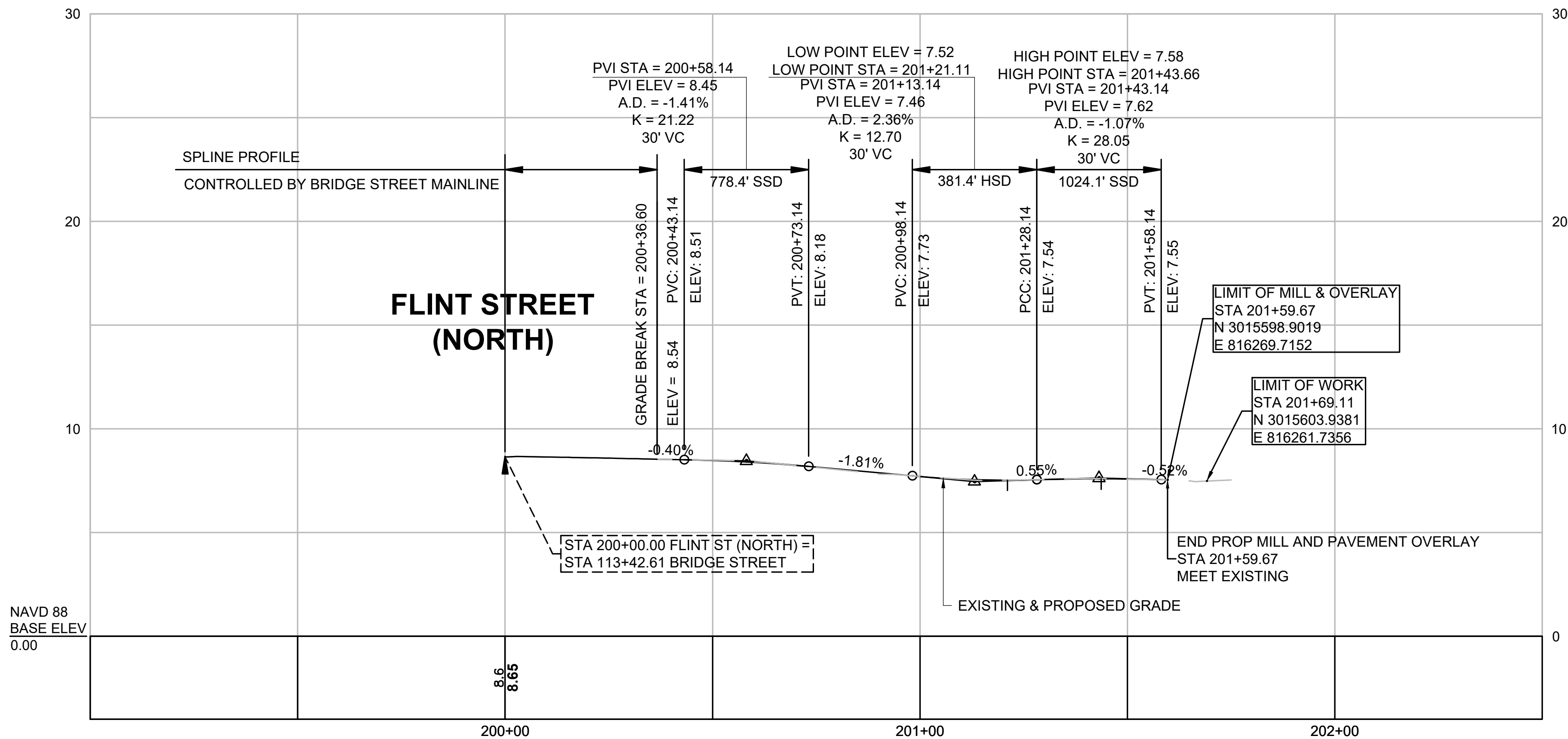
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SEE SHEET 2
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Notes

SEE SHEET 2



NOTES:
1. FOR CONSTRUCTION PLAN SEE SHEET 18.

Revision	By	Appd.	YY.MM.DD
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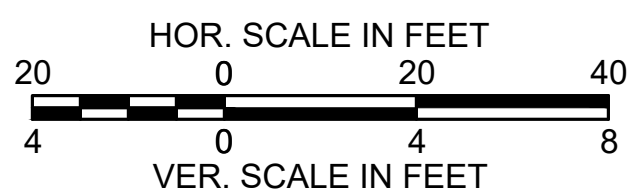
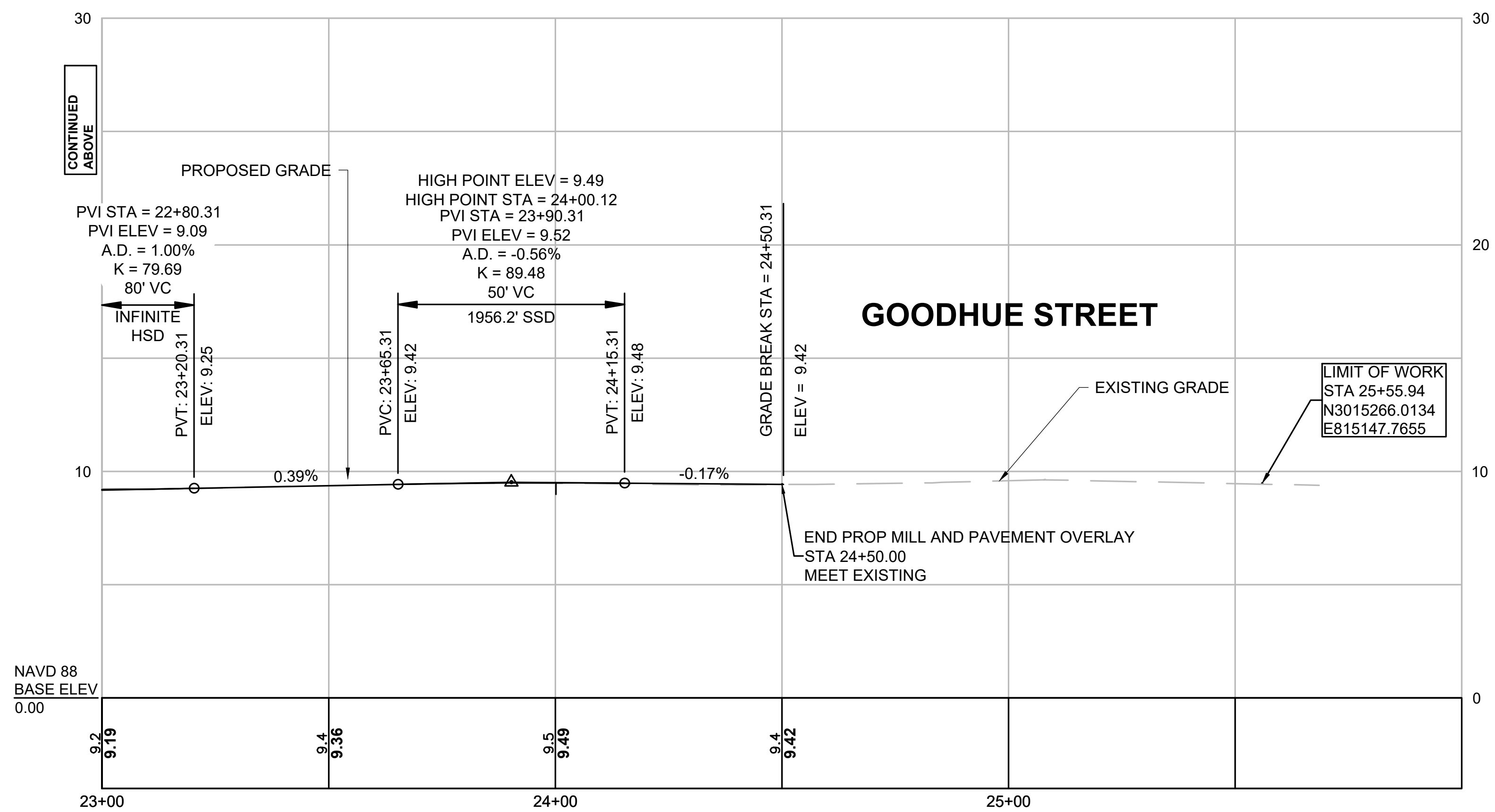
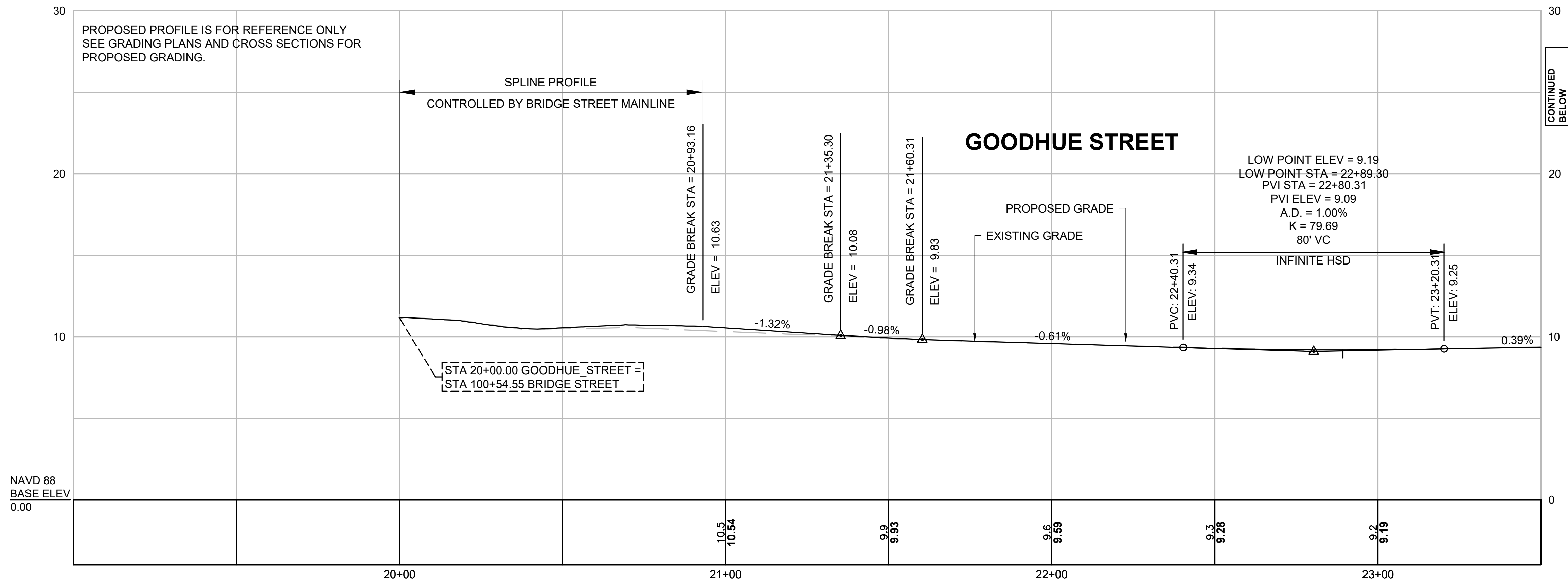
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BRIDGE STREET RECONSTRUCTION PROJECT

Title
PROFILE - FLINT STREET

179410455	H:1" = 20' V:1"=4'
Project No.	Scale
	21 of 113
Drawing No.	Sheet
	Revision



NOTES:
1. FOR CONSTRUCTION PLAN SEE SHEET 14, AND 15.

Revision	By	Appd.	YY.MM.DD
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Issued	By	Appd.	YY.MM.DD
File Name:	dwg_profiles.dwg	Dwn.	Chkd. Dsgn. YY.MM.DD

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CITY OF SALEM
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BRIDGE STREET RECONSTRUCTION PROJECT

Title
PROFILE - GOODHUE STREET

179410455	H:1" = 20' V:1"=4'
Project No.	Scale
	22 of 113
Drawing No.	Sheet Revision



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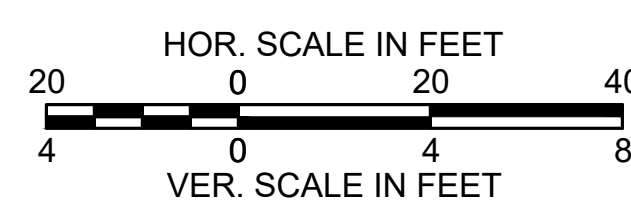
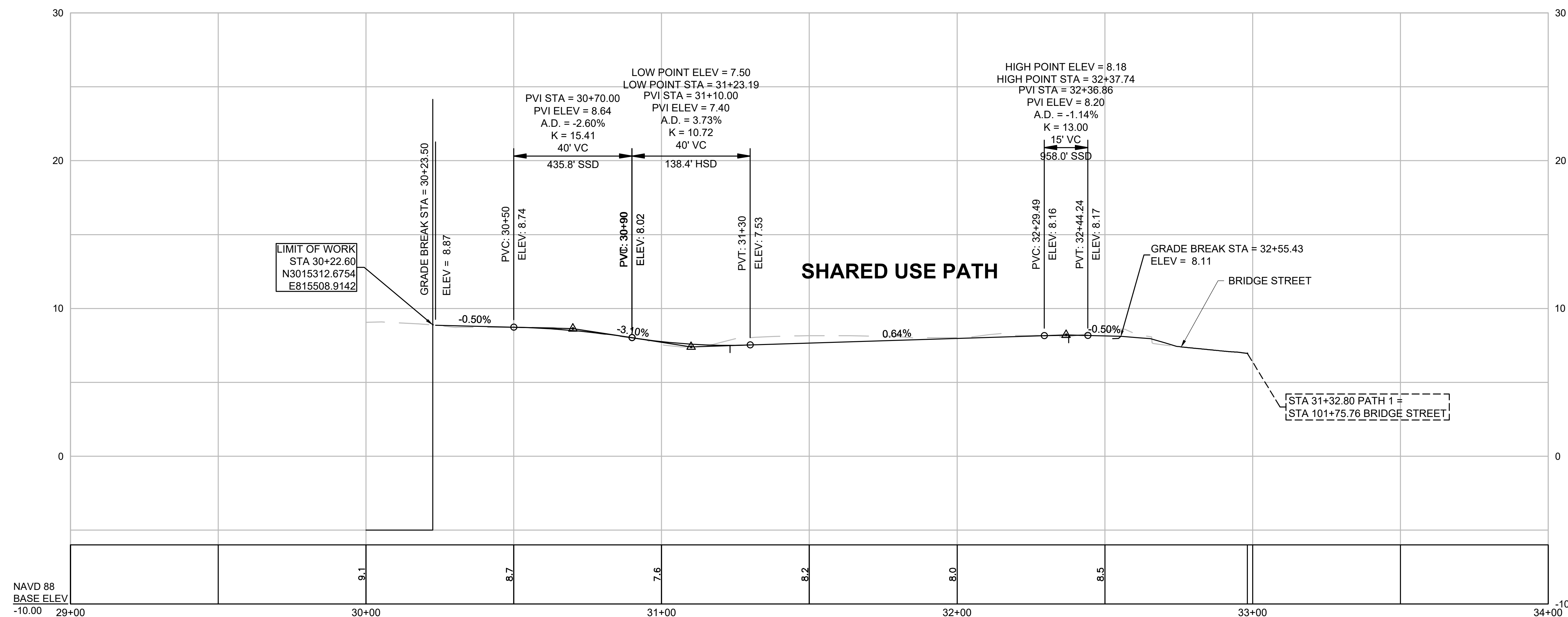
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		Dwn	Chkd	Dsgn	YY.MM.DD

BRIDGE STREET RECONSTRUCTION PROJECT	
Title	PROFILE - PATH

179410455	H:1" = 20' V:1"=4'	
Project No.	Scale	
	23 of 113	
Drawing No.	Sheet	Revision



NOTES:
1. FOR CONSTRUCTION PLAN SEE SHEET 16.

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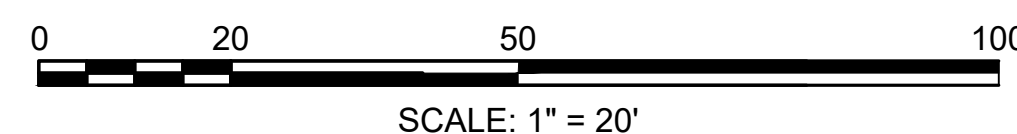
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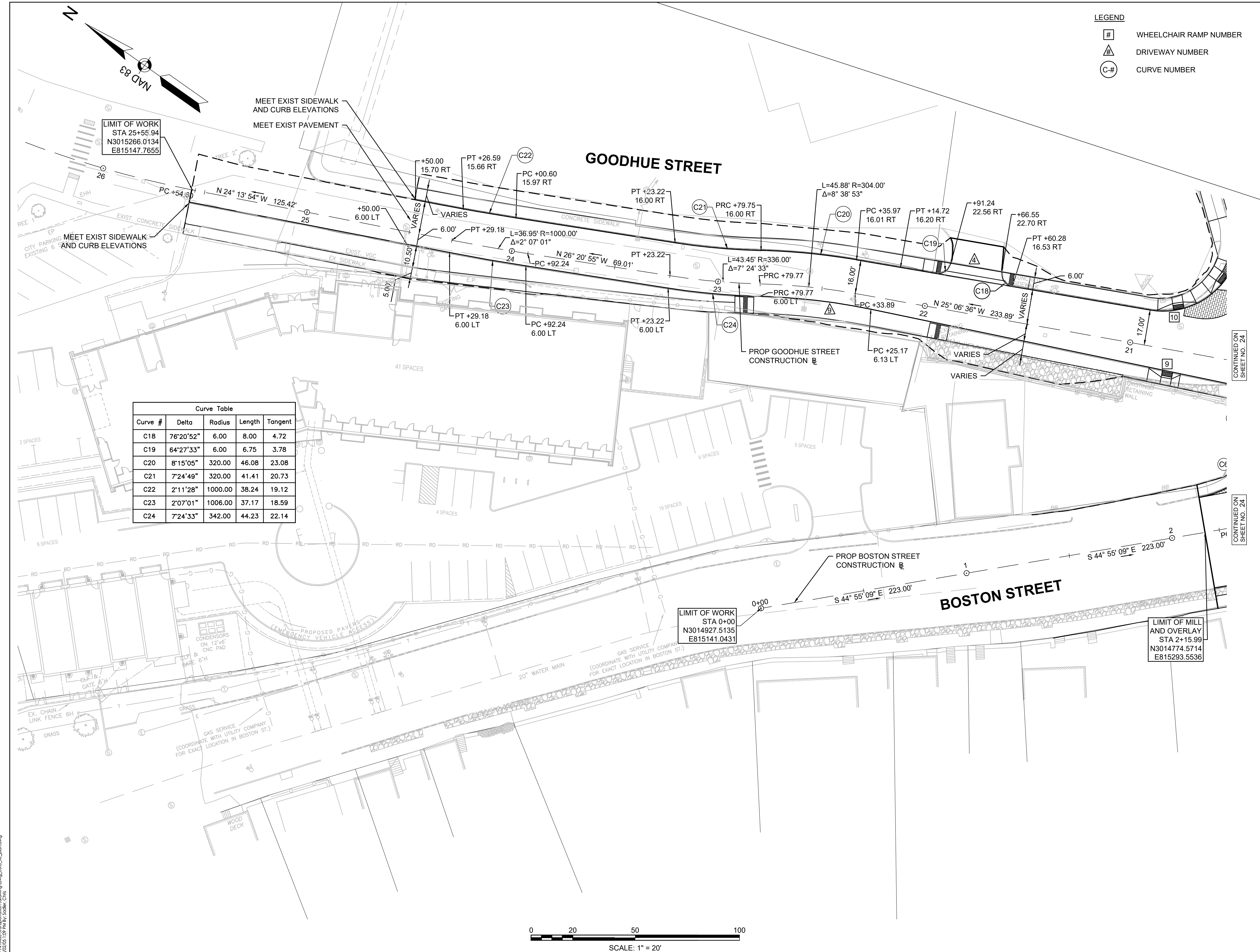
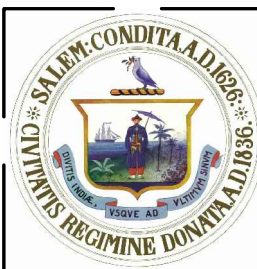
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C-3

CURVE NUMBER



Revision	By	Appd.	YY.MM.DD
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Issued	By	Appd.	YY.MM.DD
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	Dwn.	Chkd.	Dsgn.
			YY.MM.DD



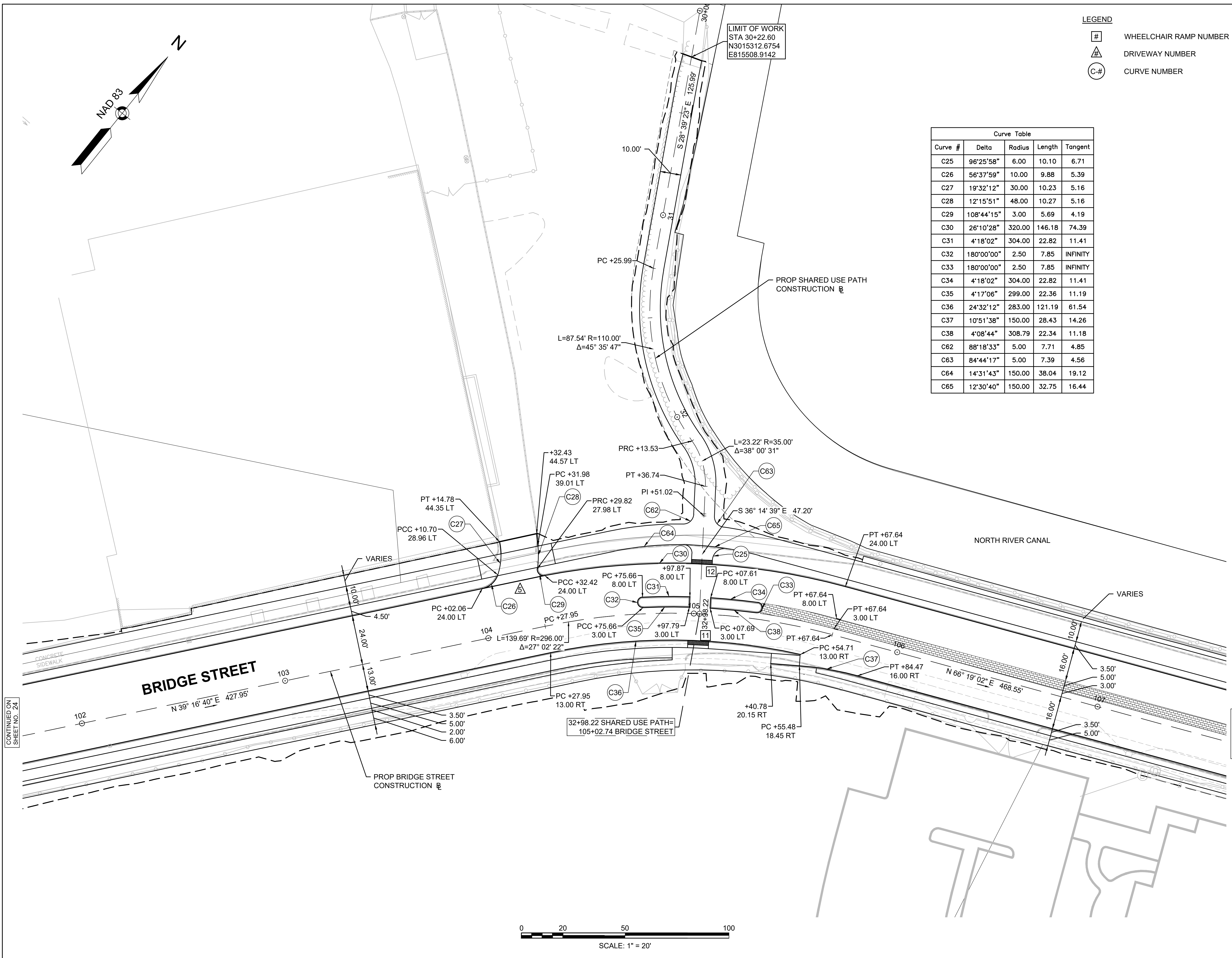
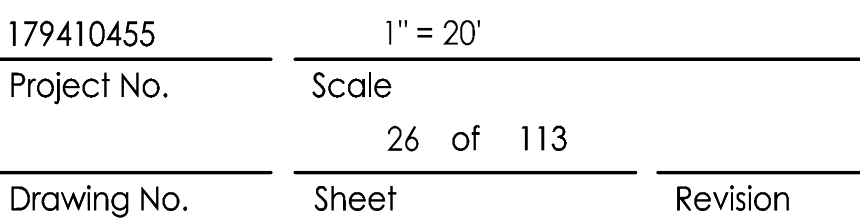
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SHEET NO. 24

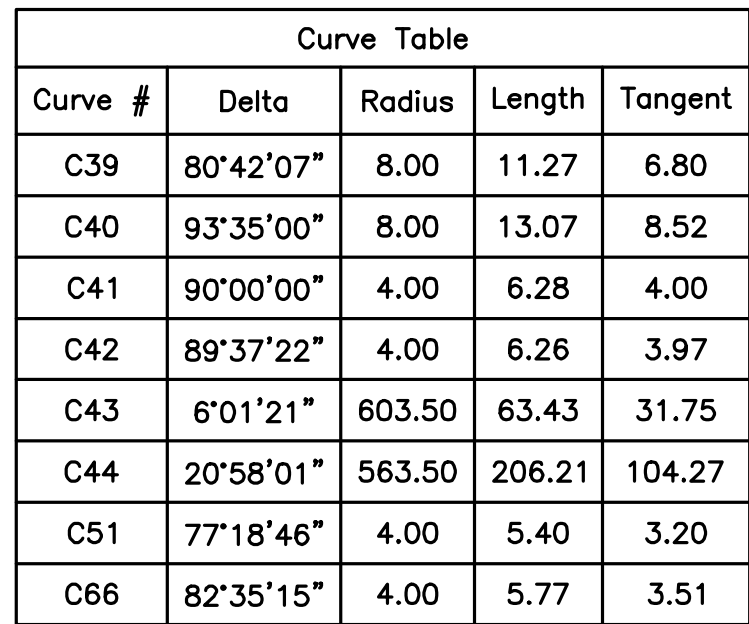
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SHEET NO. 24

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SHEET NO. 24

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SHEET NO. 24

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SHEET NO. 24





Curve Table				
Curve #	Delta	Radius	Length	Tangent
C39	80°42'07"	8.00	11.27	6.80
C40	93°35'00"	8.00	13.07	8.52
C41	90°00'00"	4.00	6.28	4.00
C42	89°37'22"	4.00	6.26	3.97
C43	60°11'21"	603.50	63.43	31.75
C44	20°58'01"	563.50	206.21	104.27
C51	77°18'46"	4.00	5.40	3.20
C66	82°35'15"	4.00	5.77	3.51

#	WHEELCHAIR RAMP NUMBER
#	DRIVEWAY NUMBER
C-#	CURVE NUMBER



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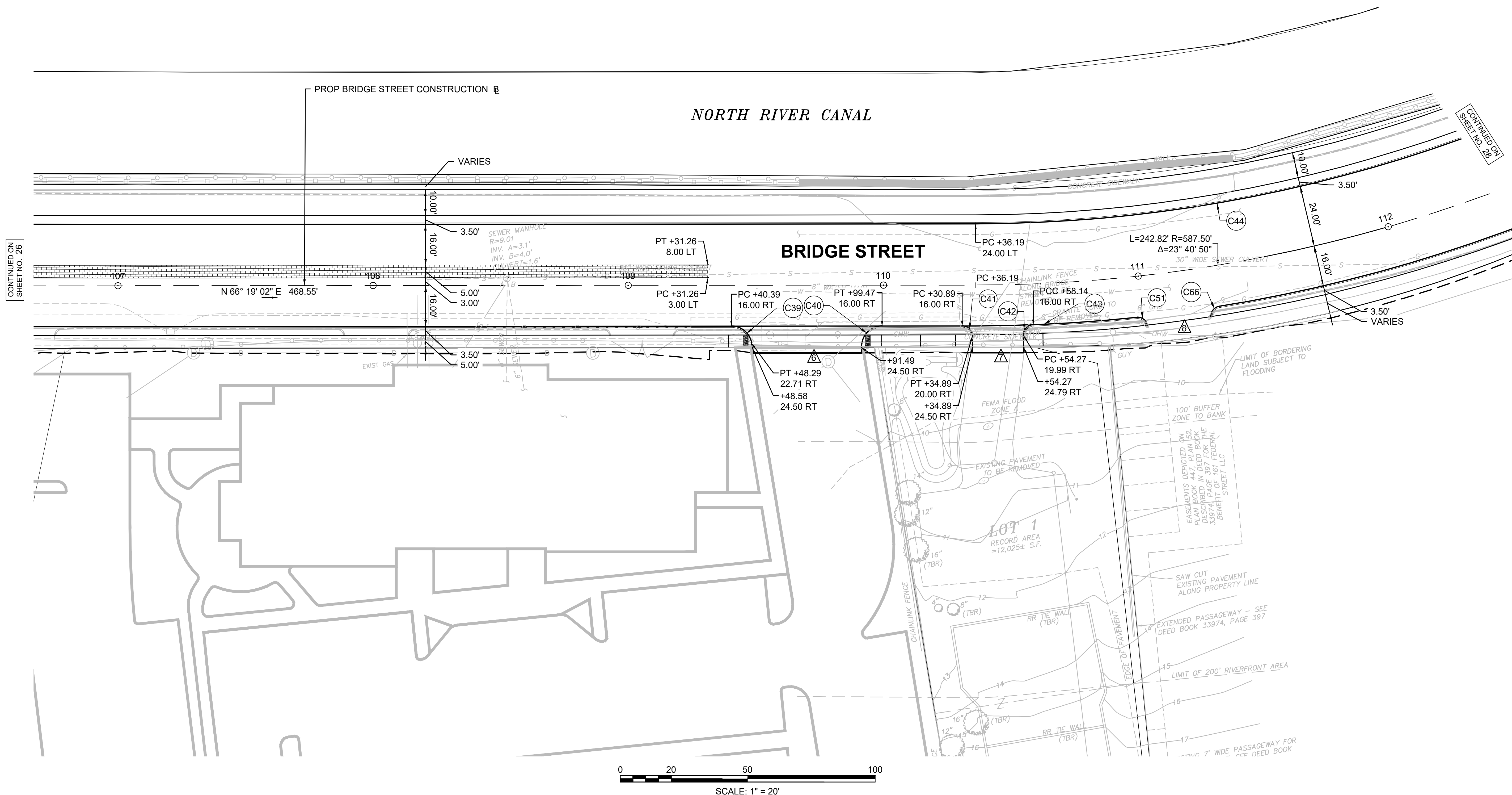
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BID SET			
Issued	By	Appd.	YY.MM.DD

File Name: dwg_curb_tie_plan.dwg				
	Dwn.	Chkd.	Dsgn.	YY.MM.DD

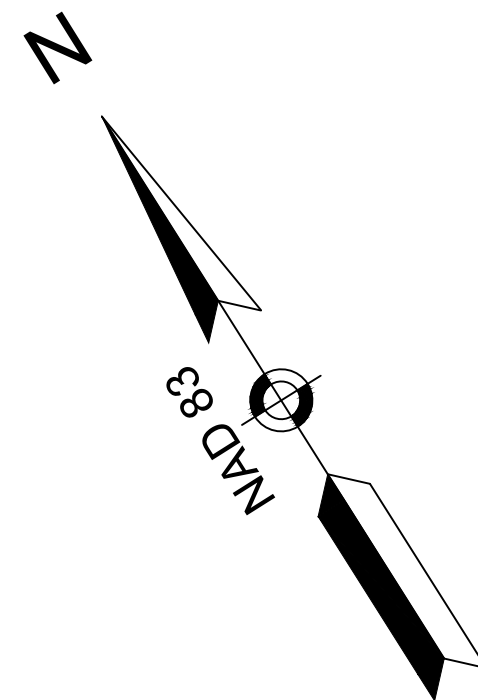


CITY OF SALEM
Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title
ALIGNMENT AND CURB TIE PLAN
PART 4 OF 5

179410455	1" = 20'	
Project No.	Scale	
	27	of 113
Drawing No.	Sheet	Revision



LEGEND

- #
- WHEELCHAIR RAMP NUMBER
- #
- DRIVEWAY NUMBER
- C-#
- CURVE NUMBER



65 NETWORK DRIVE, 2ND FLOOR
BURLINGTON, MA 01803
Tel. (781) 221-1000
www.stantec.com

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File Name: dwg_curb_tie_plan.dwg

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BRIDGE STREET RECONSTRUCTION PROJECT

Title

ALIGNMENT AND CURB TIE PLAN
PART 5 OF 5

179410455

Project No.

Drawing No.

1" = 20'

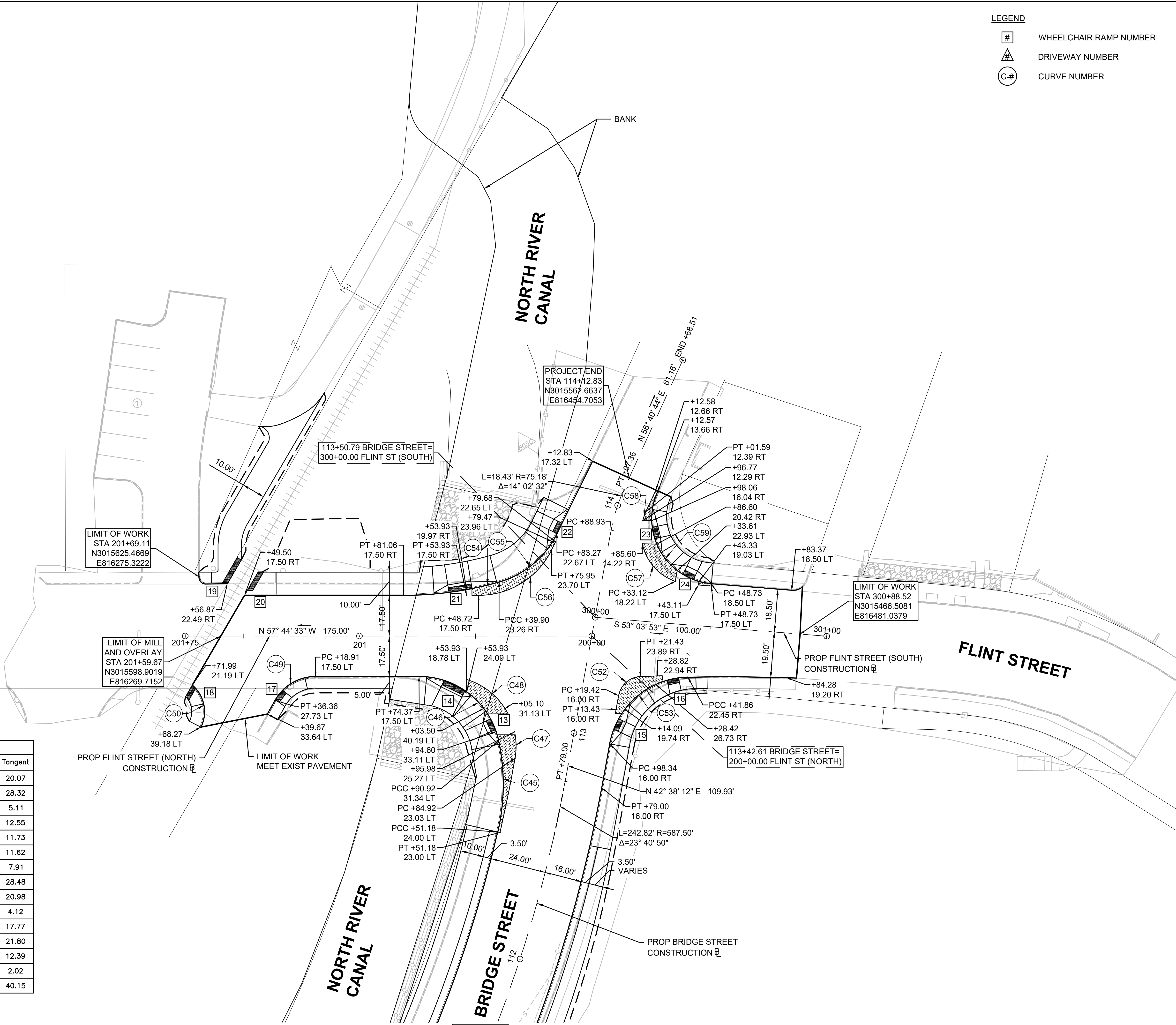
Scale

28 of 113

Sheet

Revision

Curve Table				
Curve #	Delta	Radius	Length	Tangent
C45	25°08'26"	90.00	39.49	20.07
C46	77°57'08"	35.00	47.62	28.32
C47	19°20'13"	30.00	10.12	5.11
C48	45°24'33"	30.00	23.78	12.55
C49	60°45'53"	20.00	21.21	11.73
C50	75°32'26"	15.00	19.78	11.62
C52	76°41'22"	10.00	13.38	7.91
C53	78°16'21"	35.00	47.81	28.48
C54	15°55'35"	150.00	41.69	20.98
C55	1°00'39"	466.70	8.23	4.12
C55	47°54'35"	40.00	33.45	17.77
C56	63°50'09"	35.00	39.00	21.80
C57	58°45'42"	22.00	22.56	12.39
C58	10°30'37"	22.00	4.04	2.02
C59	110°13'16"	28.00	53.86	40.15



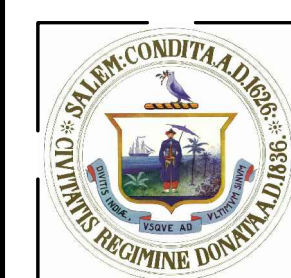
CONTINUED ON
SHEET NO. 27

SCALE: 1" = 20'

Revision	By	Appd.	YY.MM.DD
BID SET			
Issued	By	Appd.	YY.MM.DD

File Name: dwg_grading_plan.dwg			
	Dwn.	Chkd.	Dsgn.
			YY.MM.DD

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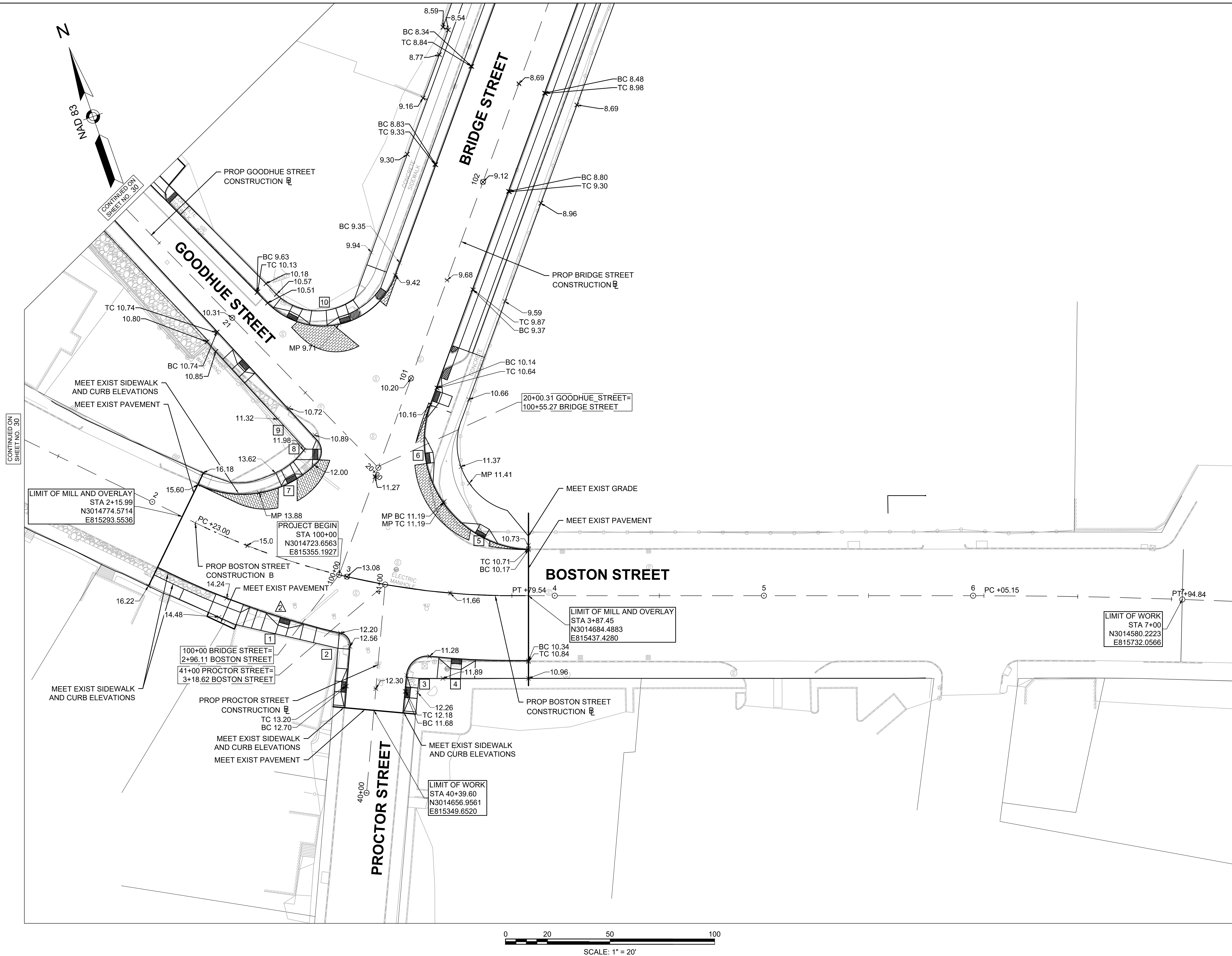


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BRIDGE STREET RECONSTRUCTION PROJECT

Title
GRADING PLAN
PART 1 OF 5

179410455	1" = 20'
Project No.	Scale
	29 of 113
Drawing No.	Sheet
	Revision





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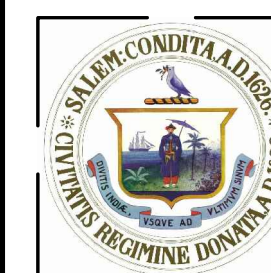
Consultants

Legend
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Notes
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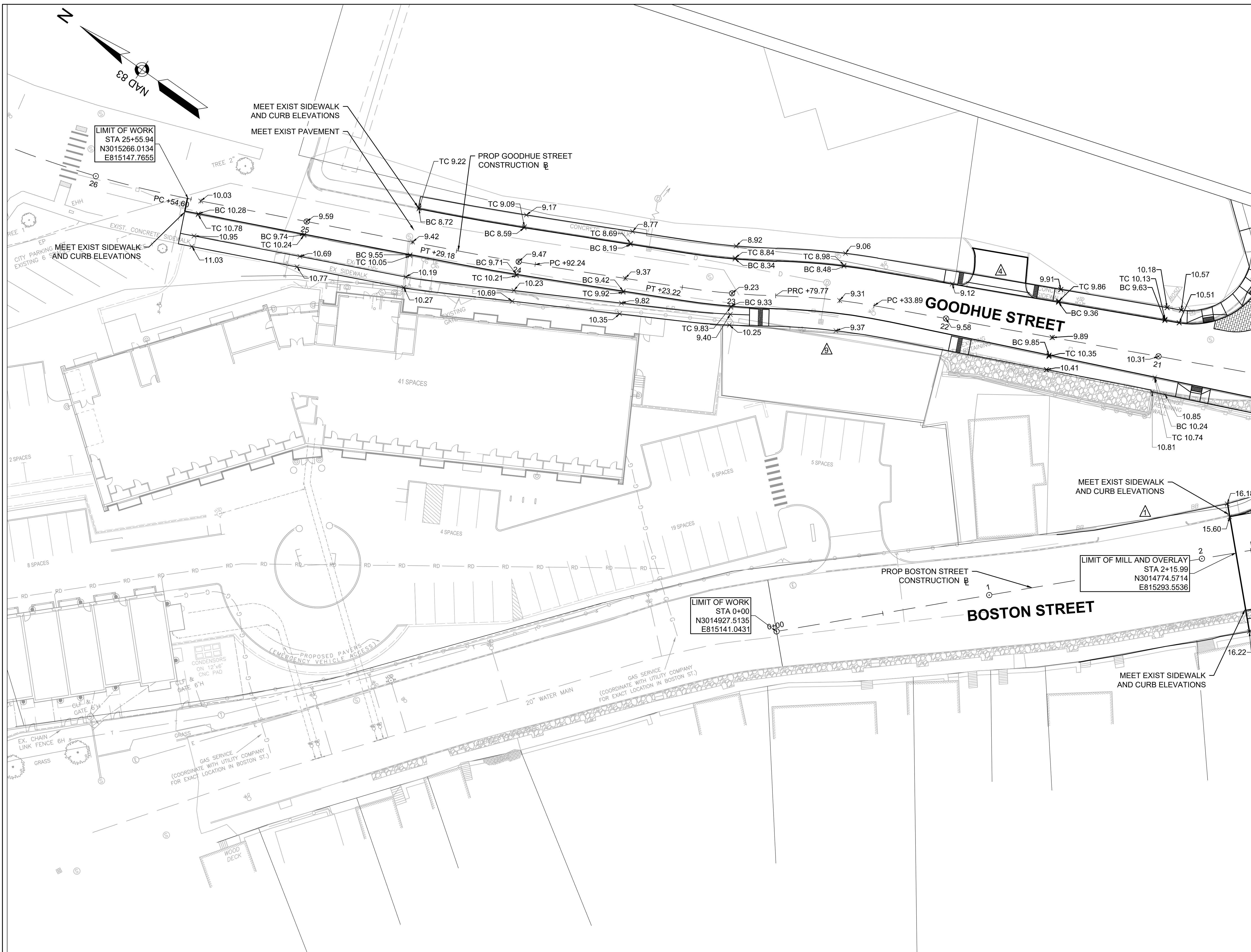


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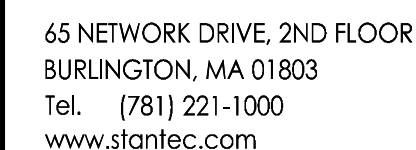
BRIDGE STREET RECONSTRUCTION PROJECT

Title
GRADING PLAN
PART 2 OF 5

179410455	1" = 20'	
Project No.	Scale	
	30 of 113	
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2020/02/05 1:11 PM By: Sadtler, Chris



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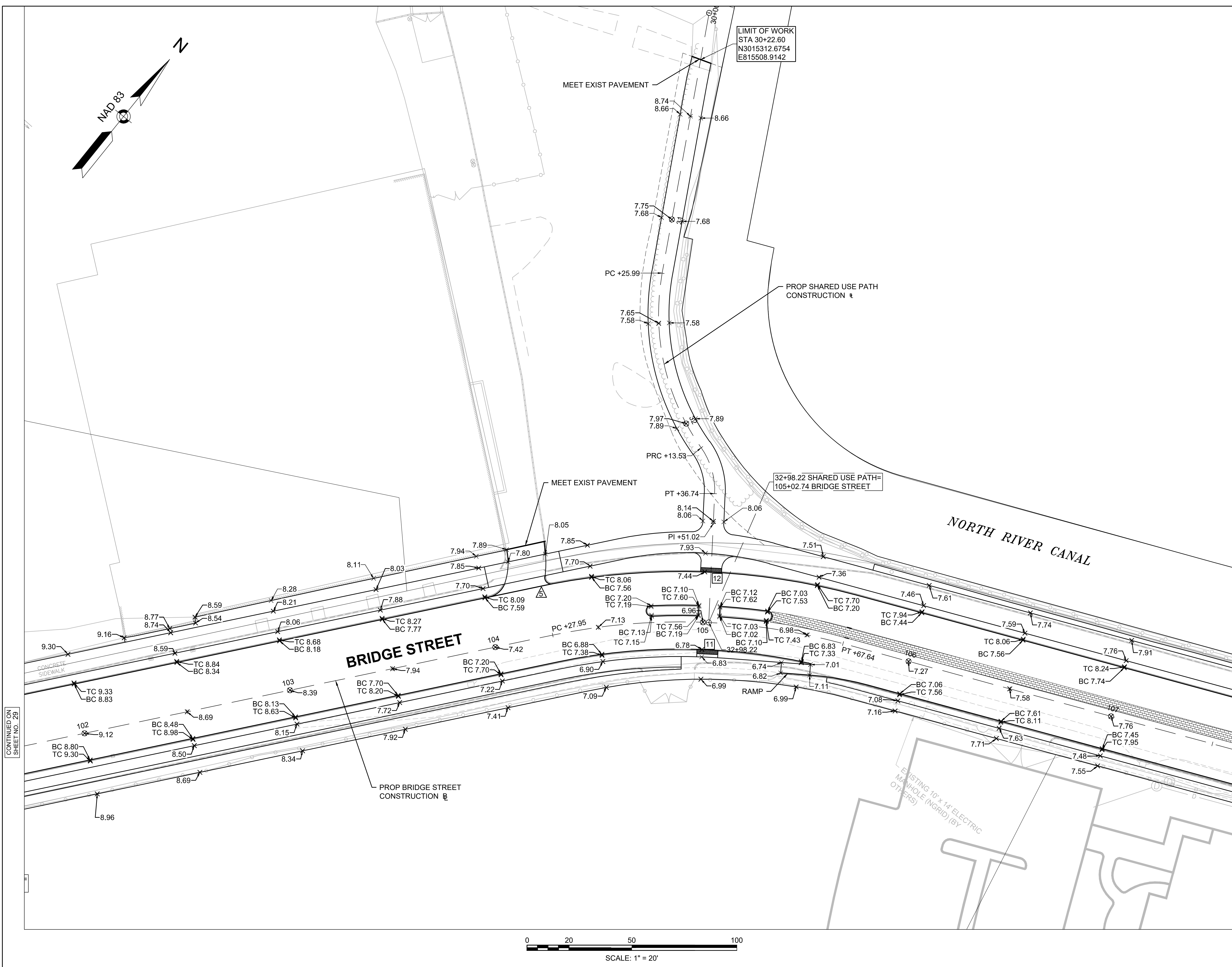
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Title

GRADING PLAN
PART 3 OF 5

179410455	1" = 20'	
Project No.	Scale	
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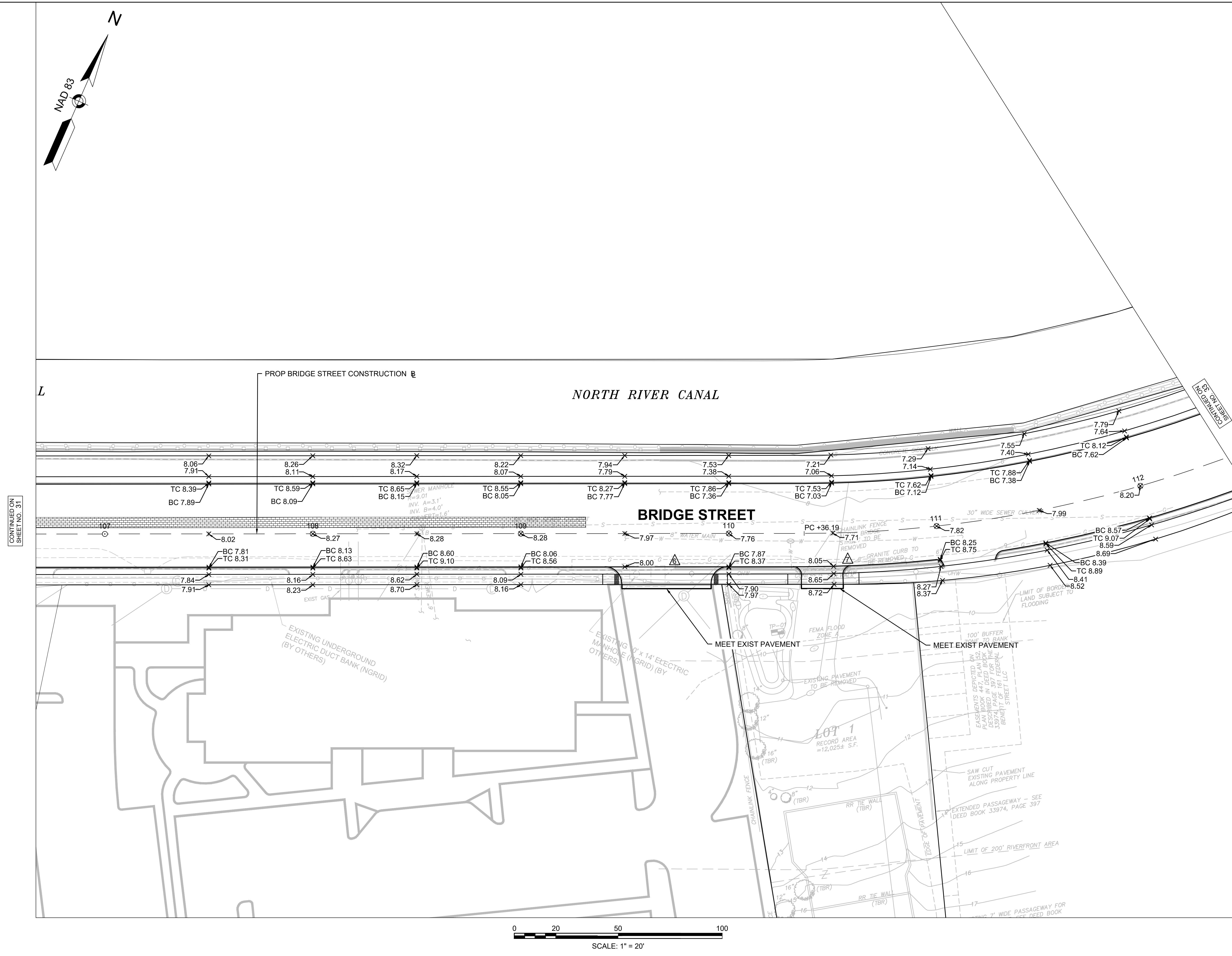
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Title
GRADING PLAN
PART 4 OF 5

170410455 1" - 30'

179410433	1 - 20
Project No.	Scale

Drawing No.	Sheet	Revision
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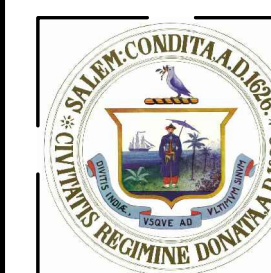
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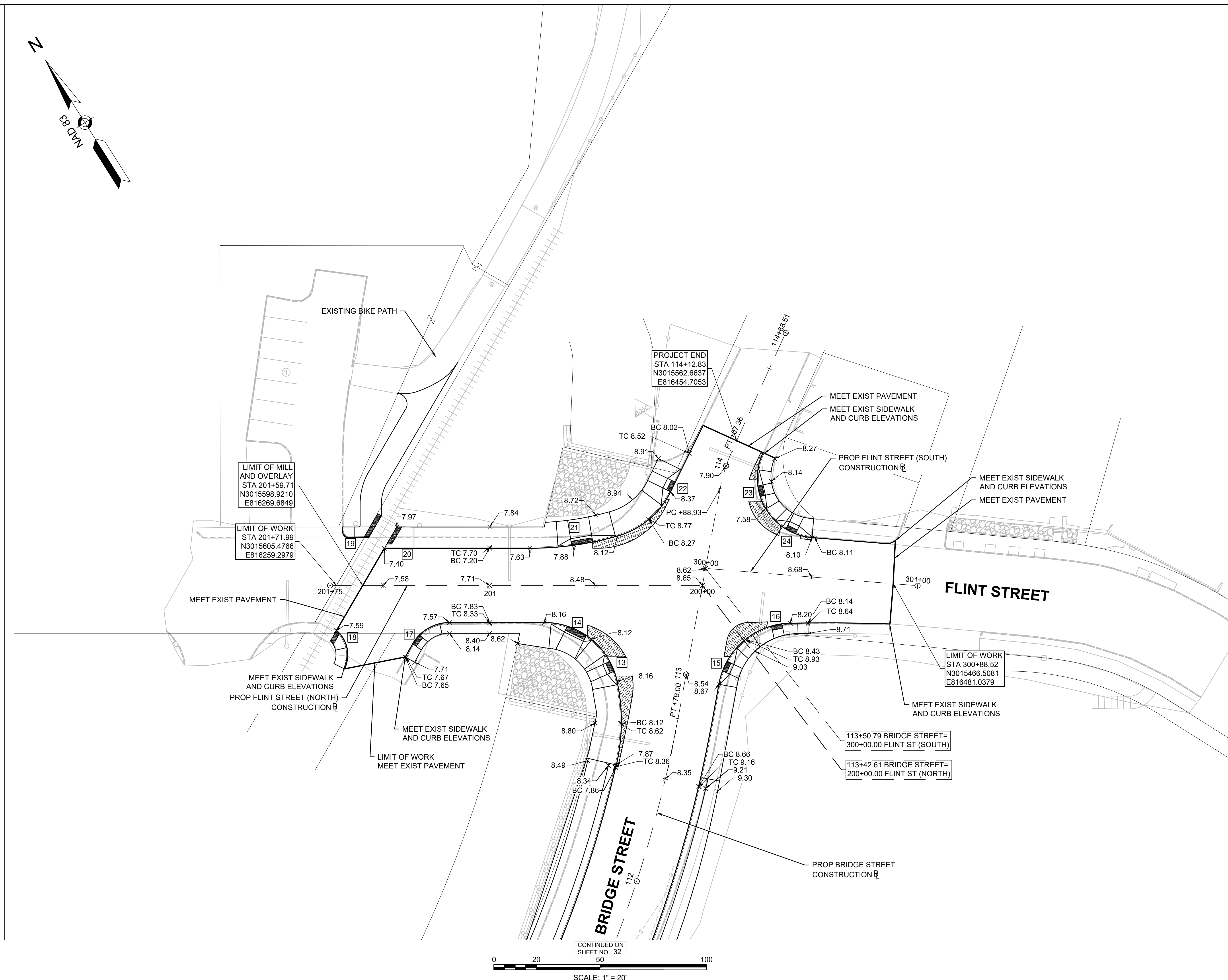
Title
GRADING PLAN
PART 5 OF 5

179410455 1" = 20'

Project No.	Scale
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Drawing No.	Sheet	Revision
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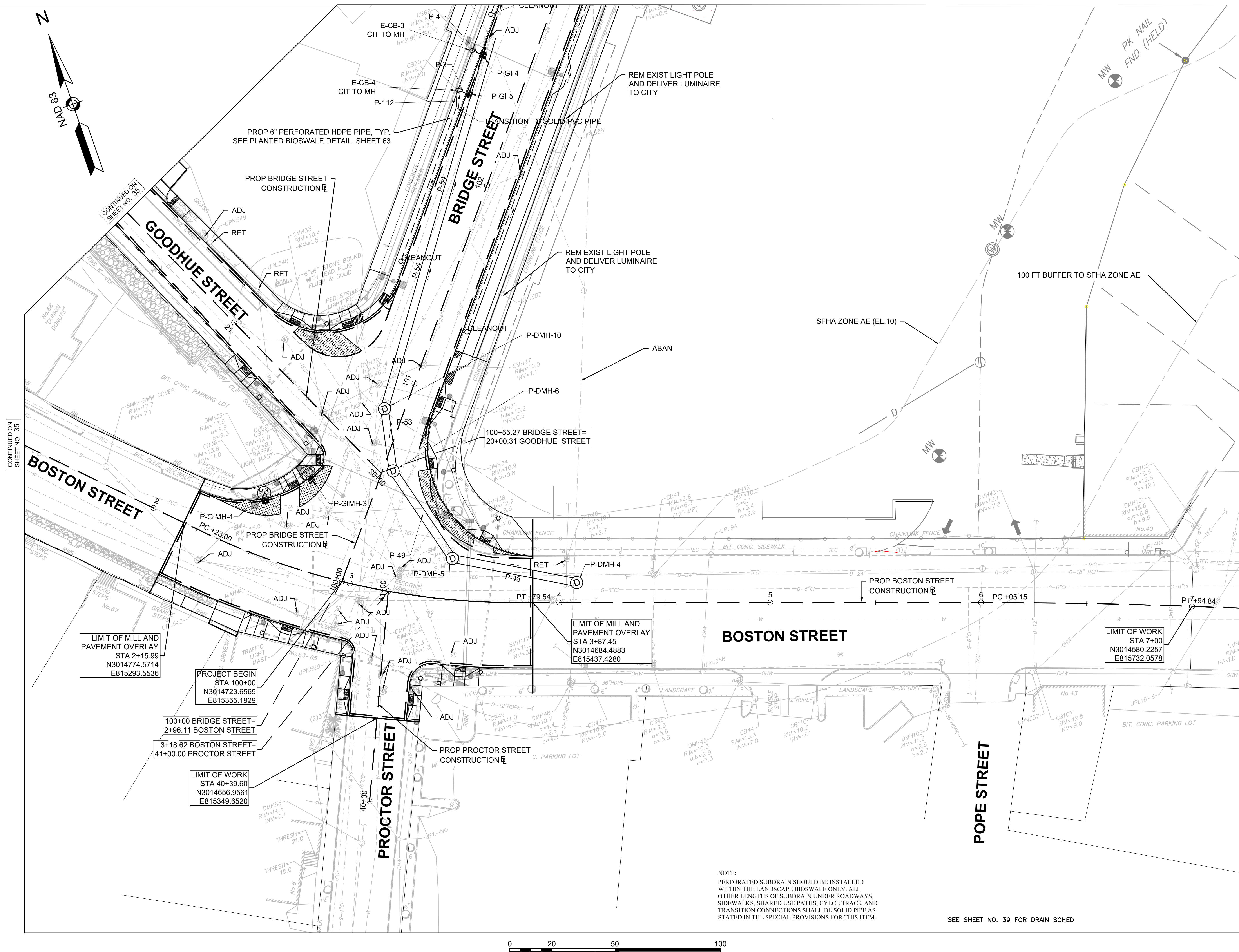


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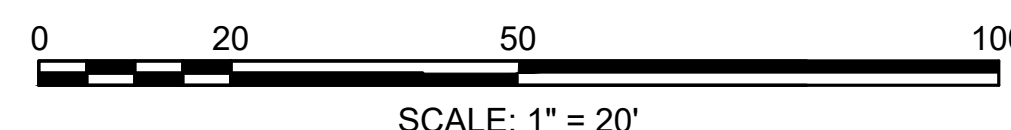
Title
DRAINAGE & UTILITY PLAN
PART 1 OF 5

179410455	1" = 20'	
Project No.	Scale	
	34 of 113	
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NOTE:
PERFORATED SUBDRAIN SHOULD BE INSTALLED
WITHIN THE LANDSCAPE BIOSWALE ONLY. ALL
OTHER LENGTHS OF SUBDRAIN UNDER ROADWAYS,
SIDEWALKS, SHARED USE PATHS, CYLCE TRACK AND
TRANSITION CONNECTIONS SHALL BE SOLID PIPE AS
STATED IN THE SPECIAL PROVISIONS FOR THIS ITEM

SEE SHEET NO. 39 FOR DRAIN SCHEDULE



SCALE: 1" = 20'

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Issued	By	Appd.	YY/MM/DD

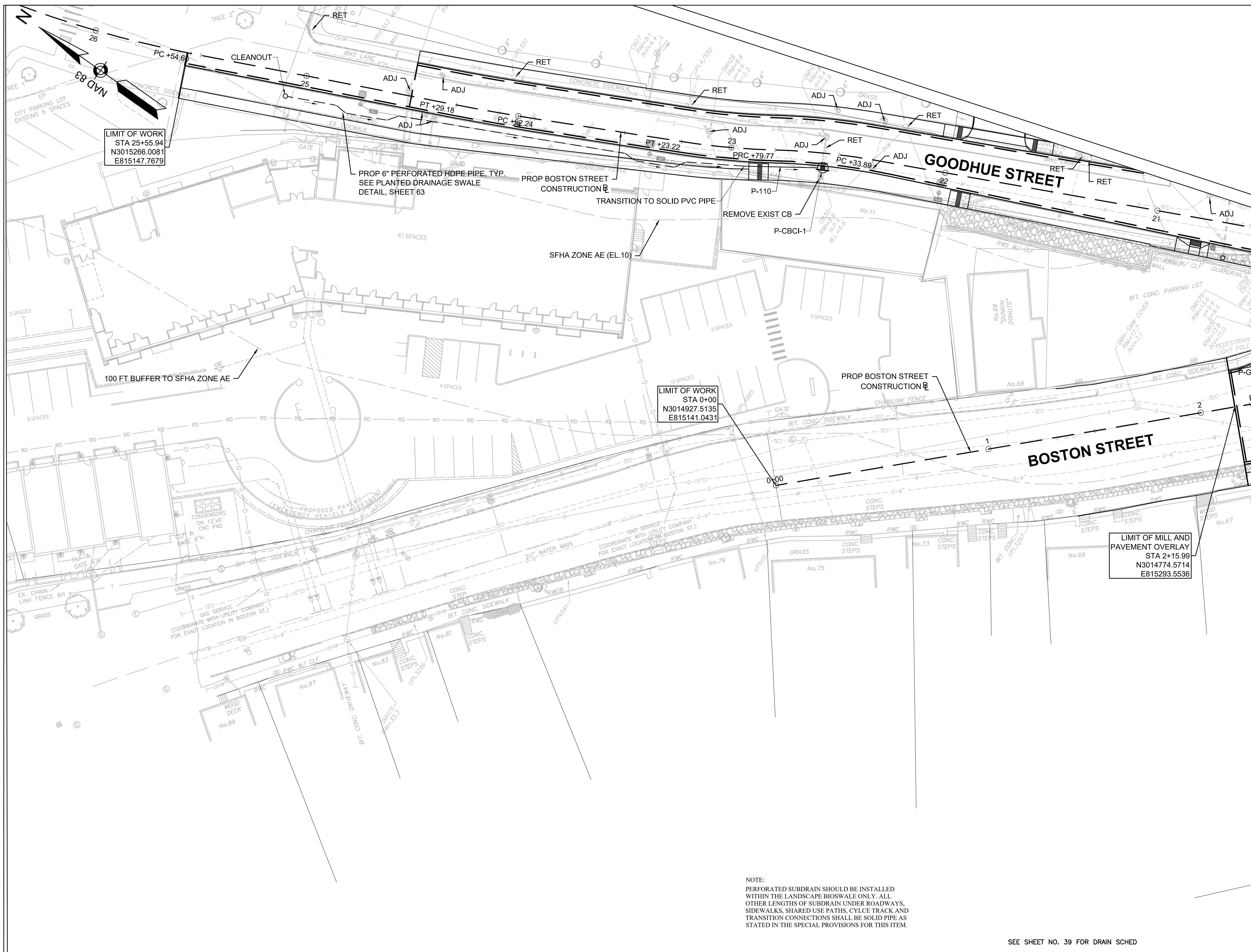
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BRIDGE STREET RECONSTRUCTION PROJECT

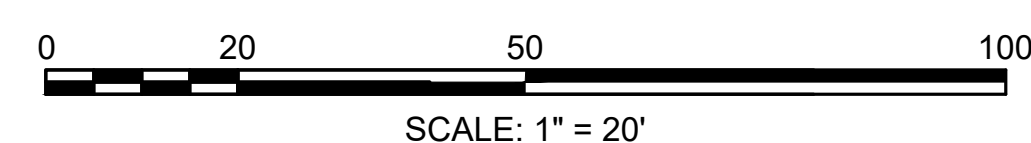
Title
DRAINAGE & UTILITY PLAN
PART 2 OF 5

179410455	1" = 20'	
Project No.	Scale	
	35 of 113	
Drawing No.	Sheet	Revision



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SIDEWALKS, SHARED USE PATHS, CYLCE TRACK AND
TRANSITION CONNECTIONS SHALL BE SOLID PIPE AS
STATED IN THE SPECIAL PROVISIONS FOR THIS ITEM.

SEE SHEET NO. 39 FOR DRAIN SCHED



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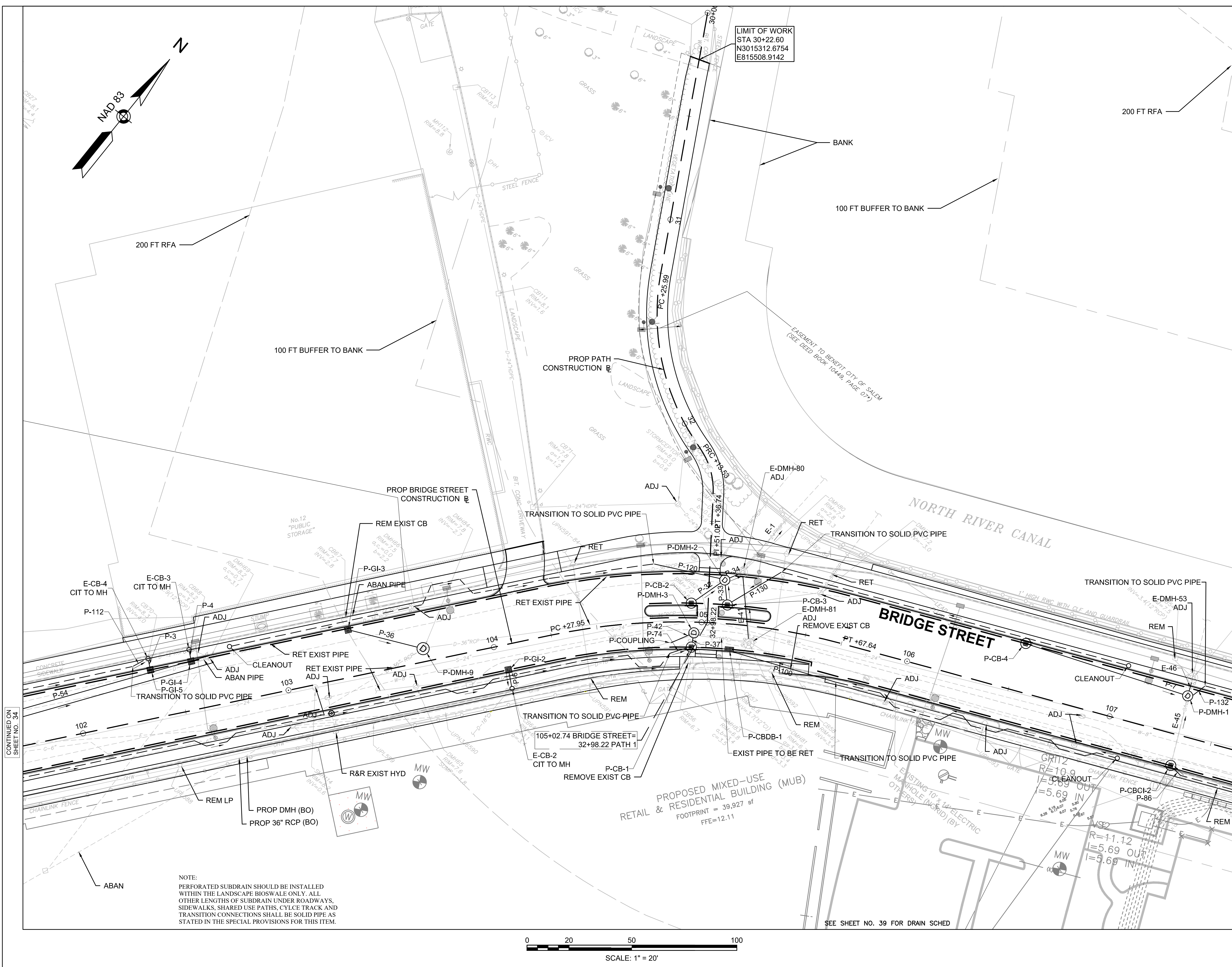
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BRIDGE STREET RECONSTRUCTION PROJECT

Title
DRAINAGE & UTILITY PLAN
PART 3 OF 5

179410455	1" = 20'	
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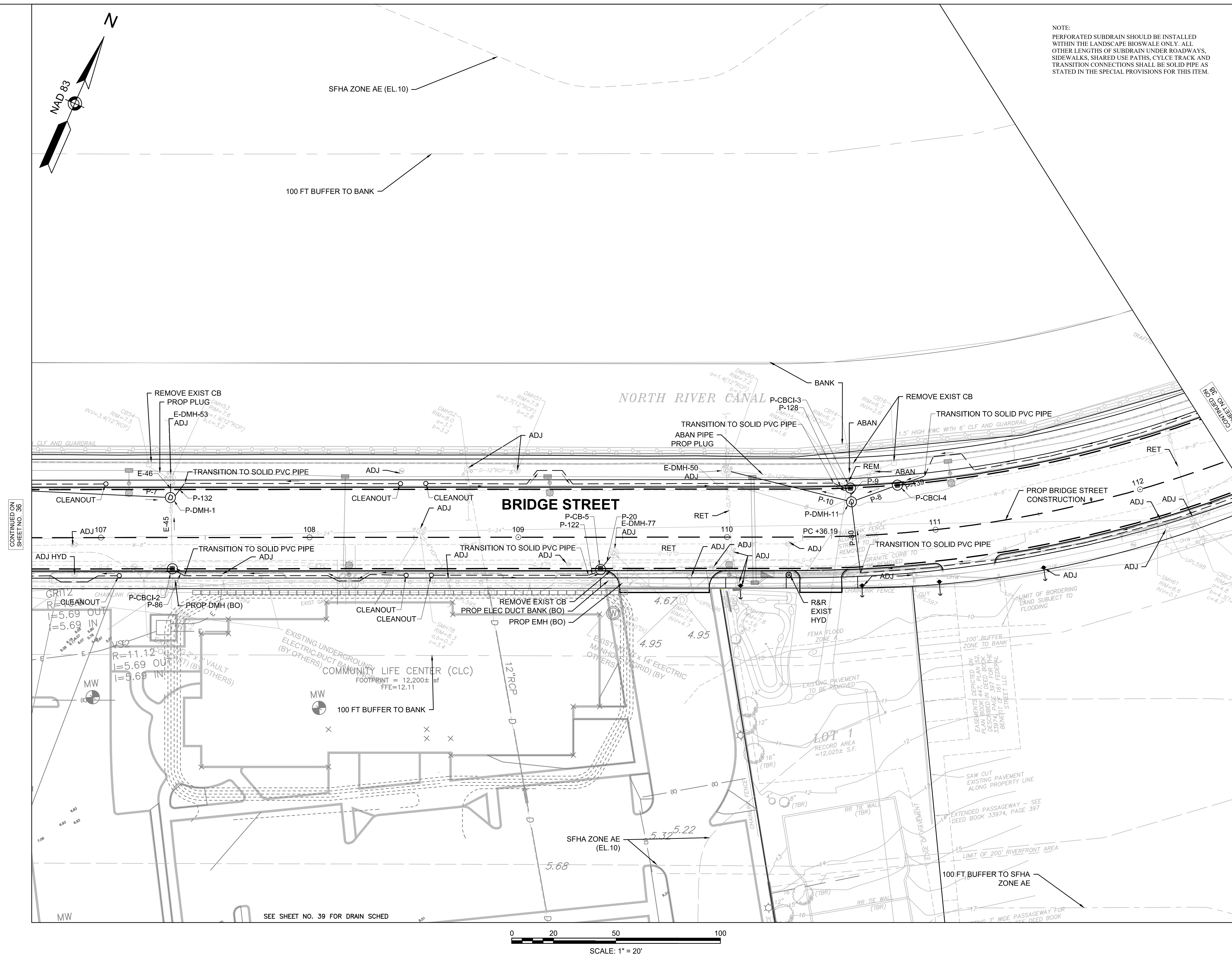


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BRIDGE STREET RECONSTRUCTION PROJECT

Title
DRAINAGE & UTILITY PLAN
PART 4 OF 5

179410455	1" = 20'	
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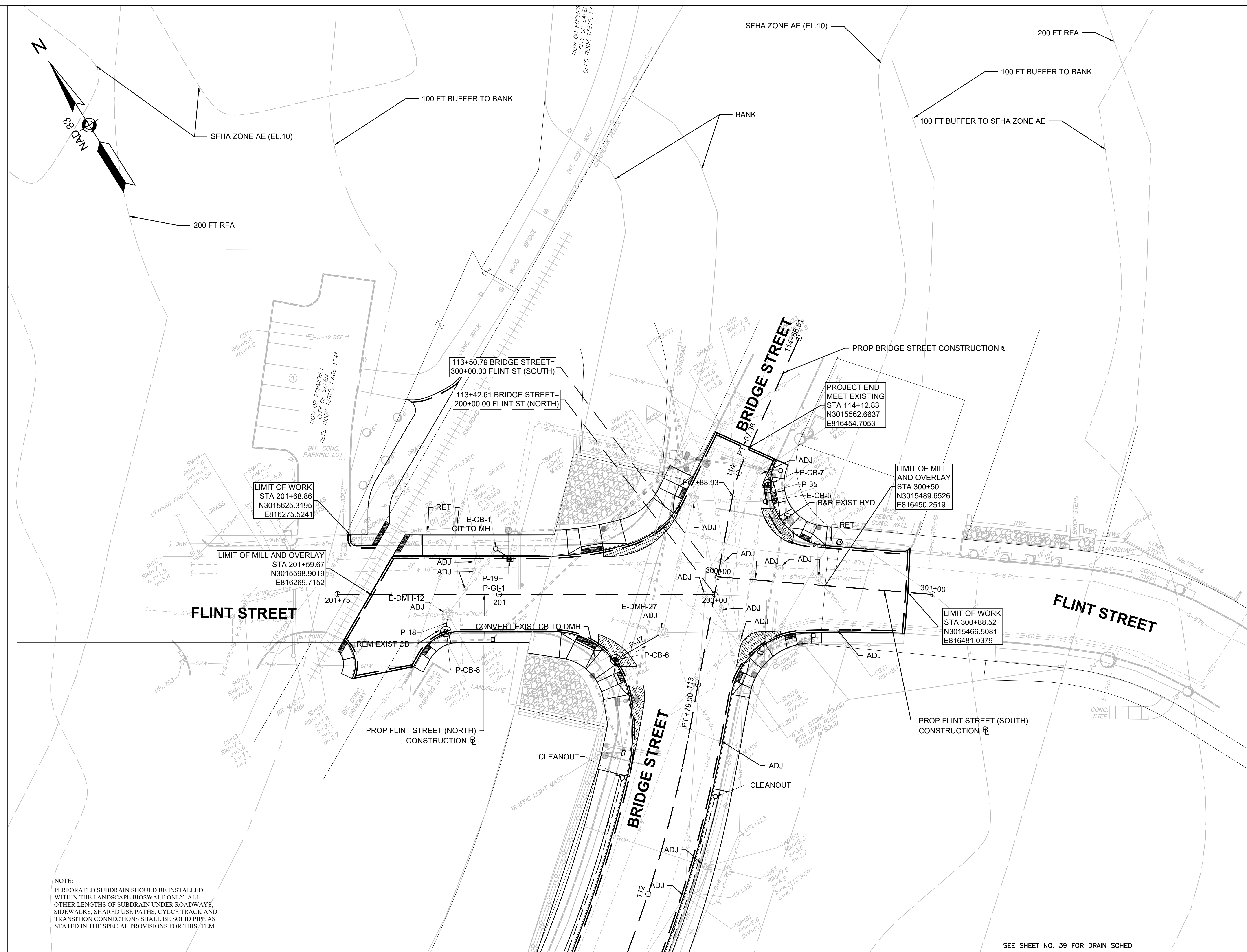
BRIDGE STREET RECONSTRUCTION PROJECT

Title

DRAINAGE & UTILITY PLAN

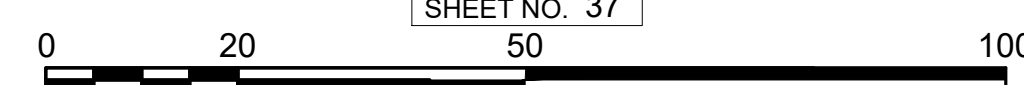
PART 5 OF 5

179410455	1" = 20'	
Project No.	Scale	
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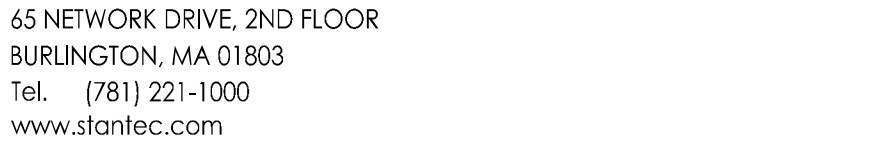
SEE SHEET NO. 39 FOR DRAIN SCHED

CONTINUED ON
SHEET NO. 37



SCALE: 1" = 20'

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Notes
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DRAINAGE PIPE & STRUCTURE SCHEDULE

PIPE TABLE				
PIPE	PIPE LOCATION	SIZE & TYPE	LENGTH	SLOPE
E-41	E-DMH-81 TO E-DMH-80 STA 105+24 TO 105+17	18" REINFORCED CONCRETE	30 LF	0.006
E-45	P-CBCI-2 TO P-DMH-1 STA 107+34 TO 107+33	12" REINFORCED CONCRETE	30 LF	0.037
E-46	P-DMH-1 TO E-DMH-53 STA 107+33 TO 107+33	12" REINFORCED CONCRETE	5 LF	-0.017
P-3	P-GI-5 TO E-CB-4 STA 102+38 TO 102+38	12" REINFORCED CONCRETE	2 LF	0.019
P-4	P-GI-4 TO E-CB-3 STA 102+57 TO 102+58	12" REINFORCED CONCRETE	2 LF	0.027
P-6	P-GI-2 TO E-CB-2 STA 104+05 TO 104+04	12" REINFORCED CONCRETE	5 LF	0.024
P-7	P-CB-4 TO P-DMH-1 STA 106+52 TO 107+33	12" REINFORCED CONCRETE	77 LF	0.006
P-8	P-CBCI-4 TO P-DMH-11 STA 110+84 TO 110+60	12" REINFORCED CONCRETE	19 LF	0.006
P-9	P-CBCI-3 TO P-DMH-11 STA 110+60 TO 110+60	12" REINFORCED CONCRETE	3 LF	0.006
P-10	P-DMH-11 TO E-DMH-50 STA 110+60 TO 110+00	12" REINFORCED CONCRETE	58 LF	0.005
P-18	P-CB-8 TO E-DMH-12 STA 201+25 TO 201+24	12" REINFORCED CONCRETE	4 LF	0.006
P-19	P-GI-1 TO E-CB-1 STA 200+95 TO 201+02	12" REINFORCED CONCRETE	4 LF	0.020
P-20	P-CB-5 TO E-DMH-77 STA 109+39 TO 109+46	12" REINFORCED CONCRETE	6 LF	0.010
P-32	P-CB-2 TO P-DMH-2 STA 104+95 TO 105+10	12" REINFORCED CONCRETE	15 LF	0.005
P-33	P-CB-3 TO P-DMH-2 STA 105+12 TO 105+10	12" REINFORCED CONCRETE	8 LF	0.003
P-34	P-DMH-2 TO E-DMH-80 STA 105+10 TO 105+17	12" REINFORCED CONCRETE	5 LF	0.038
P-35	P-CB-7 TO E-CB-5 STA 113+99 TO 113+89	12" REINFORCED CONCRETE	4 LF	0.010
P-36	P-GI-3 TO P-DMH-9 STA 103+34 TO 103+67	12" REINFORCED CONCRETE	33 LF	0.020
P-37	P-CB-1 TO P-DMH-3 STA 104+95 TO 104+96	12" REINFORCED CONCRETE	3 LF	0.005
P-38	P-DMH-3 TO E-DMH-81 STA 104+96 TO 105+24	18" REINFORCED CONCRETE	23 LF	0.011
P-39	P-CBDB-1 TO P-CB-1 STA 105+14 TO 104+95	12" REINFORCED CONCRETE	14 LF	0.005
P-42	P-COUPPLING TO P-DMH-3 STA 104+85 TO 104+96	12" REINFORCED CONCRETE	10 LF	0.000
P-47	P-CB-6 TO E-DMH-27 STA 113+04 TO 113+21	12" REINFORCED CONCRETE	21 LF	0.020
P-48	P-DMH-4 TO P-DMH-5 STA 4+08 TO 3+48	36" REINFORCED CONCRETE	55 LF	0.001
P-49	P-DMH-5 TO P-DMH-6 STA 3+48 TO 3+09	36" REINFORCED CONCRETE	45 LF	0.001
P-53	P-DMH-6 TO P-DMH-10 STA 100+58 TO 100+84	36" REINFORCED CONCRETE	25 LF	0.001
P-54	P-DMH-10 TO STA 100+84 TO 102+67	36" REINFORCED CONCRETE	181 LF	0.001

179410455	NOT TO SCALE	
Project No.	Scale	
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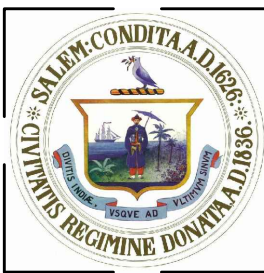
Notes

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Revision	By	Appd.	YY.MM.DD
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File Name: dwg_traffic_legend_abbrev.dwg	Dwn.	Chkd.	Dsgn.	YY.MM.DD
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CITY OF SALEM

Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title

TRAFFIC LEGEND & ABBREVIATIONS

179410455	1" = 20'
Project No.	Scale
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Drawing No.	Sheet
	Revision

○	ELECTRIC MANHOLE (NUMBER AS NOTED)
○	TELEPHONE MANHOLE (NUMBER AS NOTED)
SP	SIGNAL POST
JU	JOINT USE POLE
MA	MAST ARM
CAB	CONTROLLER CABINET
HH	ELECTRIC HAND HOLE
PB	ELECTRIC PULL BOX (12"x12") SD2.031
LP	LIGHT POST
⊗	PULL BOX LOCATION

EXISTING SIGNAL EQUIPMENT

◀	VEHICULAR SIGNAL HEAD
◻	PEDESTRIAN SIGNAL HEAD
○	SIGNAL POST AND BASE
—○	MAST ARM AND SHAFT AND BASE
⊗	PEDESTRIAN PUSH BUTTON
↑	OPTICAL EMERGENCY VEHICLE PRE-EMPTION DETECTOR
⊠	CONTROL CABINET, GROUND MOUNTED
⊠	CONTROL CABINET, POST MOUNTED
◻	PULL BOX
—◻	BTD TRAFFIC MONITORING CAMERA

PROPOSED SIGNAL EQUIPMENT

◀	VEHICULAR SIGNAL HEAD
◀	VEHICULAR SIGNAL HEAD, OPTICALLY PROGRAMMED
◻	PEDESTRIAN SIGNAL HEAD
●	SIGNAL POST AND BASE
—●	MAST ARM AND SHAFT AND BASE
⊗	ACCESSIBLE PEDESTRIAN PUSH BUTTON, SIGN AND SADDLE
*	OPTICAL EMERGENCY VEHICLE PRE-EMPTION DETECTOR
◦	OPTICAL PRE-EMPTION CONFIRMATION STROBE
⊠	CONTROL CABINET, GROUND MOUNTED
■	PULL BOX (12"x12")
■	PULL BOX (12"x24")
—	PULL BOX - LOOP DETECTOR (12"x24")
—	BTD TRAFFIC MONITORING CAMERA (360°)

-----	DETECTOR LEAD
— — — —	EXISTING CONDUIT
== == ==	PROPOSED CONDUIT (SINGLE BANK)
==X==X==	PROPOSED CONDUIT (ENCASED IN CDF)
== == ==	PROPOSED CONDUIT (MULTIPLE BANK, AS NOTED)
-----	OVERHEAD CABLE
-----	DIRECT BURIAL CABLE
== == ≡	PROPOSED CONDUIT, TO BE CAPPED

EXISTING SIGNS

—	GROUND MOUNTED SINGLE POST SUPPORT
—	GROUND MOUNTED DOUBLE POST SUPPORT

PROPOSED SIGNS

—	GROUND MOUNTED SINGLE POST SUPPORT
—	GROUND MOUNTED DOUBLE POST SUPPORT

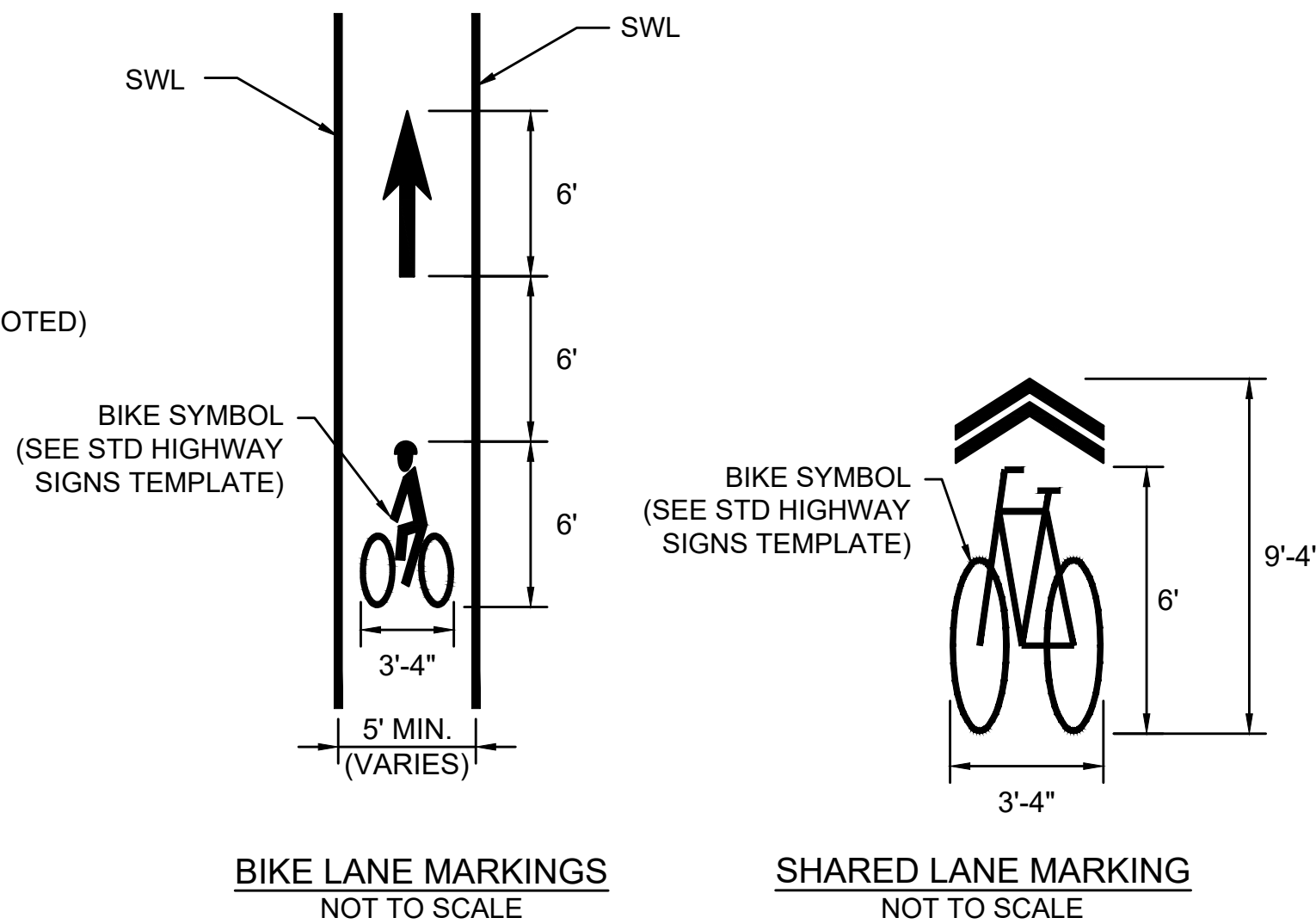
PAVEMENT MARKINGS

BWL	BROKEN WHITE LINE-6", (10' LANE LINE, 30' SPACE)	— — — —
DWL	DOTTED WHITE LINE-6", (3' LINE, 9' SPACE)	- - - - -
DWLex	DOTTED WHITE EXTENSION LINE-6", (2" LINE, 6" SPACE)	- - - - -
SWL	SOLID WHITE LINE-6"	—————
WTL	WHITE TRANSVERSE LINE-6", 10' O.C., 45°	
DBYL	DOUBLE YELLOW CENTER LINE-2-6" LINES	=====
DYL	DOTTED YELLOW LANE LINE-6", (2' LINE, 6' SPACE)	- - - - -
SYL	SOLID YELLOW LINE-6"	—————
YTL	YELLOW TRANSVERSE LINE-12", 10' O.C., 45°	
WDWL	WIDE DOTTED WHITE LINE-12"	- - - - -
WSWL	WIDE SOLID WHITE LINE-12"	—————
SWCHL	SOLID WHITE CHANNELIZING LINE-6"	
SL	STOP LINE, 12" WHITE LINE (SEE DETAIL BELOW)	
CW	CROSSWALK, 12" WHITE LINES (SEE DETAIL BELOW)	
◀	TWO-WAY YELLOW/YELLOW SLOTTED PAVEMENT MARKER (40' SPACING TYP)	

◀	EXISTING DIRECTION OF FLOW
◀	PROPOSED DIRECTION OF FLOW
—	BIKE DETECTOR PAVEMENT MARKING (SEE FHWA STANDARD HIGHWAY SIGNS TEMPLATE)

NOTES:

- ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC



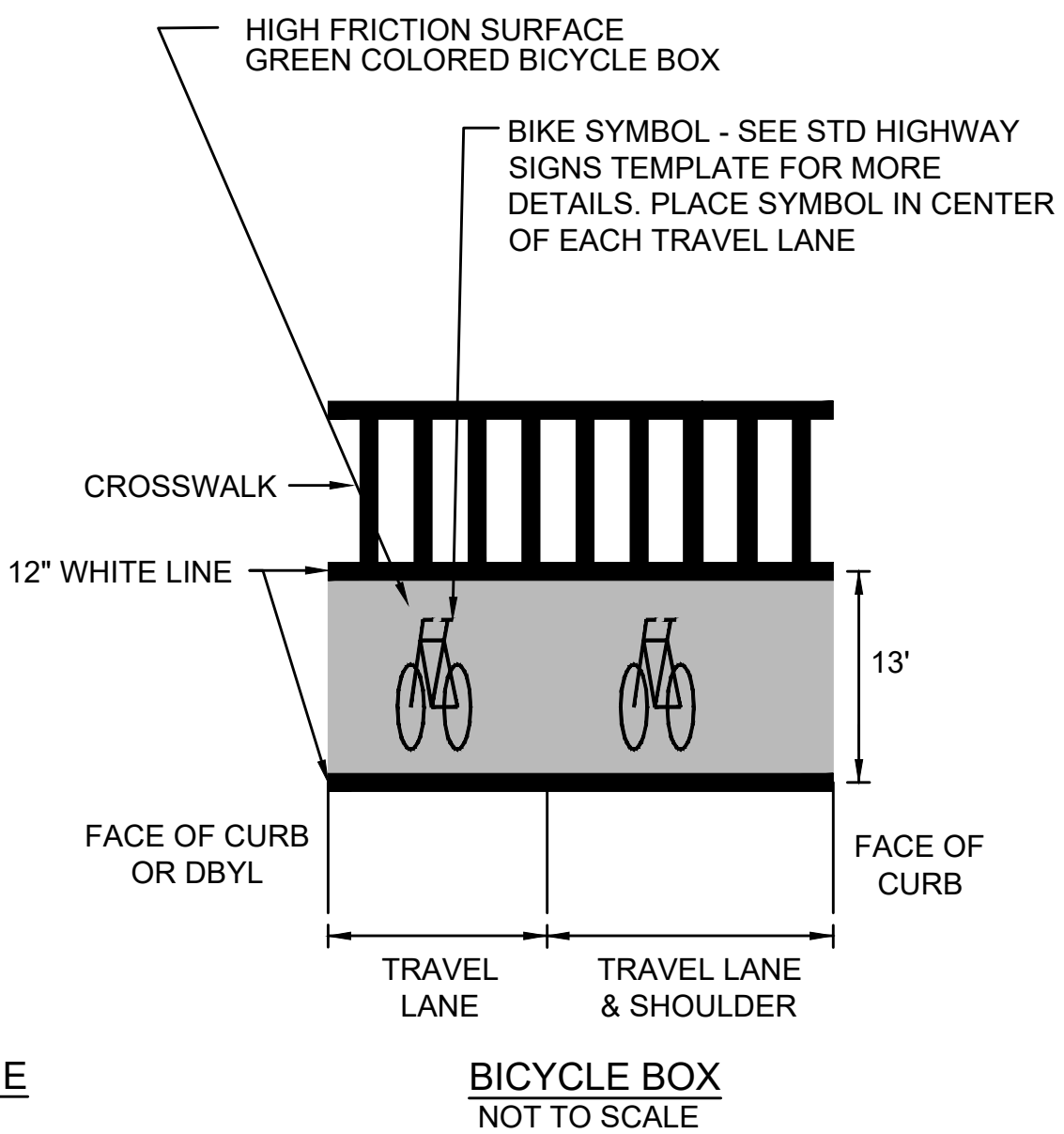
R	STEADY CIRCULAR RED
Y	STEADY CIRCULAR YELLOW
G	STEADY CIRCULAR GREEN
GV	STEADY GREEN VERTICAL ARROW
RL	STEADY RED LEFT ARROW
YL	STEADY YELLOW LEFT ARROW
GL	STEADY GREEN LEFT ARROW
RR	STEADY RED RIGHT ARROW
YR	STEADY YELLOW RIGHT ARROW
GR	STEADY GREEN RIGHT ARROW
FY	FLASHING YELLOW
FR	FLASHING RED

W	WALK - LUNAR WHITE
FDW	FLASHING DON'T WALK - PORTLAND ORANGE
DW	DON'T WALK - PORTLAND ORANGE

NOTE:

THE FOLLOWING LABELS MAY BE USED TO DENOTE EXISTING CONDITIONS OR PROPOSED CHANGES TO EXISTING CONDITIONS:

(ABAN)	TO BE ABANDONED
(ADJ)	ADJUST TO GRADE
(EXIST)	EXISTING
(MOD)	MODIFY
(NIC)	NOT IN CONTRACT
(PM)	EXISTING POLE-MOUNTED SIGN
(PROP)	PROPOSED
(R&D)	REMOVE AND DISCARD
(R&R)	REMOVE AND RESET
(R&S)	REMOVE AND STACK
(REM)	REMOVE
(RET)	RETAIN





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SEE SHEET 2
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Notes
SEE SHEET 2

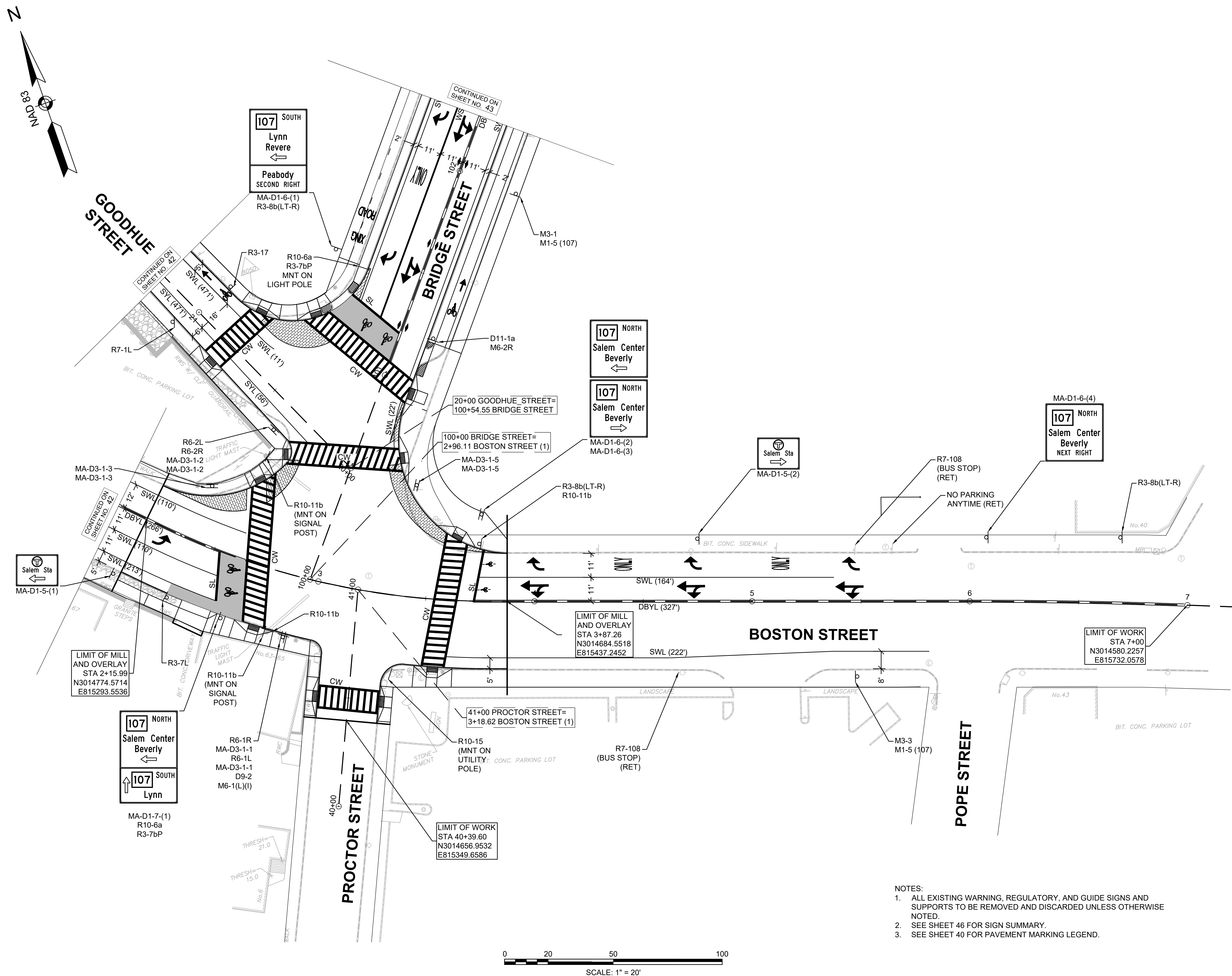
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CITY OF SALEM
Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title
SIGN & PAVEMENT MARKING PLAN
PART 1 OF 5

179410455	1" = 20'	
Project No.	Scale	
	41 of 113	
Drawing No.	Sheet	Revision



NOTES:

1. ALL EXISTING WARNING, REGULATORY, AND GUIDE SIGNS AND SUPPORTS TO BE REMOVED AND DISCARDED UNLESS OTHERWISE NOTED.
2. SEE SHEET 46 FOR SIGN SUMMARY.
3. SEE SHEET 40 FOR PAVEMENT MARKING LEGEND.

0 20 50 100

SCALE: 1" = 20'

\\US1552-F01\SHARED_PROJECTS\179410455\transportation\drawing\dwg_sign_pavement_marking_plan.dwg
2020/02/05 12:52 PM By: Borenstein, Sarah



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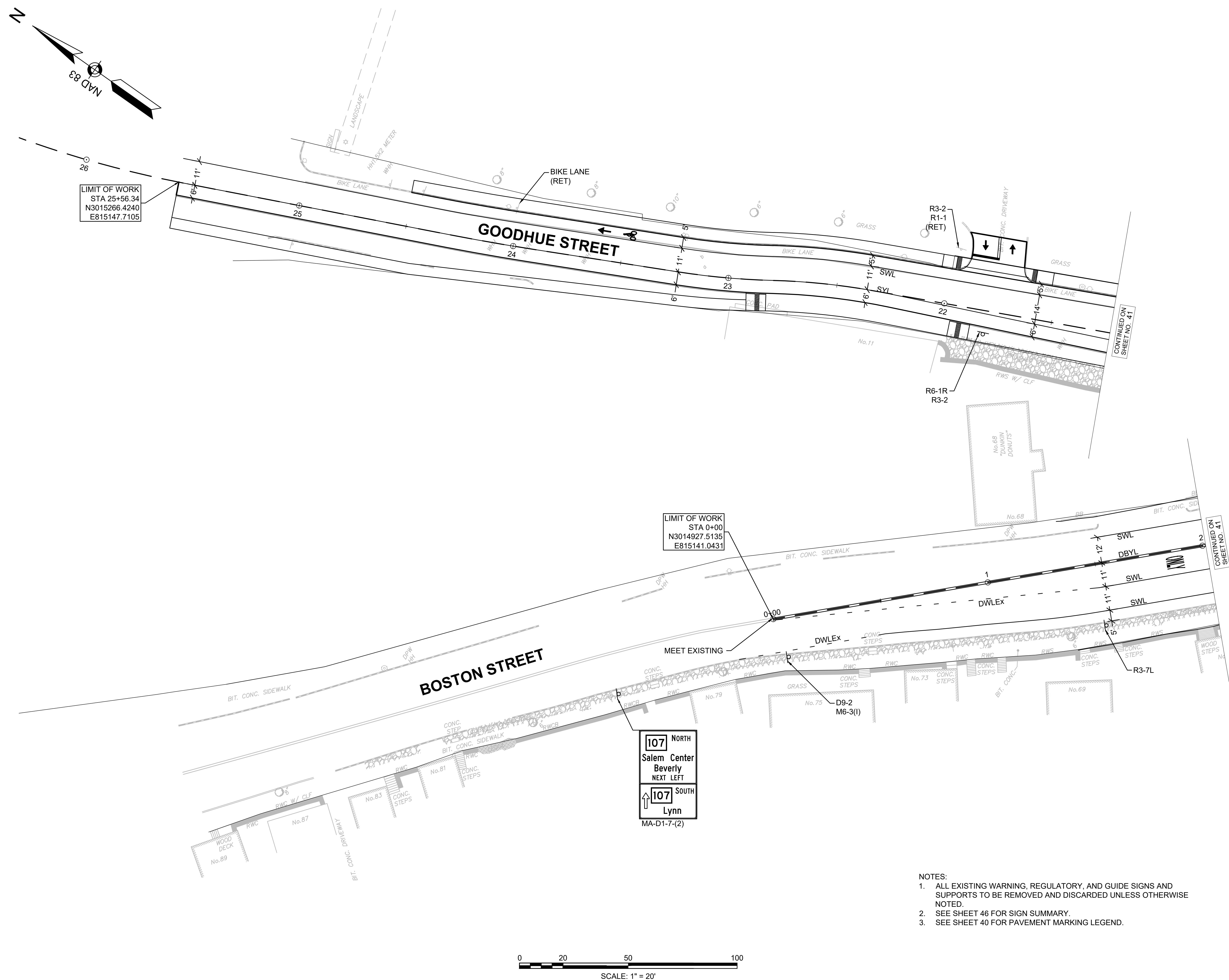
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Legend
SEE SHEET 2
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[illegible]

Title
SIGN & PAVEMENT MARKING PLAN
PART 2 OF 5

179410455	1" = 20'	
Project No.	Scale	
	42 of 113	
Drawing No.	Sheet	Revision



NOTES:

1. ALL EXISTING WARNING, REGULATORY, AND GUIDE SIGNS AND SUPPORTS TO BE REMOVED AND DISCARDED UNLESS OTHERWISE NOTED.
2. SEE SHEET 46 FOR SIGN SUMMARY.
3. SEE SHEET 40 FOR PAVEMENT MARKING LEGEND.

\\US1552-F01\SHARED_PROJECTS\179410455\transportation\drawing\dwg_sign_pavement_marking_plan.dwg
2020/02/05 12:53 PM By: Borenstein, Sarah



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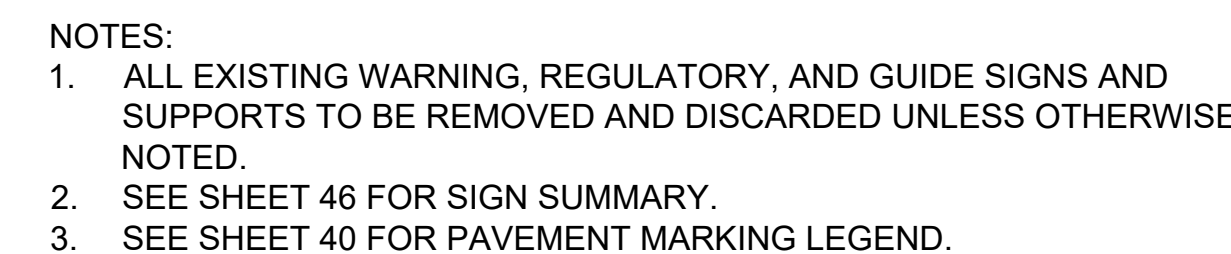
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Issued	By	Appd.	YY.MM.DD

Permit-Seal



Title
SIGN & PAVEMENT MARKING PLAN
PART 3 OF 5

179410455	1" = 20'	
Project No.	Scale	
	43 of 113	
Drawing No.	Sheet	Revision



0 20 50 100

SCALE: 1" = 20'

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Notes

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Revision

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File Name: dwg_sign_pavement_marking_plan.dwg

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CITY OF SALEM

Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title

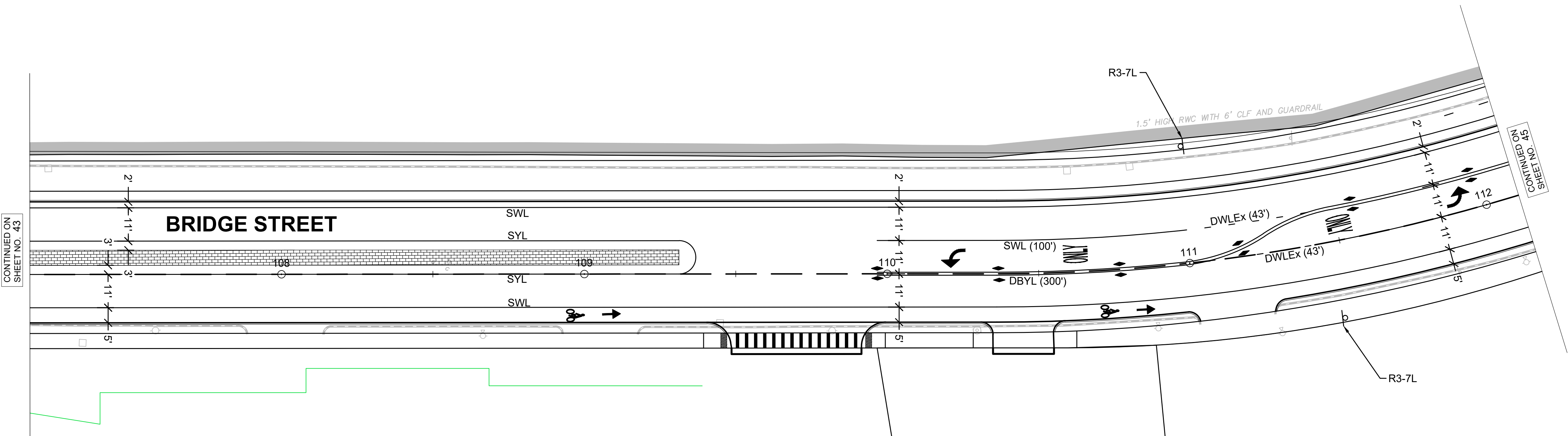
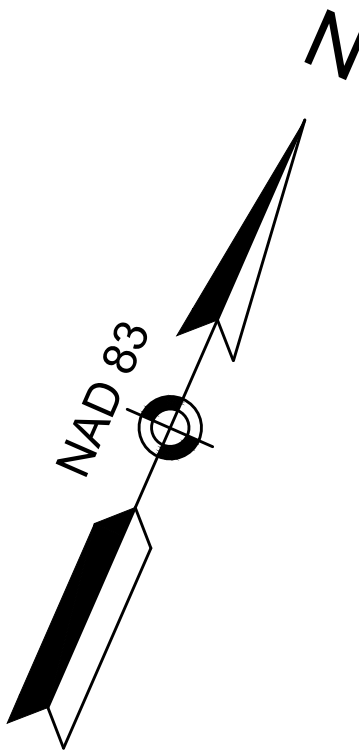
SIGN & PAVEMENT MARKING PLAN
PART 4 OF 5

179410455 1" = 20'

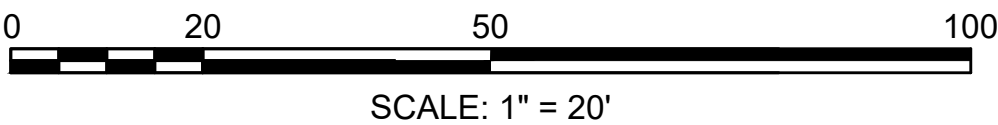
Project No. Scale

44 of 113

Drawing No. Sheet Revision



- NOTES:
1. ALL EXISTING WARNING, REGULATORY, AND GUIDE SIGNS AND SUPPORTS TO BE REMOVED AND DISCARDED UNLESS OTHERWISE NOTED.
 2. SEE SHEET 46 FOR SIGN SUMMARY.
 3. SEE SHEET 40 FOR PAVEMENT MARKING LEGEND.





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Legend
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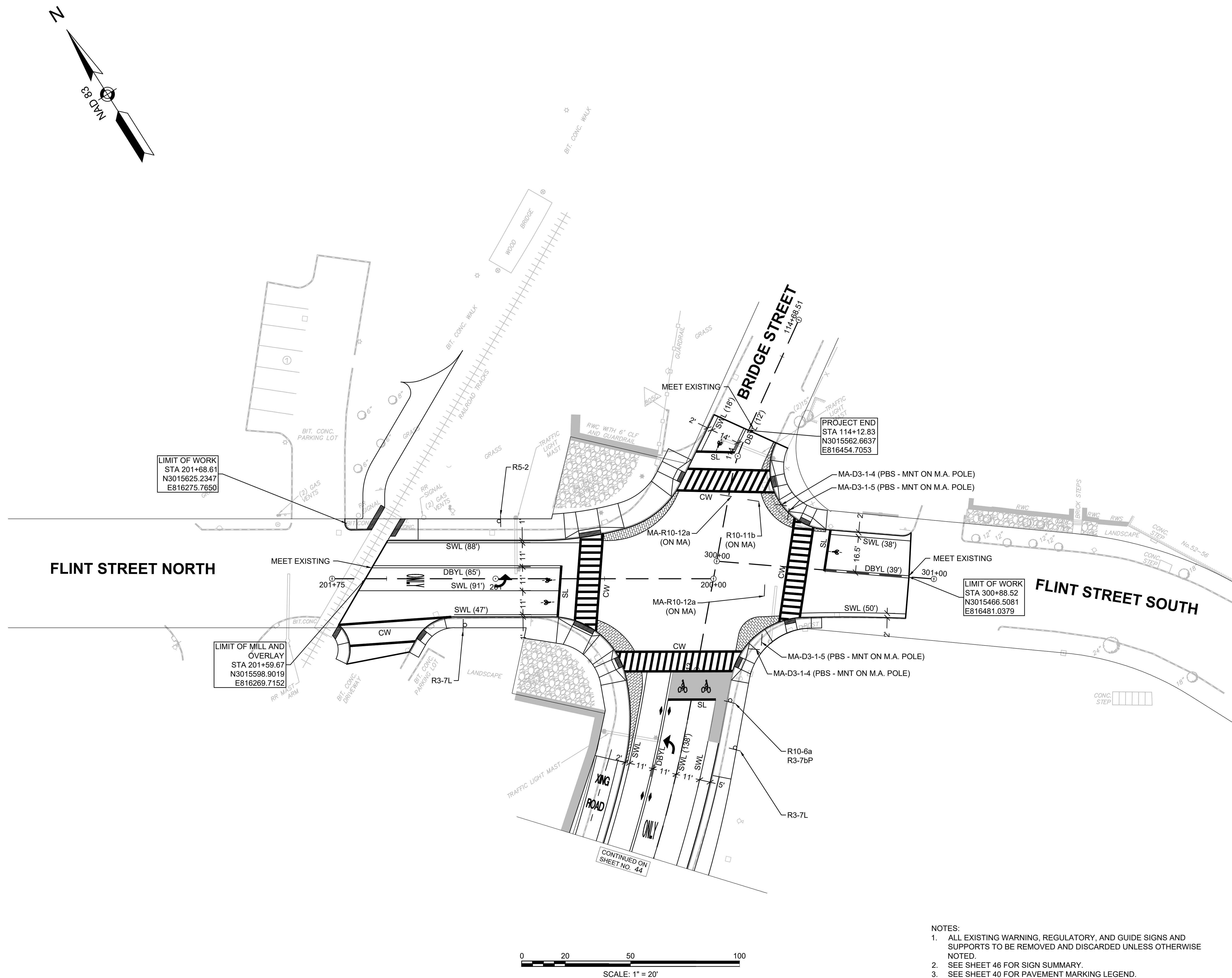
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CITY OF SALEM
Client/Project

Title
SIGN & PAVEMENT MARKING PLAN
PART 5 OF 5

179410455	1" = 20'	
Project No.	Scale	
	45 of 113	
Drawing No.	Sheet	Revision





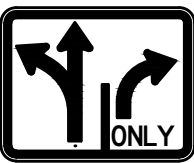


















NOTES:

1. ALL EXISTING WARNING, REGULATORY, AND GUIDE SIGNS AND SUPPORTS TO BE REMOVED AND DISCARDED UNLESS OTHERWISE NOTED.
2. SEE SHEET 46 FOR SIGN SUMMARY.
3. SEE SHEET 40 FOR PAVEMENT MARKING LEGEND.

\\US1552-F01\SHARED_PROJECTS\179410455\transportation\drawing\dwg_sign_pavement_marking_plan.dwg
2020/02/05 12:53 PM By: Borenstein, Sarah

\\US1526R01\SHARED_PROJECTS\179410455\Transportation\drawing\dwg_sign_summary.dwg
2020/02/05 12:54 PM By: Borendien, Sarah

IDENTIFI- CATION NUMBER	SIZE OF SIGN		SIGN	LEGEND DIMENSIONS			NUMBER OF SIGNS REQUIRED	COLOR			SUPPORT AND NUMBER REQUIRED	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW		BACK- GROUND	LEGEND	BORDER		
R1-1	18"	18"		SEE MUTCD	SEE MUTCD	SEE MUTCD	1	RED	WHITE	WHITE	1-P5 1	2.25
R3-2	24"	24"					1	WHITE	RED/ BLACK	BLACK	1-P5 1	4.00
R3-7bP	24"	12"					3	WHITE	BLACK	BLACK	MOUNT 3 WITH R10-6a	6.00
R3-7L	30"	30"					3	WHITE	BLACK	BLACK	1-P5 3	18.75
R3-8b(LT-R)	36"	30"					4	WHITE	BLACK	BLACK	1-P5 4	30.00
R3-17	24"	18"					2	BLACK	WHITE		1-P5 2	6.00
								WHITE	BLACK			
R5-2	24"	24"					1	WHITE	RED/ BLACK	BLACK	1-P5 1	4.00
R5-3	24"	24"					1	WHITE	BLACK	BLACK	1-P5 1	4.00
R6-1L	36"	12"					2	WHITE	BLACK	BLACK	1-P5 2	6.00
R6-1R	36"	12"					2	WHITE	BLACK	BLACK	MOUNT 1 WITH R3-2 MOUNT 1 WITH R6-1L	6.00
R10-6a	24"	36"					3	WHITE	BLACK	BLACK	MNT 1 W/MA-D1-7-(1) MNT 1 WITH LIGHT POLE 1-P5 - 1	18.00
R10-11b	36"	36"					5	WHITE	BLACK	BLACK	MOUNT 3 ON SIGNAL POST MOUNT 1 ON MA MNT. 1 W/R3-8b	45.00
R10-15	30"	30"		↓	↓	↓	1	WHITE/ YELLOW	RED/ BLACK	BLACK	MOUNT ON UTILITY POLE	6.25
MA-R10-12a	30"	36"		SEE MASSDOT STDS.	SEE MASSDOT STDS.	SEE MASSDOT STDS.	2	WHITE	BLACK/ YELLOW	BLACK	MOUNT 2 ON MA	15.00
W3-1	18"	18"		SEE MUTCD	SEE MUTCD	SEE MUTCD	1	YELLOW	BLACK/ RED	BLACK	1-P5 1	2.25
W11-15	30"	30"		↓	↓	↓	2	FLUOR. YELLOW	BLACK	BLACK	1-P5 2	12.50
W16-9P	24"	12"		↓	↓	↓	2	FLUOR. YELLOW	BLACK	BLACK	MOUNT 2 WITH W11-15	4.00

IDENTIFI- CATION NUMBER	SIZE OF SIGN		SIGN	LEGEND DIMENSIONS			NUMBER OF SIGNS REQUIRED	COLOR			SUPPORT AND NUMBER REQUIRED	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW		BACK- GROUND	LEGEND	BORDER		
R4-7	24"	30"		SEE MUTCD	SEE MUTCD	SEE MUTCD	2	WHITE	BLACK	BLACK	1-P5 2	10.00
R6-2L	24"	30"					1	WHITE	BLACK	BLACK	1-P5 1	5.00
R6-2R	24"	30"					1	WHITE	BLACK	BLACK	MOUNT 1 WITH R6-2L	5.00
R7-1L	12"	18"		↓	↓	↓	1	WHITE	RED	RED	1-P5 1	1.50



65 NETWORK DRIVE, 2ND FLOOR
BURLINGTON, MA 01803
Tel. (781) 221-1000
www.stantec.com

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Consultants

Legend

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Notes

SEE SHEET 2

Revision	By	Appd.	YY.MM.DD
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Issued	By	Appd.	YY.MM.DD

File Name: dwg_sign_summary.dwg			
	Dwn.	Chkd.	Dsgn.
			YY.MM.DD

Permit-Seal



CITY OF SALEM

Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title

TRAFFIC SIGN SUMMARY
PART 1 OF 3

179410455 NOT TO SCALE

Project No. Scale

46 of 113

Drawing No. Sheet Revision

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Revision	By	Appd.	YY.MM.DD
BID SET			
Issued	By	Appd.	YY.MM.DD

File Name: dwg_sign_summary.dwg	Dwn.	Chkd.	Dsgn.	YY.MM.DD
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Permit-Seal



CITY OF SALEM








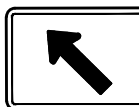
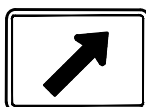







Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title

TRAFFIC SIGN SUMMARY
PART 2 OF 3

179410455	NOT TO SCALE
Project No.	Scale
	47 of 113
Drawing No.	Sheet
	Revision

IDENTIFI- CATION NUMBER	SIZE OF SIGN		SIGN	LEGEND DIMENSIONS			NUMBER OF SIGNS REQUIRED	COLOR			SUPPORT AND NUMBER REQUIRED	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW		BACK- GROUND	LEGEND	BORDER		
D9-2	24"	24"		SEE MUTCD	SEE MUTCD	SEE MUTCD	2	BLUE	WHITE	WHITE	MNT 1 W/R6-1L 1-P5 1	8.00
D11-1a	18"	18"					2	GREEN	WHITE	WHITE	1-P5 2	4.50
D11-2	18"	18"					1	GREEN	WHITE	WHITE	1-P5 1	2.25
MA-M1-5a	30"	24"					2	WHITE	BLACK	BLACK	1-P5 2	10.00
M3-1	24"	12"					1	WHITE	BLACK	BLACK	MOUNT 1 WITH MA-M1-5a	2.00
M3-3	24"	12"					1	WHITE	BLACK	BLACK	MOUNT 1 WITH MA-M1-5a	2.00
M6-1(L)(I)	21"	15"					1	BLUE	WHITE	WHITE	MOUNT 1 WITH D9-2	2.19
M6-2L	21"	15"					1	GREEN	WHITE	WHITE	MOUNT 1 WITH D11-1a	2.19
M6-2R	21"	15"					1	GREEN	WHITE	WHITE	MOUNT 1 WITH D11-1a	2.19
M6-3(I)	21"	15"					1	BLUE	WHITE	WHITE	MOUNT 1 WITH D9-2	2.19
MA-D3-1-1	54"	12"		6D 4D	3" 5" 3" 3"	TOWN SEAL, 6"	2	GREEN REFL.	WHITE REFL.	WHITE REFL.	MOUNT 2 WITH R6-1L	9.00
MA-D3-1-2	60"	12"		6D 4D	3" 5" 3" 3"	TOWN SEAL, 6"	2	GREEN REFL.	WHITE REFL.	WHITE REFL.	MOUNT 2 WITH R6-1L	10.00
MA-D3-1-3	48"	12"		6D 4D	3" 5" 3" 3"	TOWN SEAL, 6"	2	GREEN REFL.	WHITE REFL.	WHITE REFL.	1-P5 1	8.00
MA-D3-1-4 (P.B.S.)	42"	12"		6D 4D	3" 5" 3" 3"	TOWN SEAL, 6"	2	GREEN REFL.	WHITE REFL.	WHITE REFL.	MOUNT 2 ON MAST ARM POLE	7.00
MA-D3-1-5	48"	12"		6D 4D	3" 5" 3" 3"	TOWN SEAL, 6"	2	GREEN REFL.	WHITE REFL.	WHITE REFL.	1-P5 2	8.00
MA-D3-1-5 (P.B.S.)	48"	12"		6D 4D	3" 5" 3" 3"	TOWN SEAL, 6"	2	GREEN REFL.	WHITE REFL.	WHITE REFL.	MOUNT 2 ON MAST ARM POLE	8.00



SALEM CITY SEAL
NOT TO SCALE

NOTE:
CONTRACTOR TO
COORDINATE WITH
CITY OF SALEM
FOR SEAL

Revision	By	Appd.	YY.MM.DD
BID SET			
Issued	By	Appd.	YY.MM.DD

File Name: dwg_sign_summary.dwg	Dwn.	Chkd.	Dsgn.	YY.MM.DD
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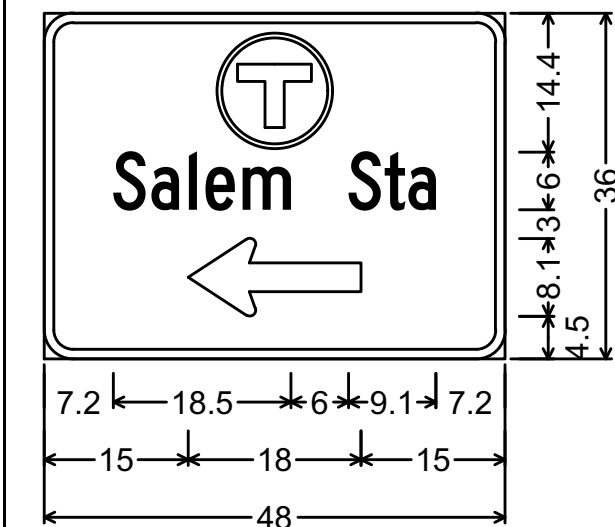
CITY OF SALEM
Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title
TRAFFIC SIGN SUMMARY
PART 3 OF 3

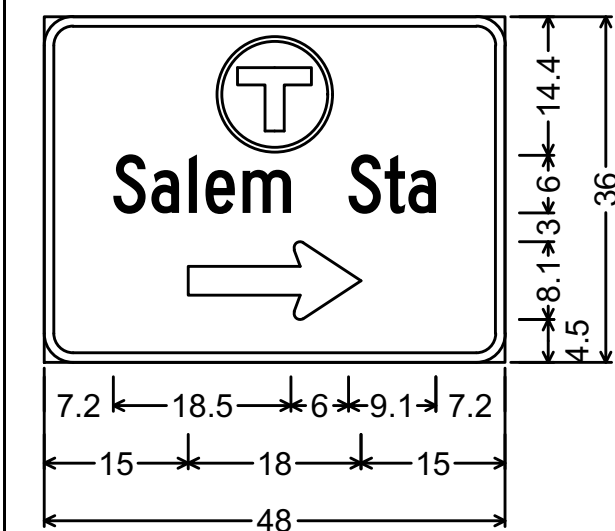
179410455	NOT TO SCALE
Project No.	Scale
	48 of 113
Drawing No.	Sheet
	Revision

MA-D1-5-1		
4.0000'	3.0000'	12.00 SF
BACKGROUND	LEGEND	BORDER
GREEN REFL.	SILVER WHITE	SILVER WHITE



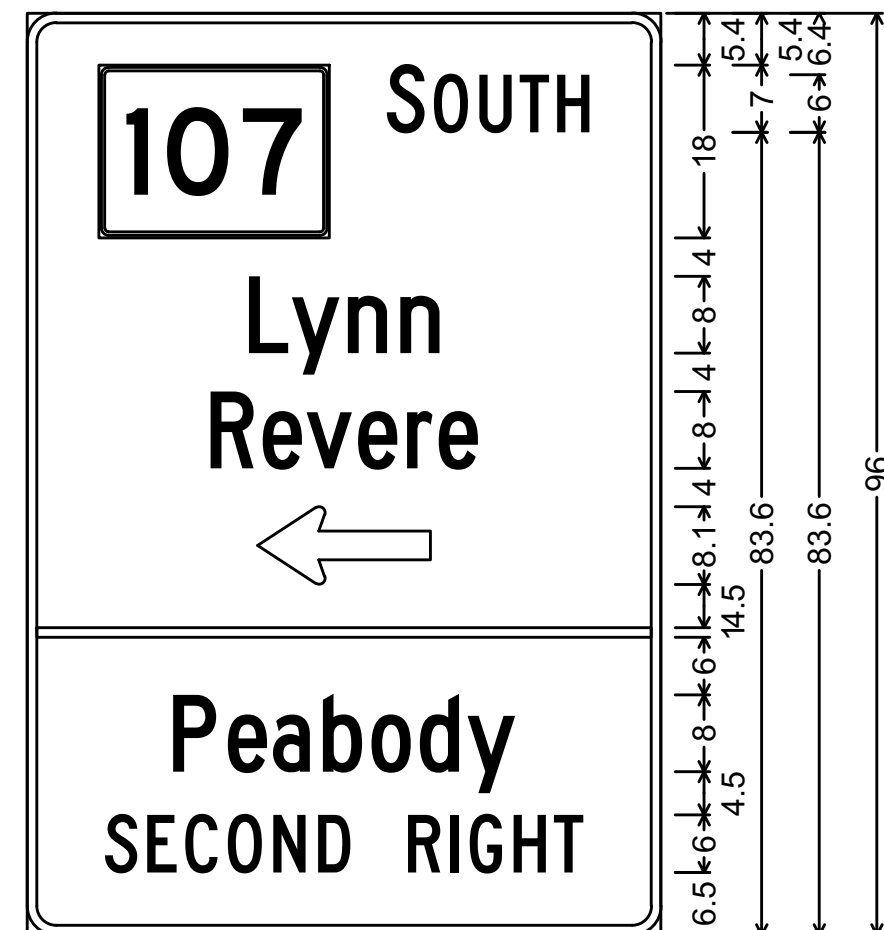
3.0" Radius, 1.0" Border, White on Green;
[Salem Sta] C 2K;
Standard Arrow Custom 18.0" X 8.1" 180°;

MA-D1-5-2		
4.0000'	3.0000'	12.00 SF
BACKGROUND	LEGEND	BORDER
GREEN REFL.	SILVER WHITE	SILVER WHITE



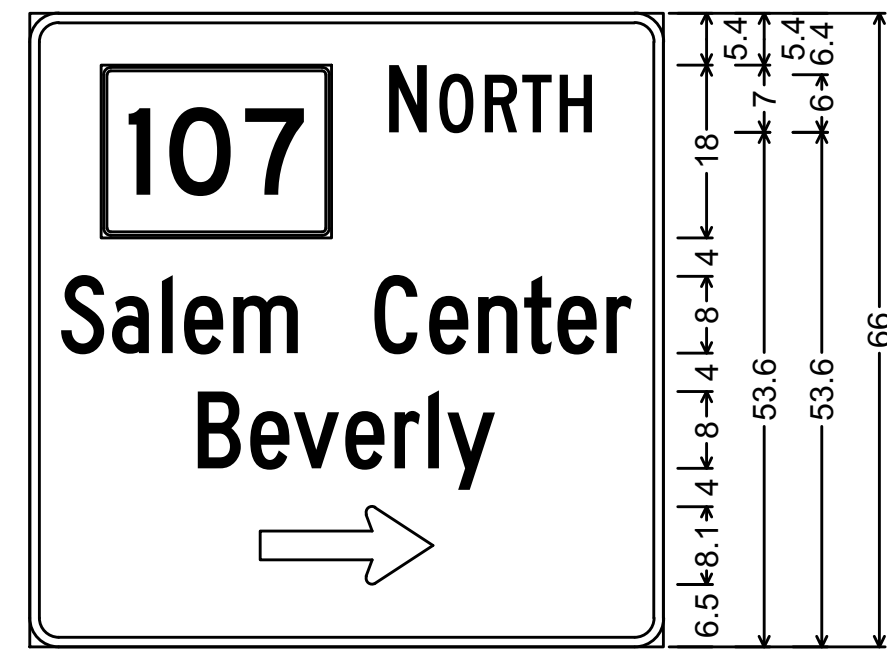
3.0" Radius, 1.0" Border, White on Green;
[Salem Sta] C 2K; Standard Arrow Custom 18.0" X 8.1" 0°;

MA-D1-6-1		
5.50'	8.00'	44.00 SF
BACKGROUND	LEGEND	BORDER
GREEN REFL.	SILVER WHITE	SILVER WHITE



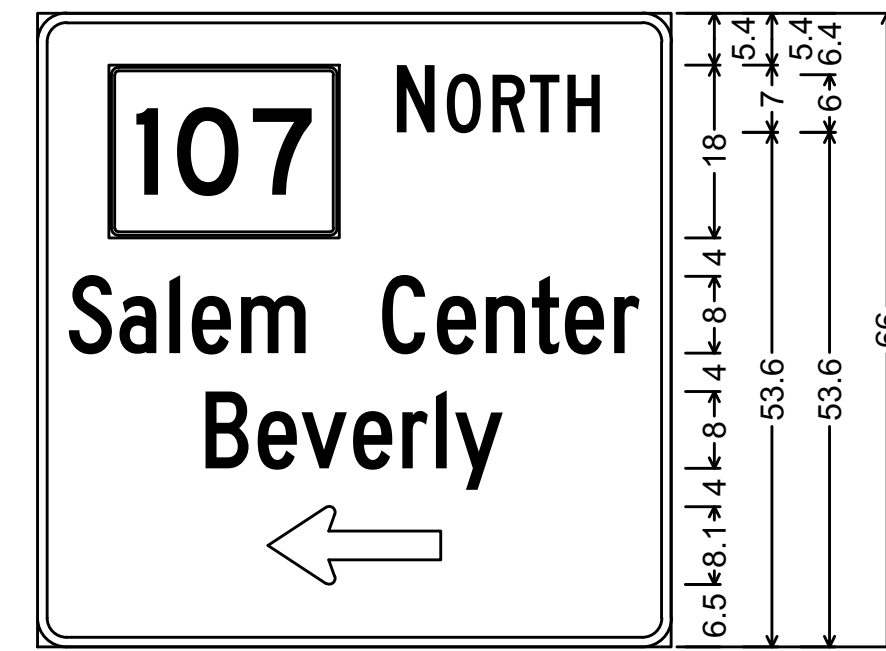
3.0" Radius, 1.0" Border, White on Green;
State Highway 107 M1-5;
[Revere] C 2K; [SOUTH] C 2K; [Lynn] C 2K;
Standard Arrow Custom 18.0" X 8.1" 180°;
[Peabody] C 2K; [SECOND RIGHT] C 2K;

MA-D1-6-2		
5.5000'	5.5000'	30.25 SF
BACKGROUND	LEGEND	BORDER
GREEN REFL.	SILVER WHITE	SILVER WHITE



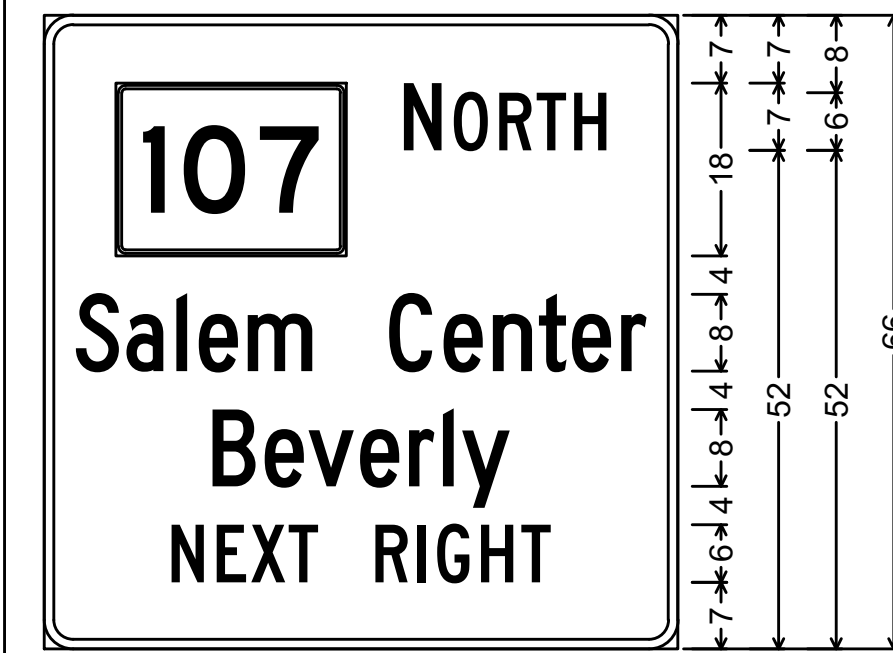
3.0" Radius, 1.0" Border, White on Green;
State Highway 107 M1-5;
[NORTH] C 2K; [Salem Center] C 2K;
[Beverly] C 2K;
Standard Arrow Custom 18.0" X 8.1" 0°;

MA-D1-6-3		
5.5000'	5.5000'	30.25 SF
BACKGROUND	LEGEND	BORDER
GREEN REFL.	SILVER WHITE	SILVER WHITE



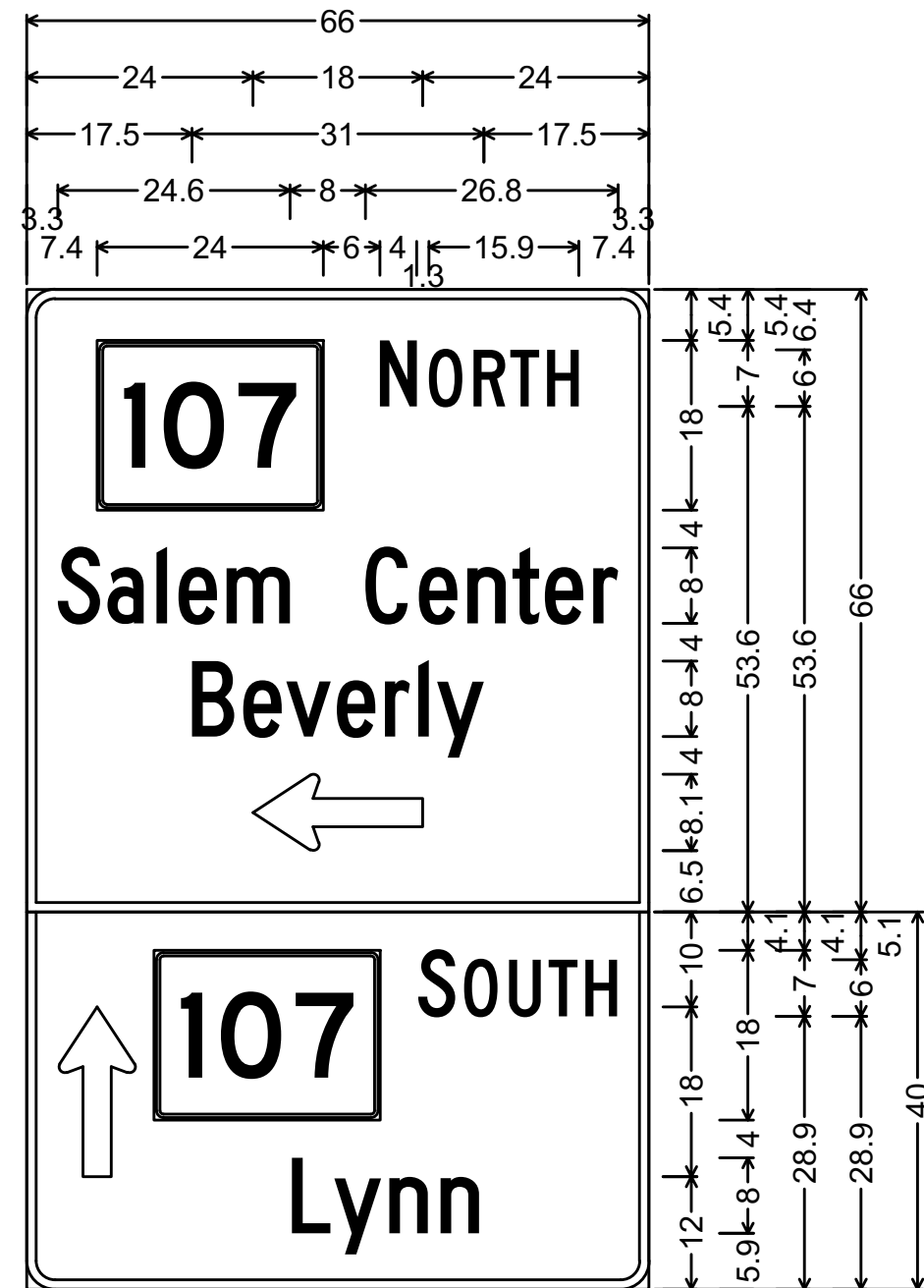
3.0" Radius, 1.0" Border, White on Green;
State Highway 107 M1-5;
[NORTH] C 2K; [Salem Center] C 2K;
[Beverly] C 2K;
Standard Arrow Custom 18.0" X 8.1" 0°;

MA-D1-6-4		
5.5000'	5.5000'	30.25 SF
BACKGROUND	LEGEND	BORDER
GREEN REFL.	SILVER WHITE	SILVER WHITE



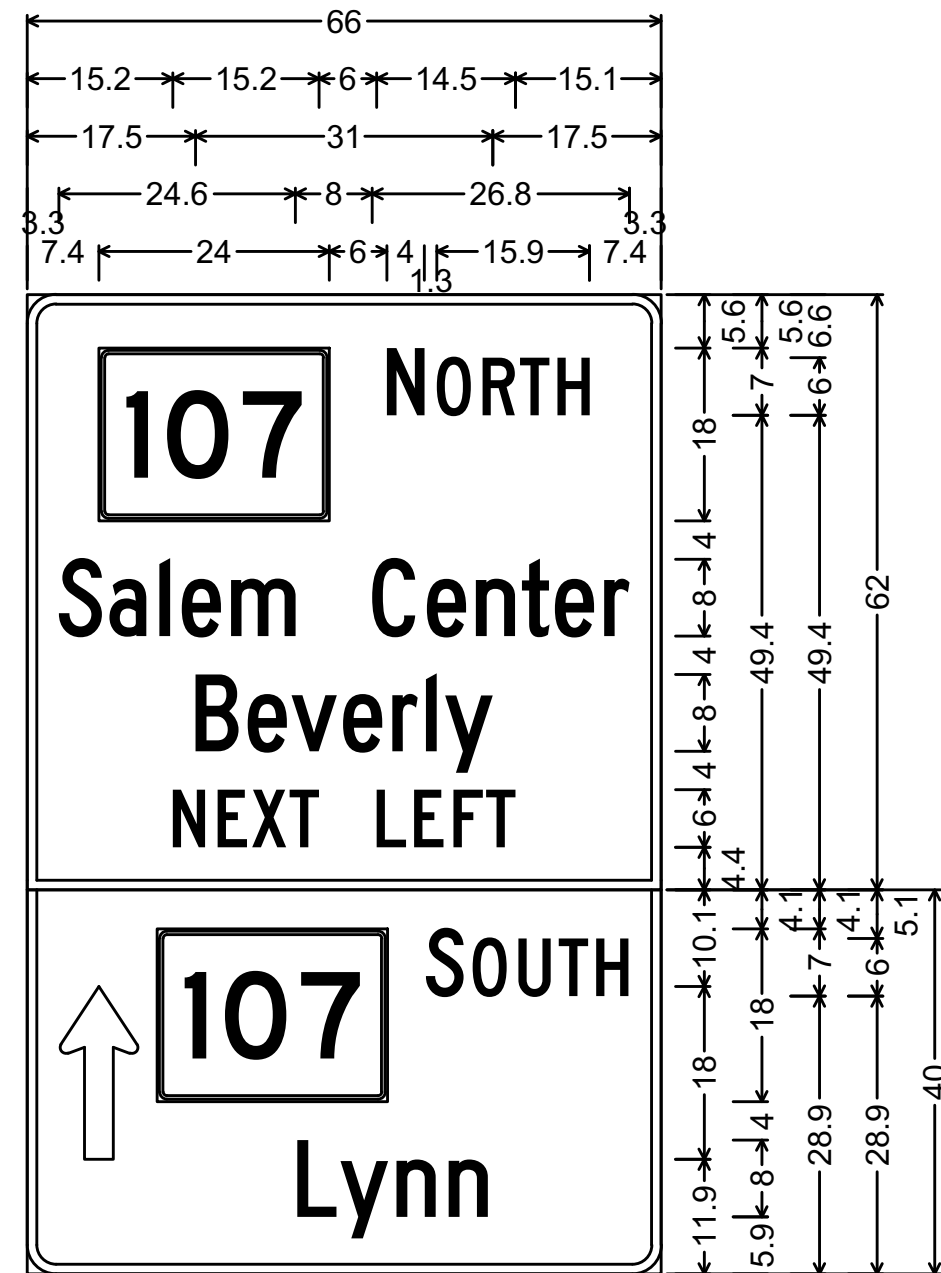
3.0" Radius, 1.0" Border, White on Green;
State Highway 107 M1-5;
[NORTH] C 2K; [Salem Center] C 2K;
[Beverly] C 2K; [NEXT RIGHT] C 2K;

MA-D1-7-1		
5.5000'	8.8333'	48.58 SF
BACKGROUND	LEGEND	BORDER
GREEN REFL.	SILVER WHITE	SILVER WHITE



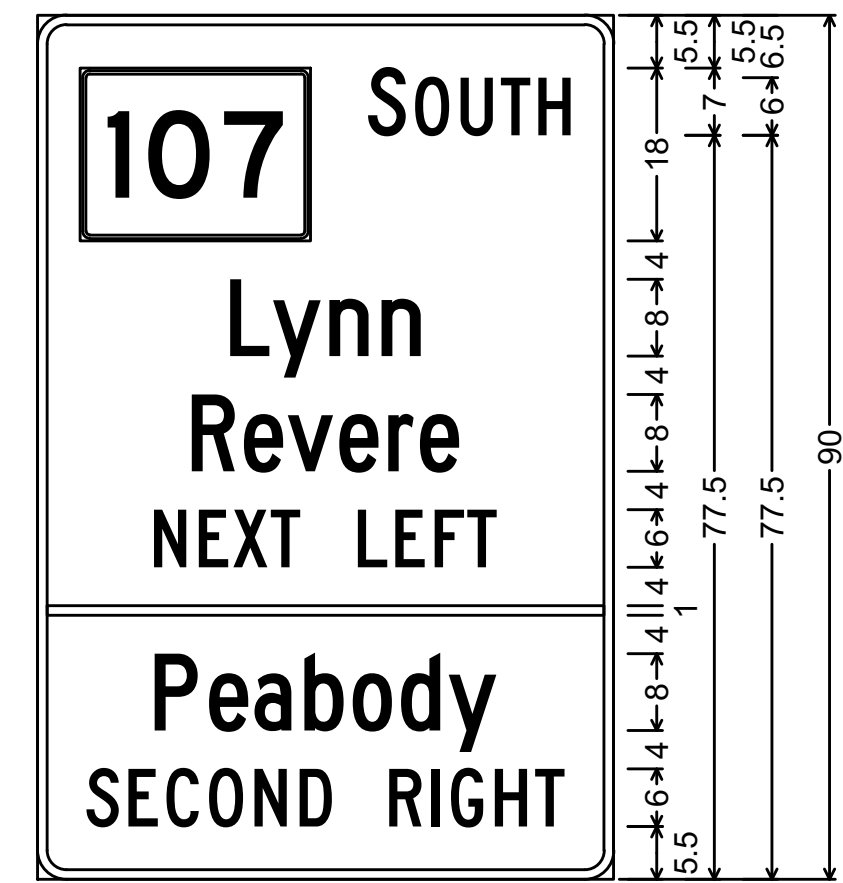
3.0" Radius, 1.0" Border, White on Green;
State Highway 107 M1-5;
[NORTH] C 2K; [Salem Center] C 2K;
[Beverly] C 2K;
Standard Arrow Custom 18.0" X 8.1" 180°;
3.0" Radius, 1.0" Border, White on Green;
Standard Arrow Custom 18.0" X 8.1" 90°;
State Highway 107 M1-5; [SOUTH] C 2K; [Lynn] C 2K;

MA-D1-7-2		
5.5000'	8.5000'	46.75 SF
BACKGROUND	LEGEND	BORDER
GREEN REFL.	SILVER WHITE	SILVER WHITE



3.0" Radius, 1.0" Border, White on Green;
State Highway 107 M1-5;
[NORTH] C 2K; [Salem Center] C 2K;
[Beverly] C 2K; [NEXT LEFT] C 2K;
3.0" Radius, 1.0" Border, White on Green;
Standard Arrow Custom 18.0" X 8.1" 90°;
State Highway 107 M1-5;
[SOUTH] C 2K; [Lynn] C 2K;

MA-D1-7a-(1)		
5.0000'	7.5000'	37.50 SF
BACKGROUND	LEGEND	BORDER
GREEN REFL.	SILVER WHITE	SILVER WHITE



3.0" Radius, 1.0" Border, White on Green;
State Highway 107 M1-5;
[SOUTH] C 2K; [Lynn] C 2K;
[Revere] C 2K;
[NEXT LEFT] C 2K;
[Peabody] C 2K;
[SECOND RIGHT] C 2K;



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Legend
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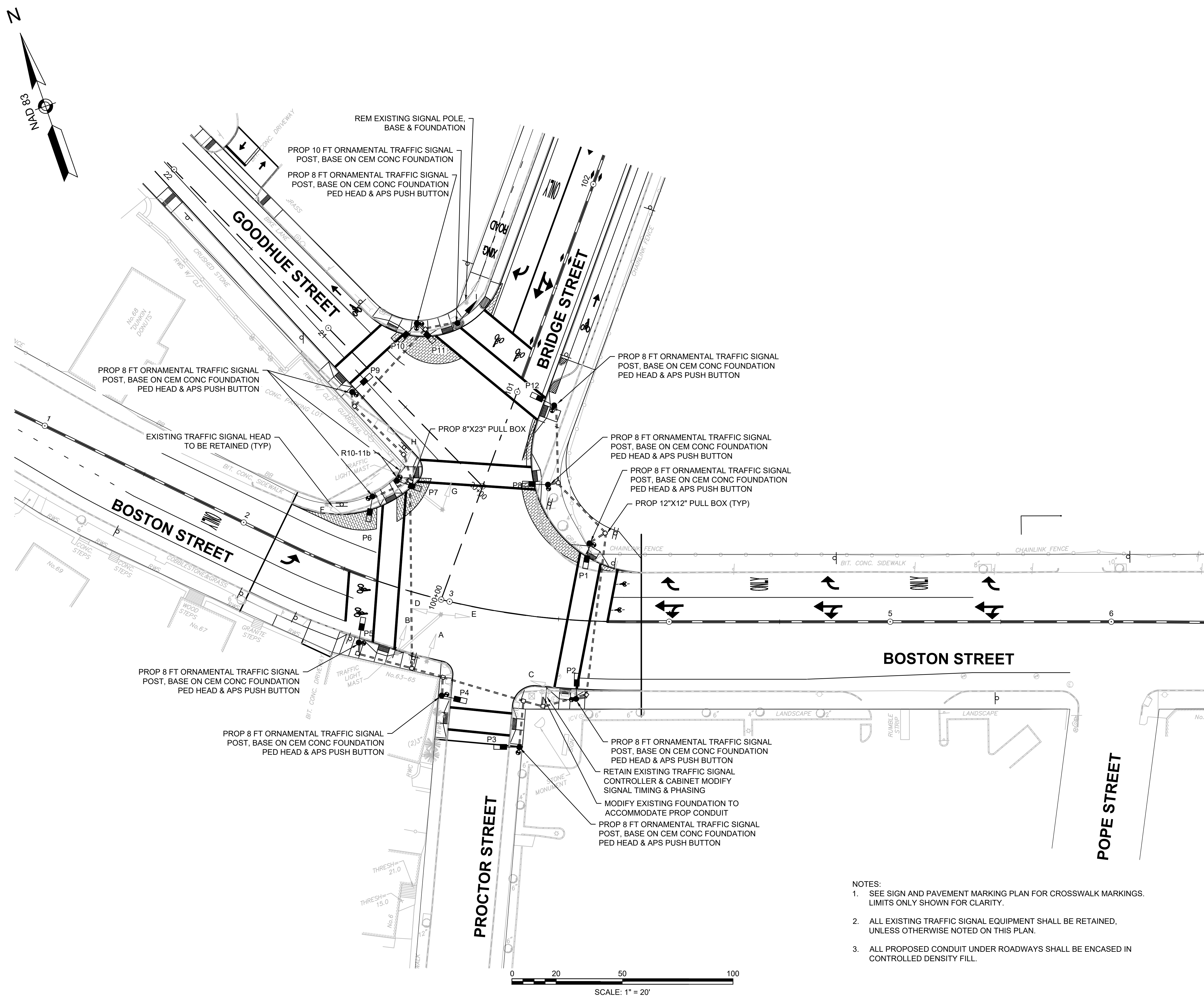
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File Name: dwg_traffic_signal_plan.dwg				
	Dwn	Chkd	Dsan	YY.MM.DD

CITY OF SALEM
Client/Project

Title	TRAFFIC SIGNAL PLAN LOCATION 1
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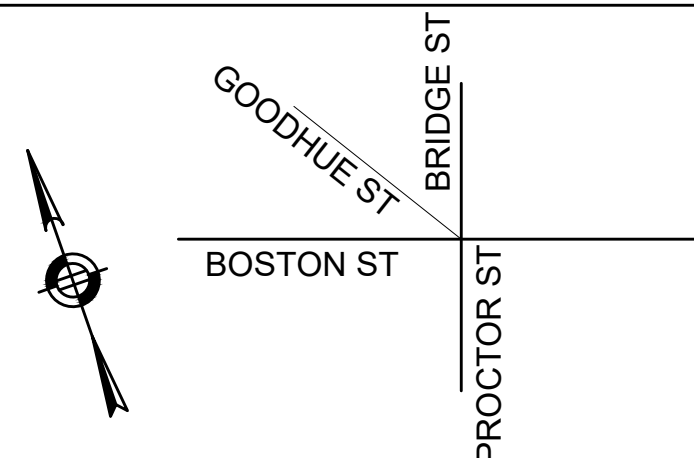
179410455	1" = 20'	
Project No.	Scale	
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Drawing No.	Sheet	Revision



NOTES:

1. SEE SIGN AND PAVEMENT MARKING PLAN FOR CROSSWALK MARKINGS LIMITS ONLY SHOWN FOR CLARITY.
2. ALL EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE RETAINED, UNLESS OTHERWISE NOTED ON THIS PLAN.
3. ALL PROPOSED CONDUIT UNDER ROADWAYS SHALL BE ENCASED IN CONTROLLED DENSITY FILL.

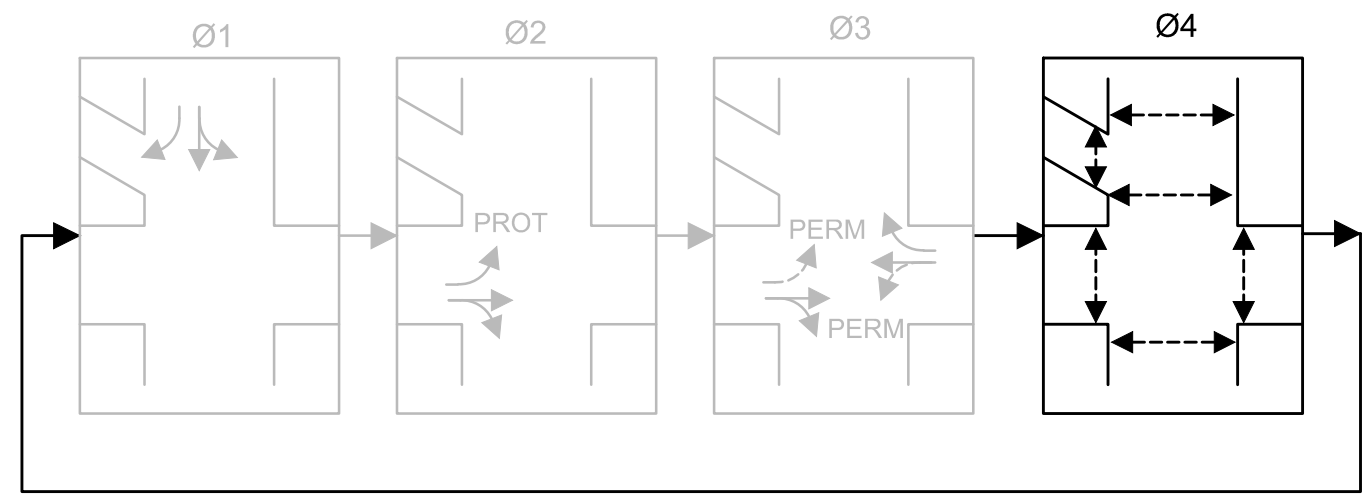
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2020/02/05 12:55 PM By: Borenstein, Sarah

			PHASE 1		PHASE 2		PHASE 3		PHASE 4		PHASE 5		PHASE 2		PHASE 7		PHASE 8		PHASE 9														
SEQUENCE AND TIMING FOR FULL ACTUATED CONTROL (ISOLATED)																																	
STREET		DIRECTION	HOUSINGS	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	FLASH OPER.		
BRIDGE STREET		SB	A,B	G	G	Y	R	R	R	R	R	R	R	R	R	R															FR		
BOSTON STREET		EB	C	R	R	R	R	G	G	G	G	Y	R	R	R	R															FY		
BOSTON STREET		EB	D	R	R	R	R	GL/G	YL/G	G	G	Y	R	R	R	R															FY		
BOSTON STREET		WB	E,F	R	R	R	R	R	R	R	G	Y	R	R	R	R															FY		
BRIDGE STREET		SB	G,H,I	G	Y	R	R	R	R	R	R	R	R	R	R	R															FR		
PEDESTRIAN		ALL	P1-P2	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	W/FDW	FDW	DW															OUT		
			P3-P4	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	W/FDW	FDW	DW															OUT		
			P5-P6	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	W/FDW	FDW	DW															OUT		
			P7-P8	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	W/FDW	FDW	DW															OUT		
			P9-P10	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	W/FDW	FDW	DW															OUT		
			P11-P12	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	W/FDW	FDW	DW															OUT		
TIMING IN SECONDS																																	
MINIMUM GREEN (INITIAL)					8			8			8																						
PASSAGE TIME (VEHICLE)					2			2			2																						
MAXIMUM 1				37	3			20			30																						
MAXIMUM 2				37	3			20			30																						
YELLOW CLEARANCE					3	4			3			4																					
RED CLEARANCE							3.5			2.5			1.5																				
WALK (W)														7																			
PEDESTRIAN CLEARANCE														15	3	1																	
RECALL				OFF			OFF			MAX			OFF																				
MEMORY													LOCK (PED)																				

TECHNICAL NOTES

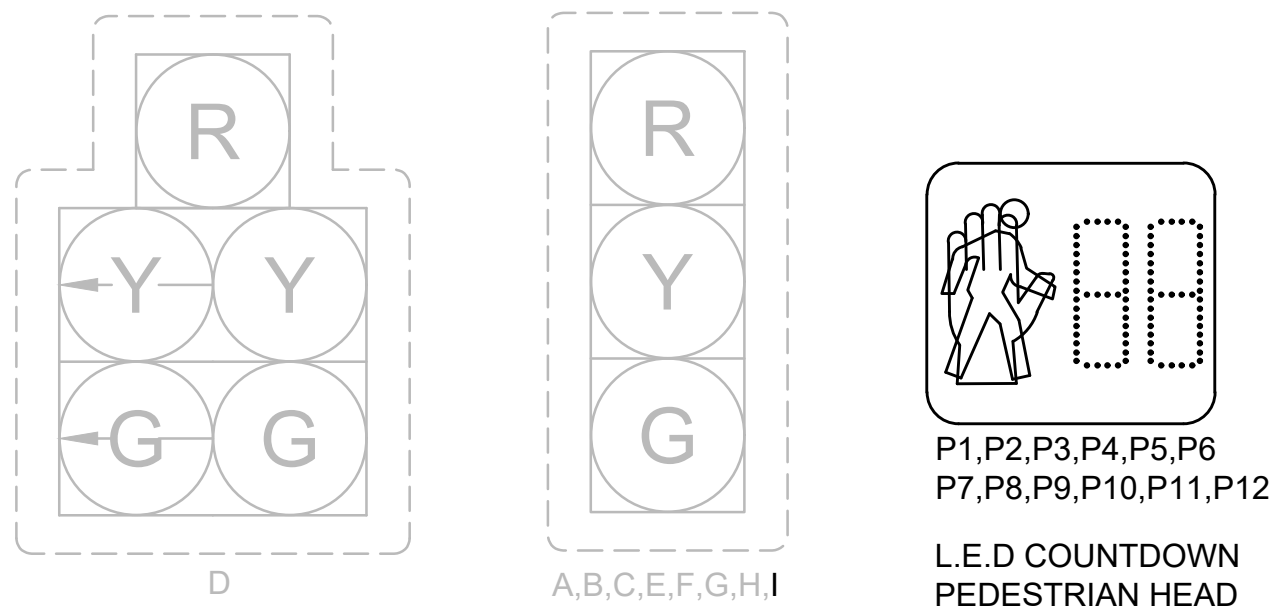
1. RETAIN EXISTING TRAFFIC SIGNAL PHASING.
2. ADD PEDESTRIAN PHASE (PHASE 4).
3. MAX 1 = 6 AM - 10 AM M-F
MAX 2 =ALL OTHER TIMES.

PREFERENTIAL PHASING SEQUENCE



- VEHICLE MOVEMENT
←-----> PEDESTRIAN MOVEMENT - UPON PEDESTRIAN ACTUATION

SIGNAL FACES



NOTES:

1. VEHICLE SIGNAL HEADS A,B,C,D,E,F,G,H ARE EXISTING HEADS AND SHALL BE RETAINED.
2. VEHICLE SIGNAL HEAD "I" SHALL BE 12-IN L.E.D. EQUIPPED WITH CAP VISORS.
3. VEHICLE SIGNAL HEAD "I" SHALL BE EQUIPPED WITH 5-IN NON-LOUVERED BACKPLATES WITH 3-IN REFLECTORIZED YELLOW BORDER.
4. ALL PEDESTRIAN SIGNAL FACES SHALL BE 16-IN L.E.D. ONE-SECTION DUAL-DISPLAY WITH COUNTDOWN EQUIPPED WITH CAP VISORS.

MAJOR ITEM LIST	
QTY.	DESCRIPTION
1	10-FT. ORNAMENTAL TRAFFIC SIGNAL POST, ORNAMENTAL BASE, AND CEM CONC FOUNDATION
11	8-FT ORNAMENTAL TRAFFIC SIGNAL POST, ORNAMENTAL BASE, AND CEM CONC FOUNDATION
12	PEDESTRIAN SIGNAL HEAD (L.E.D. WITH COUNTDOWN DISPLAY)
12	ACCESSIBLE PEDESTRIAN SIGNAL PUSHBUTTON ASSEMBLY (ADA COMPLIANT) INCLUDING SIGN AND SADDLE
1	8"X23" PULL BOX (SD2.030)
12	12"X12" PULL BOX (SD2.031)

PLUS ALL NECESSARY DUCT, CABLE, LABOR, MISCELLANEOUS MATERIALS AND EQUIPMENT TO COMPLETE THE INSTALLATION



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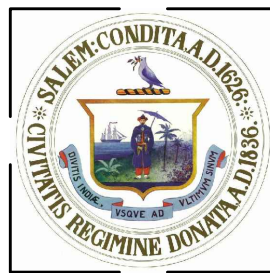
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File Name: dwg_traffic_signal_timing_plan.dwg	Dwn.	Chkd.	Dsgn.	YY.MM.DD
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BRIDGE STREET RECONSTRUCTION PROJECT

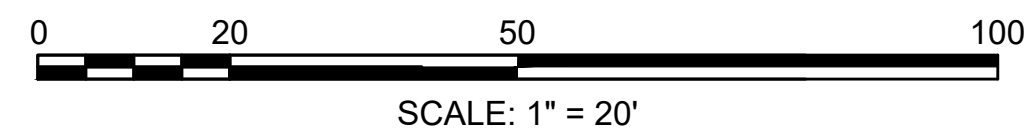
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TRAFFIC SIGNAL TIMING LOCATION 1

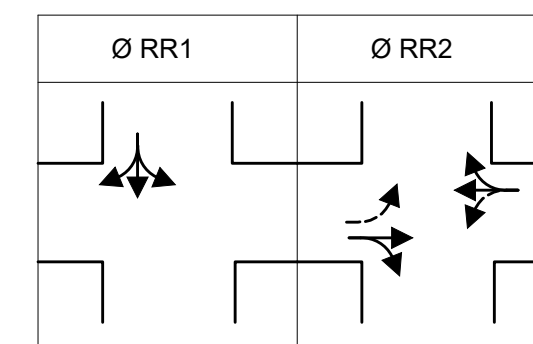
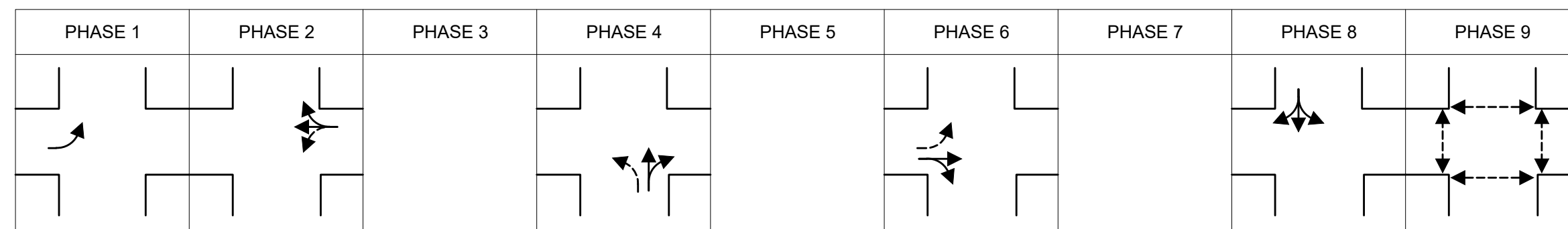
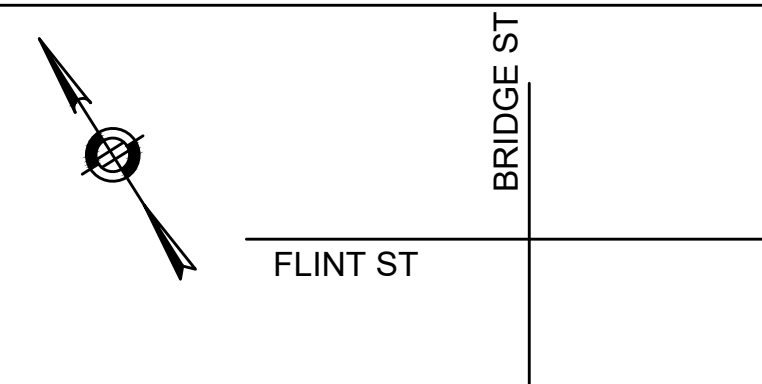
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Project No.	Scale
	50 of 113
Drawing No.	Sheet
	Revision



Drawing No.	Sheet	Revision
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2020/02/05 12:56 PM By: Borenstein, Sarah



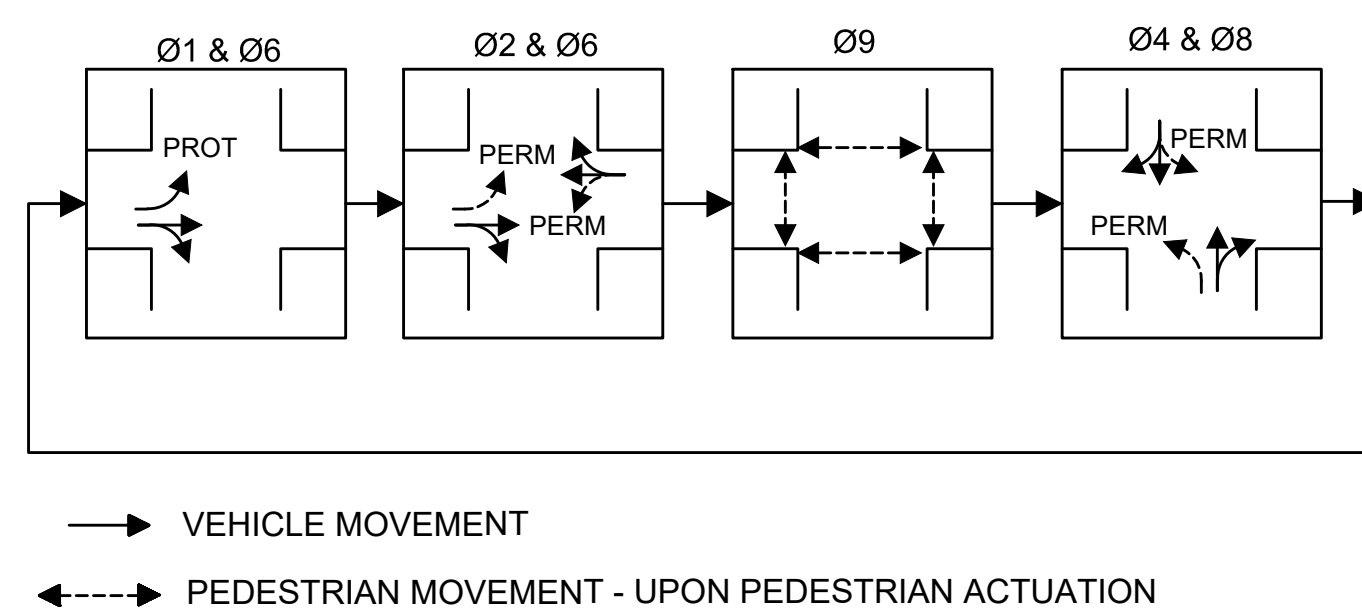
SEQUENCE AND TIMING FOR FULL ACTUATED CONTROL (ISOLATED)																														
STREET	DIRECTION	HOUSINGS	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	FLASH OPER.
FLINT STREET	EB	A	GL	YL	RL	RL	RL	RL				RL	RL	RL				FYL	YL	RL				RL	RL	RL	RL	RL	RL	FRL
FLINT STREET	EB	B,C	R	R	R	R	R	R				R	R	R				G	Y	R				R	R	R	R	R	R	FR
FLINT STREET	WB	D,E	R	R	R	G	Y	R				R	R	R				R	R	R				R	R	R	R	R	R	FR
BRIDGE STREET	NB	J	RL	RL	RL	RL	RL	RL				FYL	YL	RL				RL	RL	RL				RL	RL	RL	RL	RL	RL	FYL
BRIDGE STREET	NB	K,L,M	R	R	R	R	R	R				G	Y	R				R	R	R				R	R	R	R	R	R	FY
BRIDGE STREET	SB	F,G,H	R	R	R	R	R	R				R	R	R				R	R	R				G	Y	R	R	R	R	FY
PEDESTRIAN	ALL	P1-P8	DW	DW	DW	DW	DW	DW				DW	DW	DW				DW	DW	DW				DW	DW	DW	W	FDW	DW	OUT
TIMING IN SECONDS																														
MINIMUM GREEN (INITIAL)			6			8						8						8						8						
PASSAGE TIME (VEHICLE)			2			2						2						2						2						
MAXIMUM 1			18			30						60						30					30							
MAXIMUM 2			8			20						60						20					40							
YELLOW CLEARANCE				3			3.5						4						3						4					
RED CLEARANCE					3			2						1.5						3						1.5				
WALK (W)																										7				
PEDESTRIAN CLEARANCE																												12	3	
RECALL			OFF		OFF						MIN						OFF						MIN				OFF			
MEMORY			NON-LOCK		NON-LOCK						NON-LOCK						NON-LOCK						NON-LOCK				LOCK (PED)			

TECHNICAL NOTES

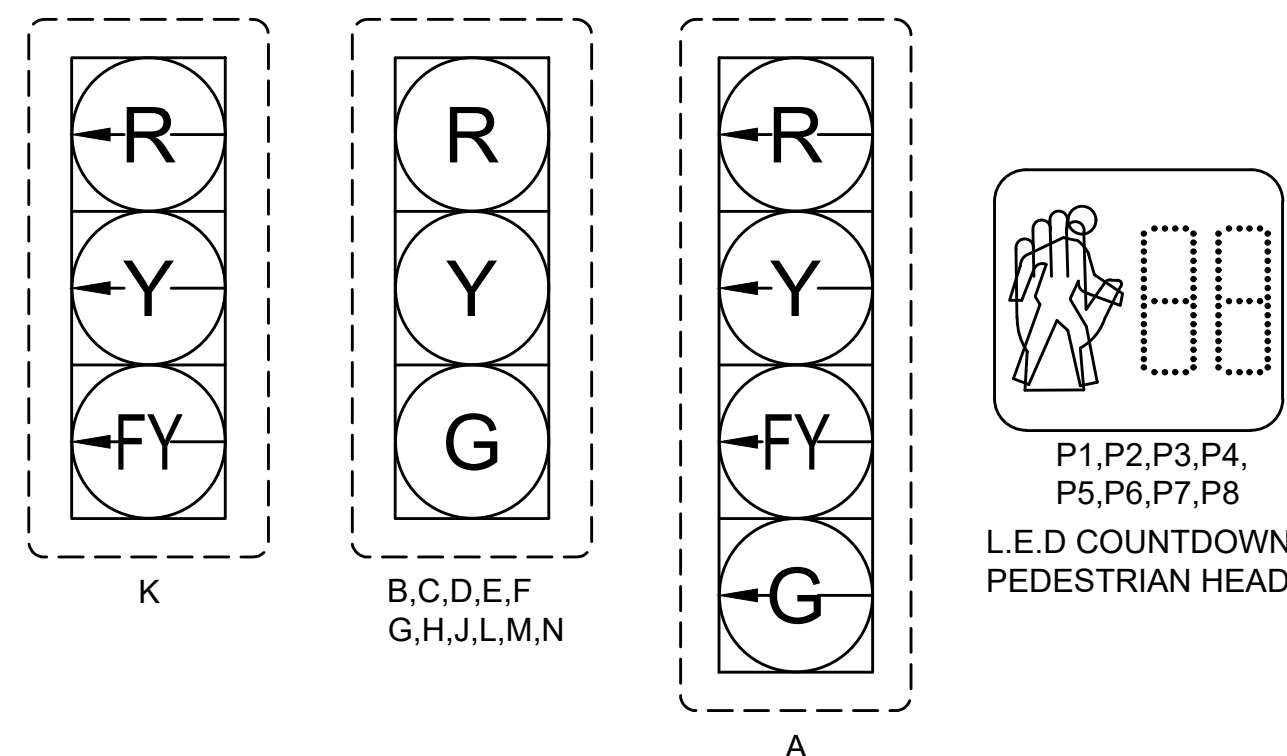
1. ANY PHASE NOT CALLED WILL BE SKIPPED. SIGNAL INDICATION WILL NOT CHANGE IF THE ASSIGNED RIGHT OF WAY DOES NOT CHANGE DURING THE NEXT PHASE CALLED.
2. THE RIGHT-OF-WAY MAY BE ASSIGNED TO ANY PHASE OR ANY COMBINATION OF NON-CONFLICTING PHASES. IF CALLS EXIST ON ALL PHASES, THE RIGHT-OF-WAY SHALL BE ASSIGNED IN ACCORDANCE WITH THE PREFERENTIAL PHASING SEQUENCE.
3. FLASHING OPERATION PER M.U.T.C.D. SECTION 4D.28 THROUGH SECTION 4D.31
4. MAX 1 = 6 AM - 10 AM M-F
MAX 2 = ALL OTHER TIMES
5. THE FLASHING YELLOW ARROW SHALL NOT DISPLAY AT THE SAME TIME AS THE STEADY GREEN ARROW.

EMERGENCY PREEMPTION SCHEDULE			
APPROACH	CHANNEL	PREEMPTION PHASE	NEXT PHASE CALLED
EASTBOUND	F1	1 & 6	2 & 6
WESTBOUND	F2	2 & 6	1 & 6
SOUTHBOUND	F3	8	4 & 8
NORTHBOUND	F4	4	4 & 8

PREFERENTIAL PHASING SEQUENCE



SIGNAL FACES



NOTES:

1. ALL VEHICLE SIGNAL FACES SHALL BE 12-IN L.E.D. EQUIPPED WITH CAP VISORS.
2. ALL VEHICLE SIGNAL FACE ASSEMBLIES SHALL BE BE EQUIPPED WITH 5-IN NON-LOUVERED BACKPLATES WITH 3-IN REFLECTORIZED YELLOW BORDER.
3. ALL PEDESTRIAN SIGNAL FACES SHALL BE 16-IN L.E.D. ONE-SECTION DUAL-DISPLAY WITH COUNTDOWN EQUIPPED WITH CAP VISORS.

RAILROAD PRE-EMPTION PHASING AND PRIORITY NOTES:

1. NORMAL VEHICLE AND PEDESTRIAN CLEARANCE SHALL BE PROVIDED ON PHASES THAT ARE TO BE TERMINATED BY RAILROAD PRE-EMPTION DEMAND.
2. IN RESPONSE TO A RAILROAD PRE-EMPTION SIGNAL RECEIVED AT THE INTERSECTION, THE CONTROLLER SHALL HOLD OR ADVANCE TO AND HOLD RAILROAD PRE-EMPTION PHASE RR1 UNTIL MAX OUT.
3. THE RAILROAD CROSSING WARNING LIGHTS SHALL BEGIN TO FLASH AFTER RAILROAD PRE-EMPTION PHASE RR1 MAX OUT
4. THE CONTROLLER SHALL THEN SERVICE RAILROAD PRE-EMPTION PHASE RR2 UNTIL THE RAIL ROAD PRE-EMPTION SIGNAL CEASES
5. THE TRAIN OPERATOR WILL WAIT 20 SECONDS AFTER THE BEGINNING OF THE RAILROAD CROSSING WARNING LIGHT SEQUENCE BEFORE ADVANCING THE TRAIN ONTO THE AT-GRADE CROSSING.
6. NORMAL TRAFFIC SIGNAL OPERATIONS SHALL RESUME, ONCE THE RAILROAD PRE-EMPTION SIGNAL CEASES.
7. THE CONTRACTOR, IN CONSULTATION WITH THE ENGINEER, SHALL WORK WITH THE RAILROAD (PAN AM RAILWAYS) TO ENSURE PROPER OPERATION (TIMING/PHASING/PRE-EMPTION) OF THE COORDINATION BETWEEN THE TRAFFIC SIGNAL AND THE RAILROAD CROSSING.

MAJOR ITEM LIST	
QTY.	DESCRIPTION
1	TRAFFIC SIGNAL CONTROLLER (TS2, TYPE 1) TYPE 8DW W/ 8DW CABINET AND CEM CONC FOUNDATION
1	MALFUNCTION MANAGEMENT UNIT (CAPABLE OF FLASHING YELLOW ARROW)
2	20-FT ORNAMENTAL MAST ARM (STEEL), BASE, AND CEM CONC FOUNDATION
1	35-FT ORNAMENTAL MAST ARM (STEEL) WITH R10-12a SIGN, R10-11b SIGN, BASE, AND CEM CONC FOUNDATION
1	30-FT ORNAMENTAL MAST ARM (STEEL) WITH R10-12a SIGN, BASE, AND CEM CONC FOUNDATION
3	10-FT ORNAMENTAL TRAFFIC SIGNAL POST, ORNAMENTAL BASE, AND CEM CONC FOUNDATION
3	8-FT ORNAMENTAL TRAFFIC SIGNAL POST, ORNAMENTAL BASE, AND CEM CONC FOUNDATION
12	1-WAY 3-SECTION SIGNAL HEAD, 12-IN LED LENS (W/VISORS)
1	1-WAY 4-SECTION SIGNAL HEAD, 12-IN LED LENS (W/VISORS)
13	5-IN NON-LOUVERED BACKPLATE WITH 3-IN RETROREFLECTIVE YELLOW BORDER
8	PEDESTRIAN SIGNAL HEAD (L.E.D. WITH COUNTDOWN DISPLAY)
8	ACCESSIBLE PEDESTRIAN SIGNAL PUSHBUTTON ASSEMBLY (ADA COMPLIANT) INCLUDING SIGN AND SADDLE
1	EMERGENCY PRE-EMPTION CONFIRMATION STROBE
4	EMERGENCY PRE-EMPTION RECEIVER
1	EMERGENCY PRE-EMPTION PHASE SELECTOR
1	VIDEO DETECTION SYSTEM
1	ELECTRICAL SERVICE CONNECTION (UNDERGROUND)
3	12"X12" PULL BOX (SD2.031)
3	8"X23" PULL BOX (SD2.030)

PLUS ALL NECESSARY DUCT, CABLE, LABOR, MISCELLANEOUS MATERIALS AND EQUIPMENT TO COMPLETE THE INSTALLATION

VIDEO DETECTOR OPERATION								
DETECT. NO.	ZONE SIZE	PROC. NO.	CHAN. NO.	Ø CALL.	Ø EXTEN.	MODE: A=PULSE B=PRES. C=CALL.	DELAY (SEC)	EXTEN (SEC)
1	6' X 54'	1	1	1	1	B	0	0
2	6' X 54'	1	2	6	6	B	0	0
3	10' X 54'	2	1	2	2	B	0	0
4	6' X 54'	2	2	4	4	B	0	0
5	6' X 54'	3	1	4	4	B	0	0
6	8' X 54'	3	2	8	8	B	0	0



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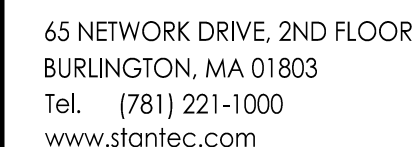
Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title	TRAFFIC SIGNAL TIMING LOCATION 2
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179410455	NOT TO SCALE
Project No.	Scale

Drawing No.	Sheet	Revision
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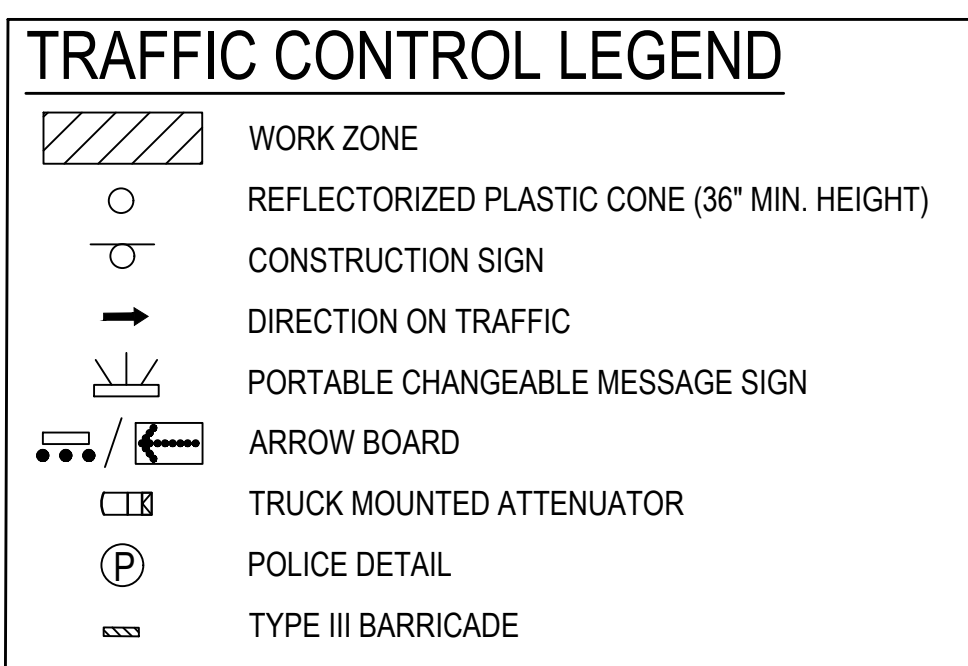
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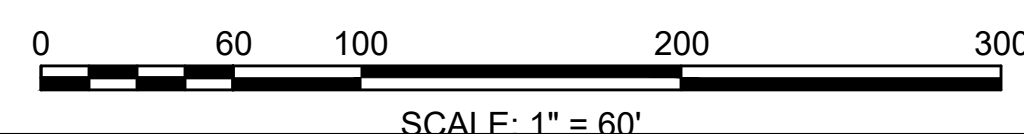
Title	TEMPORARY TRAFFIC CONTROL PLANS STAGE 1
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179410455	1" = 60'	
Project No.	Scale	
	53	of 113
Drawing No.	Sheet	Revision



NOTE:

1. FOR ADDITIONAL TYPICAL TRAFFIC MANAGEMENT DETAILS, LEGEND, AND GENERAL NOTES, SEE THE TEMPORARY TRAFFIC CONTROL PLAN TYPICAL DETAILS PARTS 1 AND 2. DRAWINGS NO'S X & X





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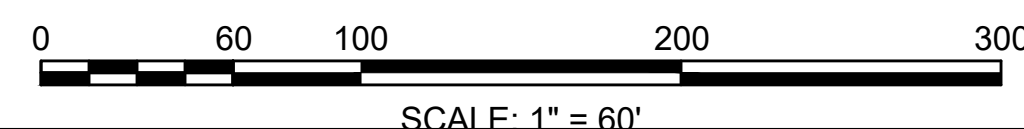
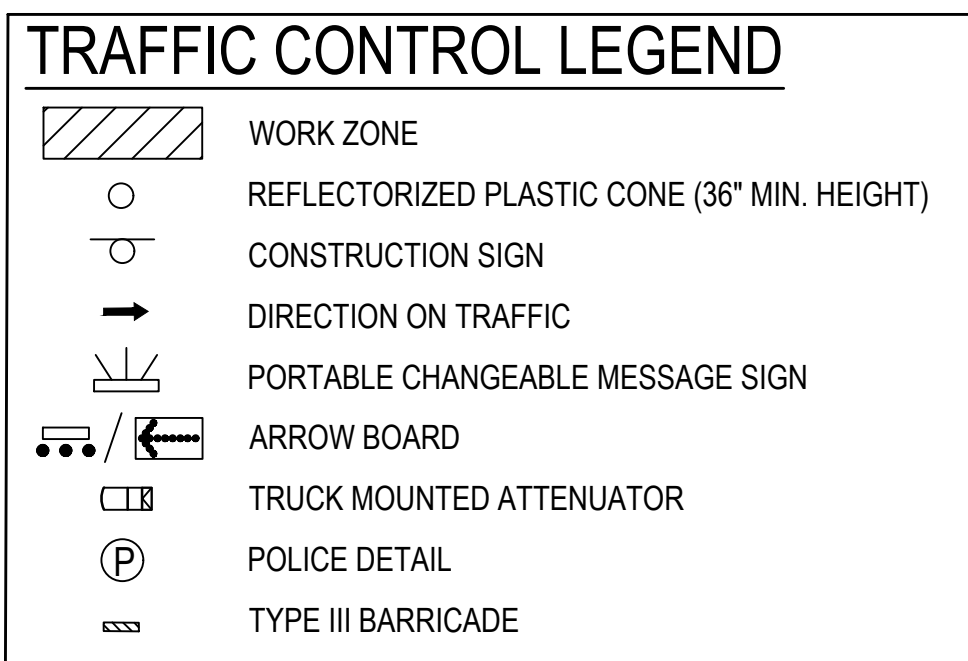
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BRIDGE STREET RECONSTRUCTION PROJECT

Title
TEMPORARY TRAFFIC CONTROL PLANS STAGE 2

179410455	1" = 60'	
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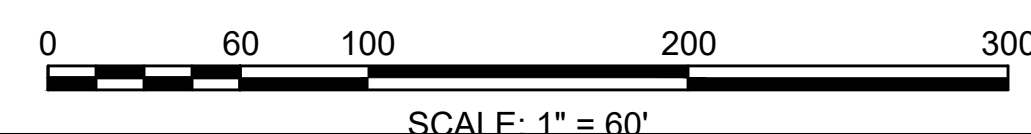
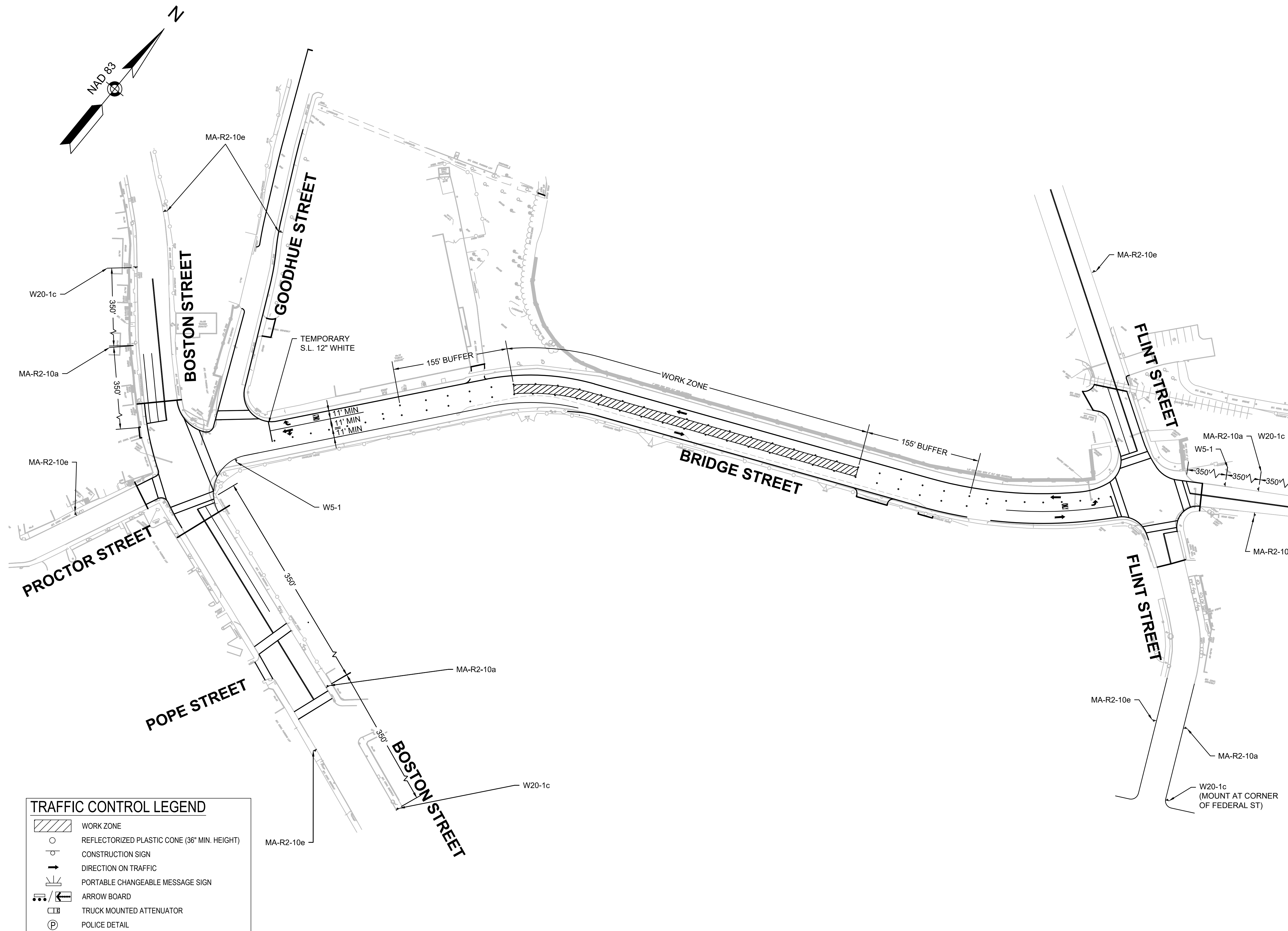
BRIDGE STREET RECONSTRUCTION PROJECT

Title

TEMPORARY TRAFFIC CONTROL PLANS STAGE 3

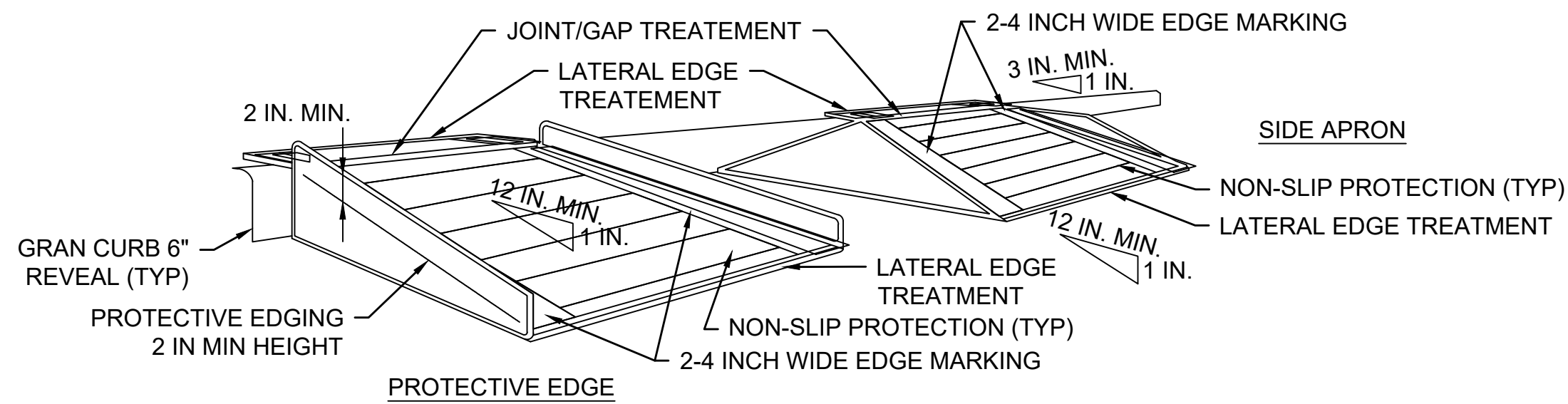
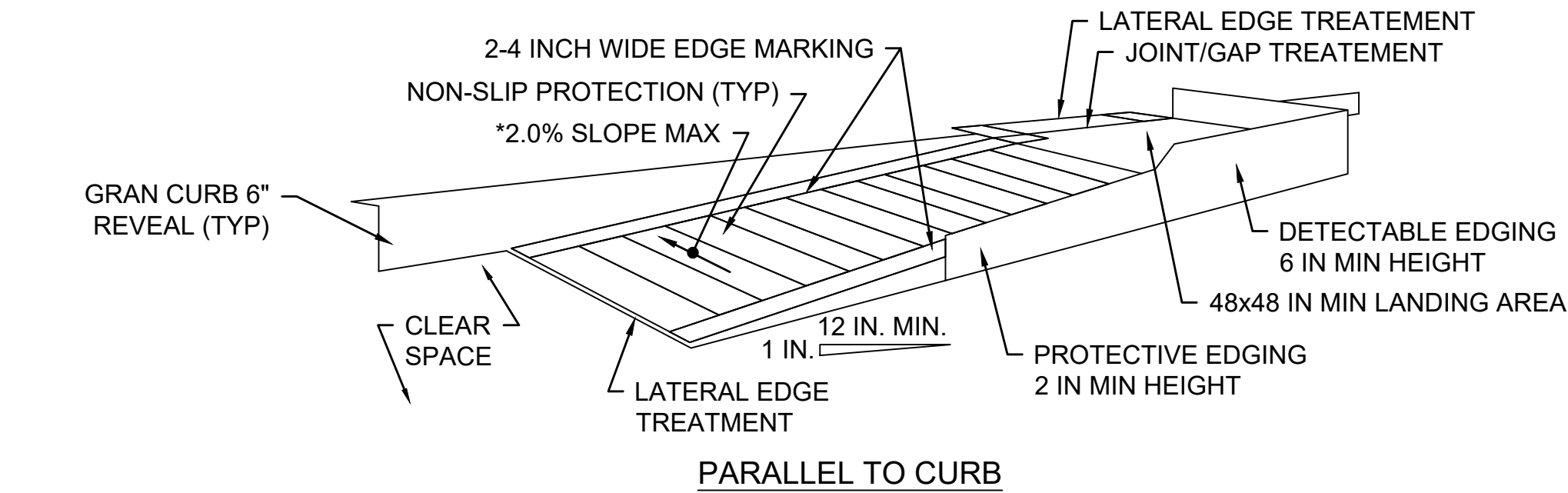
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Project No.	Scale

	55	of	113
Drawing No.	Sheet		Revision

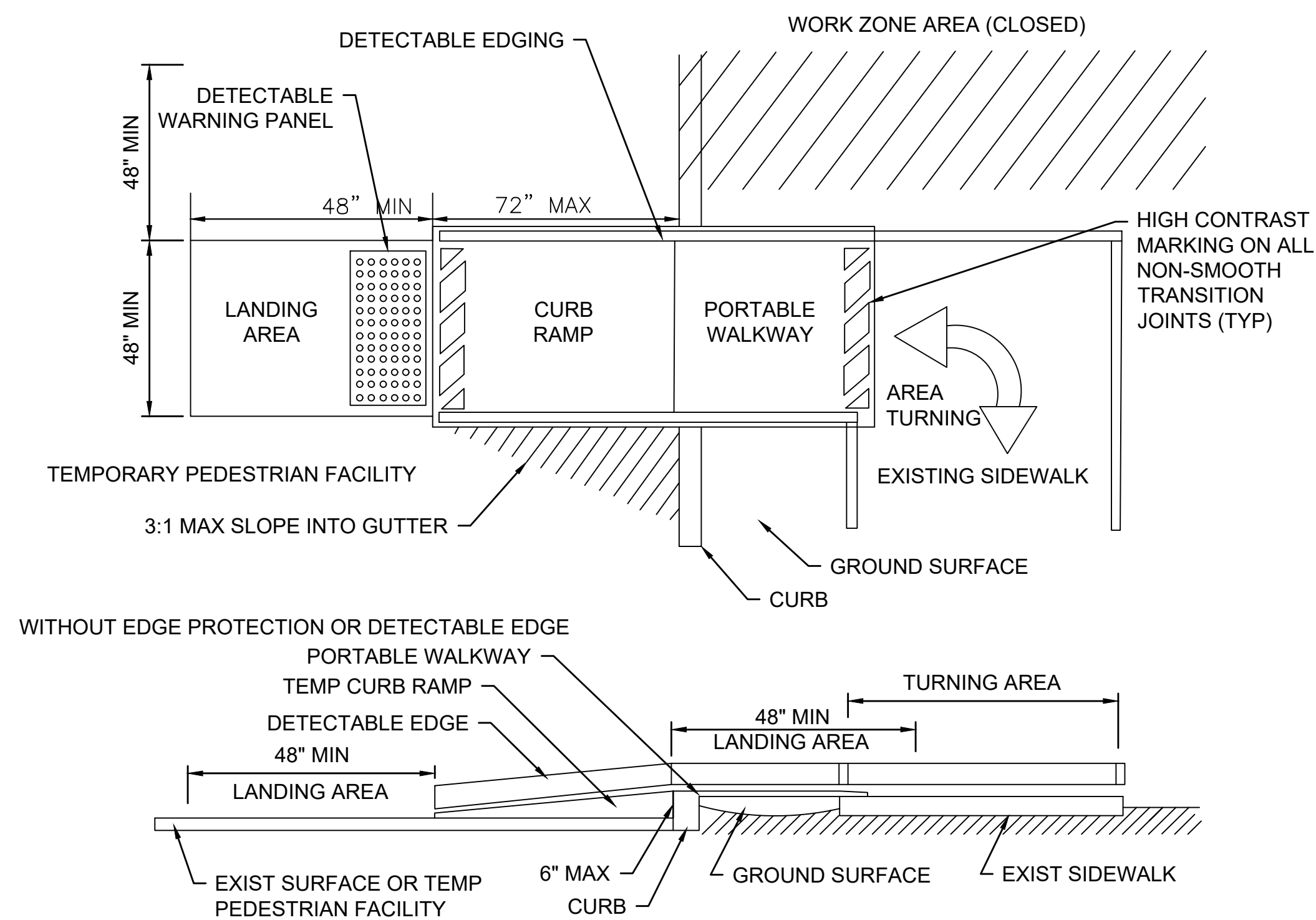


1. CURB RAMPS SHALL BE 60 IN. MINIMUM WIDTH WITH A FIRM, STABLE AND NON-SLIP SURFACE.
2. PROTECTIVE EDGING WITH A 2 IN. MINIMUM HEIGHT SHALL BE INSTALLED WHEN THE CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6 IN. OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN THE CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3 IN. OR MORE.
3. DETECTABLE EDGING WITH 6 IN. MINIMUM HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
4. CURB RAMPS AND LANDINGS SHOULD HAVE A 1:50 (2%)

5. CLEAR SPACE OF 48x48 IN. MINIMUM SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
6. THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A CONTRASTING COLOR 2 TO 4 IN. WIDE MARKING. THE MARKING IS OPTIONAL WHERE COLOR CONTRASTING EDDING IS USED.
7. WATER FLOW IN THE GUTTER SYSTEM SHALL HAVE MINIMAL RESTRICTION.
8. LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 0.5 IN. WIDTH.
9. CHANGES BETWEEN SURFACE HEIGHTS SHOULD NOT EXCEED 0.5 IN. LATERAL EDGES SHOULD BE VERTICAL UP TO 0.25 IN. HIGH, AND BEVELED AT 1:2 BETWEEN 0.25 IN. AND 0.5 IN. HEIGHT.

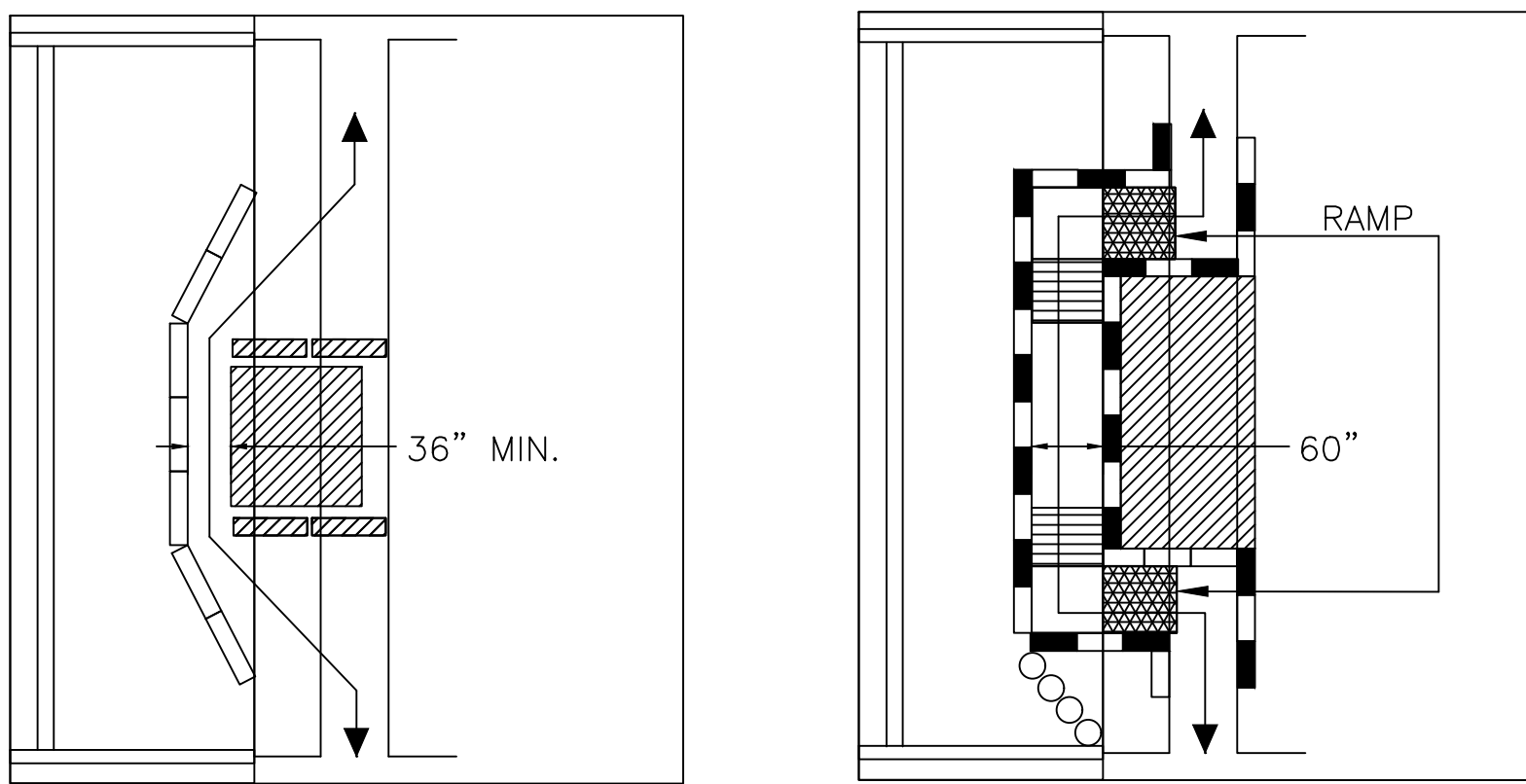


PERPENDICULAR TO CURB
TEMPORARY CURB RAMP
 NOT TO SCALE



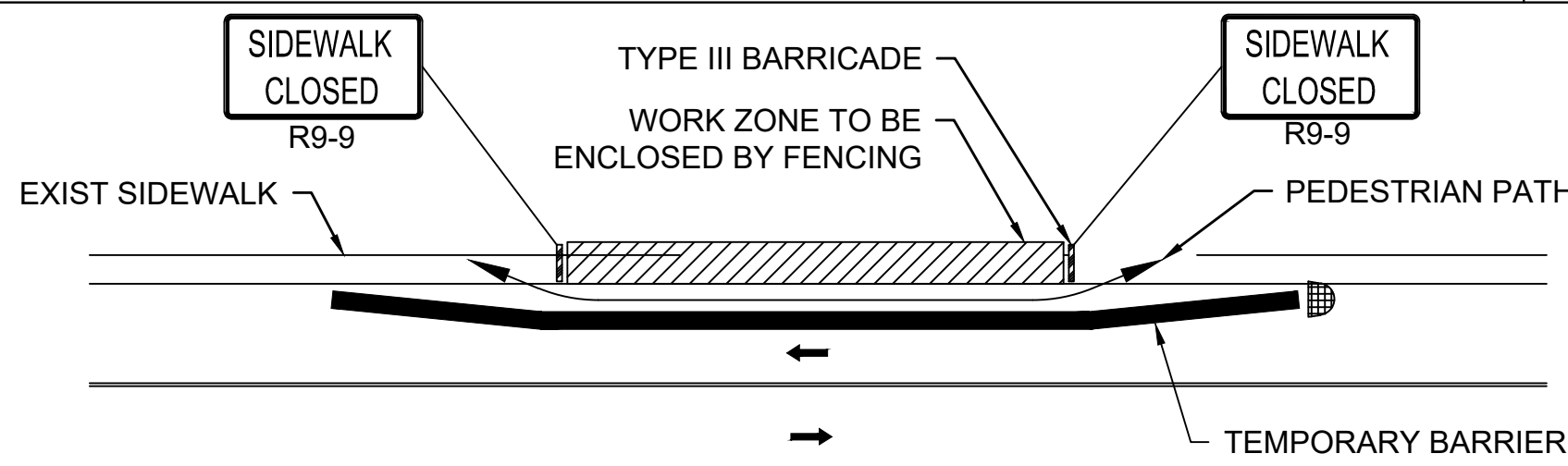
TEMPORARY CURB RAMP-TYPE 2

NOT TO SCALE



- When existing pedestrian facilities are disrupted, closed, or relocated in a TTC zone, temporary facilities shall be provided and they shall be detectable and include accessibility features consistent with the features present in the existing pedestrian facility.
- A pedestrian channelizing device that is detectable by a person with a visual disability traveling with the aid of a long cane shall be placed across the full width of the closed sidewalk.
- When used, temporary ramps shall comply with Americans with Disabilities Act (see Temporary Curb Ramp Detail, this sheet).
- The alternate pathway should have a smooth continuous hard surface for the entire length of the temporary pedestrian facility.
- The protective requirements of a TTC situation have priority in determining the need for temporary traffic barriers and their use in this situation should be based on engineering judgment.
- Audible information devices should be considered where midblock closings and changed crosswalk areas cause inadequate communication to be provided to pedestrians who have visual disabilities.

For long term sidewalk closures (at a minimum overnight) a form of speech messaging for pedestrians with visual disabilities shall be provided. Audible information devices such as detectable barriers or barricades and other passive pedestrian activation (motion activated) devices should be considered for these cases. These audible devices can be mountable or stand alone.

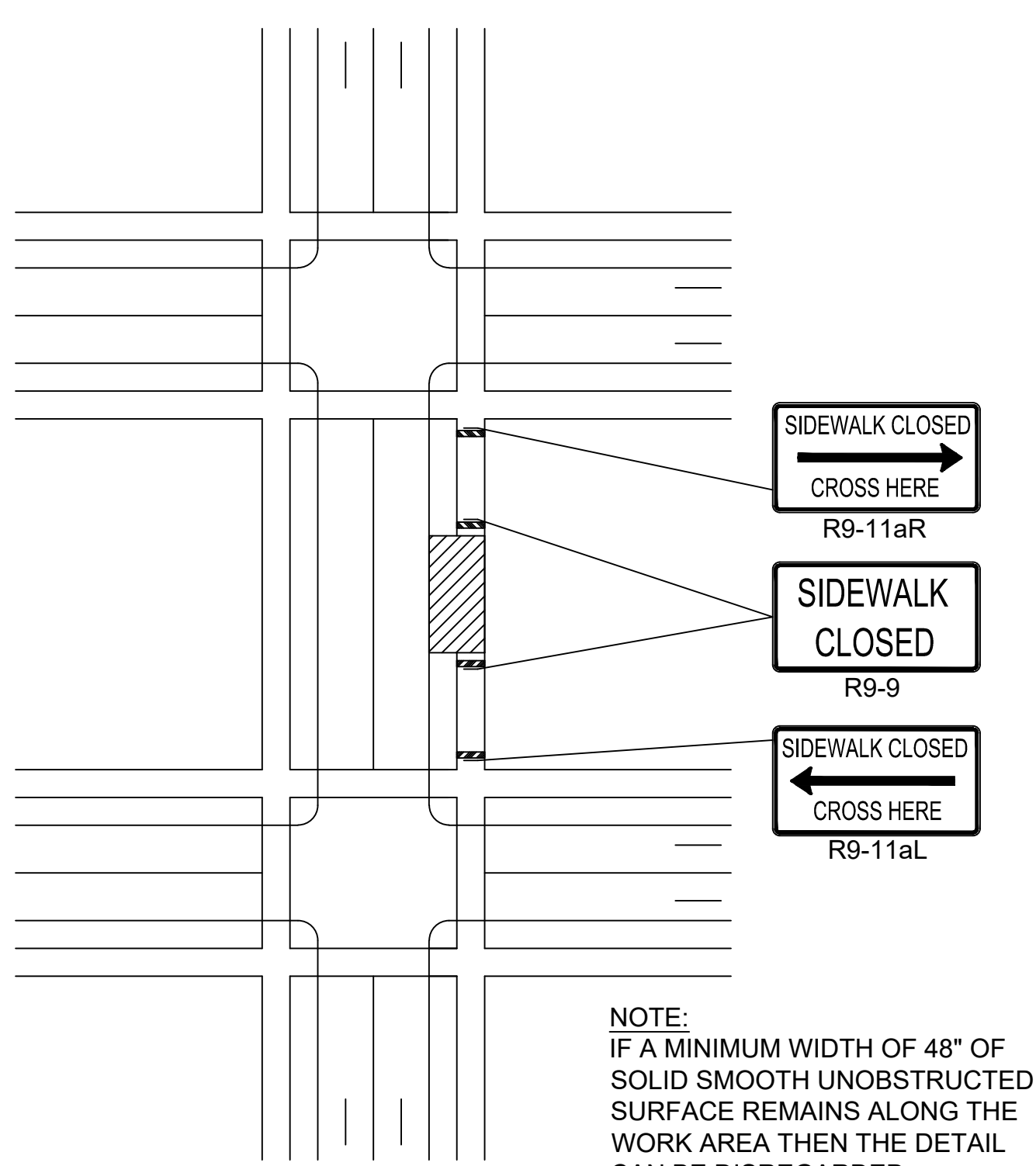


1. ADDITIONAL ADVANCE WARNING MAY BE NECESSARY.
2. CONTROLS ONLY FOR PEDESTRIAN TRAFFIC ARE SHOWN. VEHICULAR TRAFFIC SHOULD BE HANDLED AS SHOWN ELSEWHERE.
3. STREET LIGHTING SHOULD BE CONSIDERED WHEN LOCATING CONTROL DEVICES.
4. BYPASS IS TO BE USED IN CONJUNCTION WITH THE PROPOSED LANE CLOSURE DETAILS AND DURING CONSTRUCTION STAGING, AS REQUIRED BY THE ENGINEER.
5. THE TEMPORARY SIDEWALK SHOULD BE A MINIMUM OF 4 FEET WIDE. IF THIS WALKWAY EXCEEDS 200 FEET IN LENGTH THEN A 5 FOOT X 5 FOOT PASSING ZONE SHALL BE PROVIDED. (FOR SHORT TERM SETUPS < 10 HOURS, THIS CONDITION MAY BE WAIVED. A NOTE WOULD NEED TO BE INCLUDED IN THE TTCP THAT STATES HOW THE CONTRACTOR SHOULD ADDRESS THIS ISSUE.)
6. JERSEY OR F-TYPE BARRIERS ARE NOT AN ACCEPTABLE PEDESTRIAN CHANNELIZING DEVICE
7. PROVIDE TEMPORARY RAMPS TO ALLOW TRAVEL TO/FROM WALKWAY ON ROADWAY.

The diagram shows two vertical components relative to a horizontal ground line. The upper component is labeled "HAND-TRAILING EDGE" and has a minimum clearance of "32\" MIN." from the ground. The lower component is labeled "DETECTION PLATE" and has a maximum clearance of "2\" GAP MAX.*" from the ground. A horizontal dimension of "2\" GAP MIN.*" is shown between the two components.

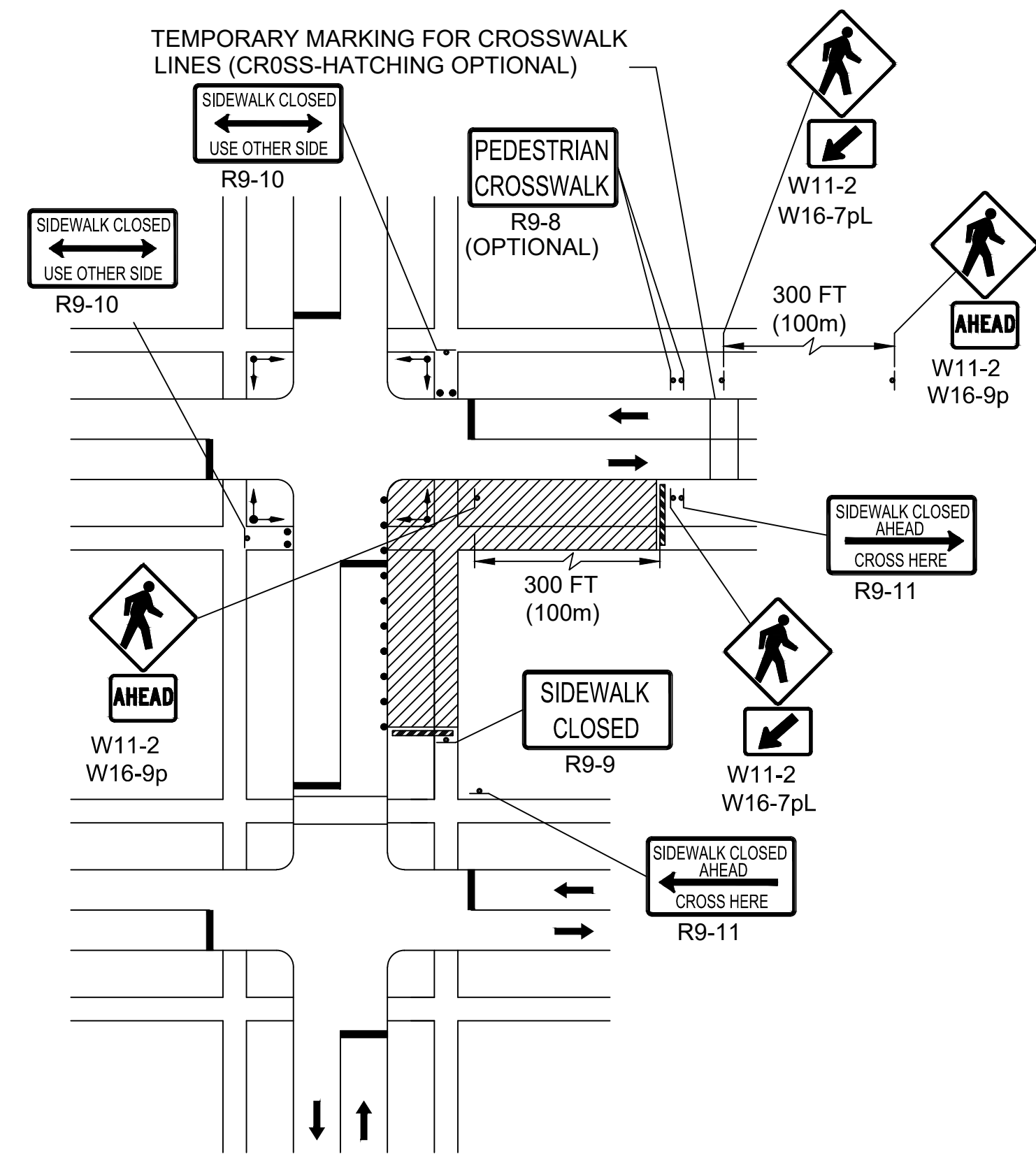
NOTES: * THERE SHALL BE A 2 INCH GAP BETWEEN THE HAND-TRAILING EDGE AND ITS SUPPORT.
 **A MAXIMUM 2 INCH GAP BETWEEN THE BOTTOM OF THE BOTTOM RAIL AND THE SURFACE MAY BE USED TO PROVIDE DRAINAGE.

PEDESTRIAN CHANNELIZING DEVICE
NOT TO SCALE



SIDEWALK CLOSURE

NOT TO SCALE



NOTE:
FOR LONG-TERM STATIONARY WORK, THE DOUBLE
YELLOW CENTERLINE AND/OR LANE LINES SHOULD BE
REMOVED BETWEEN THE CROSSWALK LINES.

PEDESTRIAN DETOUR PLAN

NOT TO SCALE



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Notes
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File Name: dwg_traffic_ttcp_typicals.dwg				
	Dwn.	Chkd.	Dsgn.	YY.MM.DD

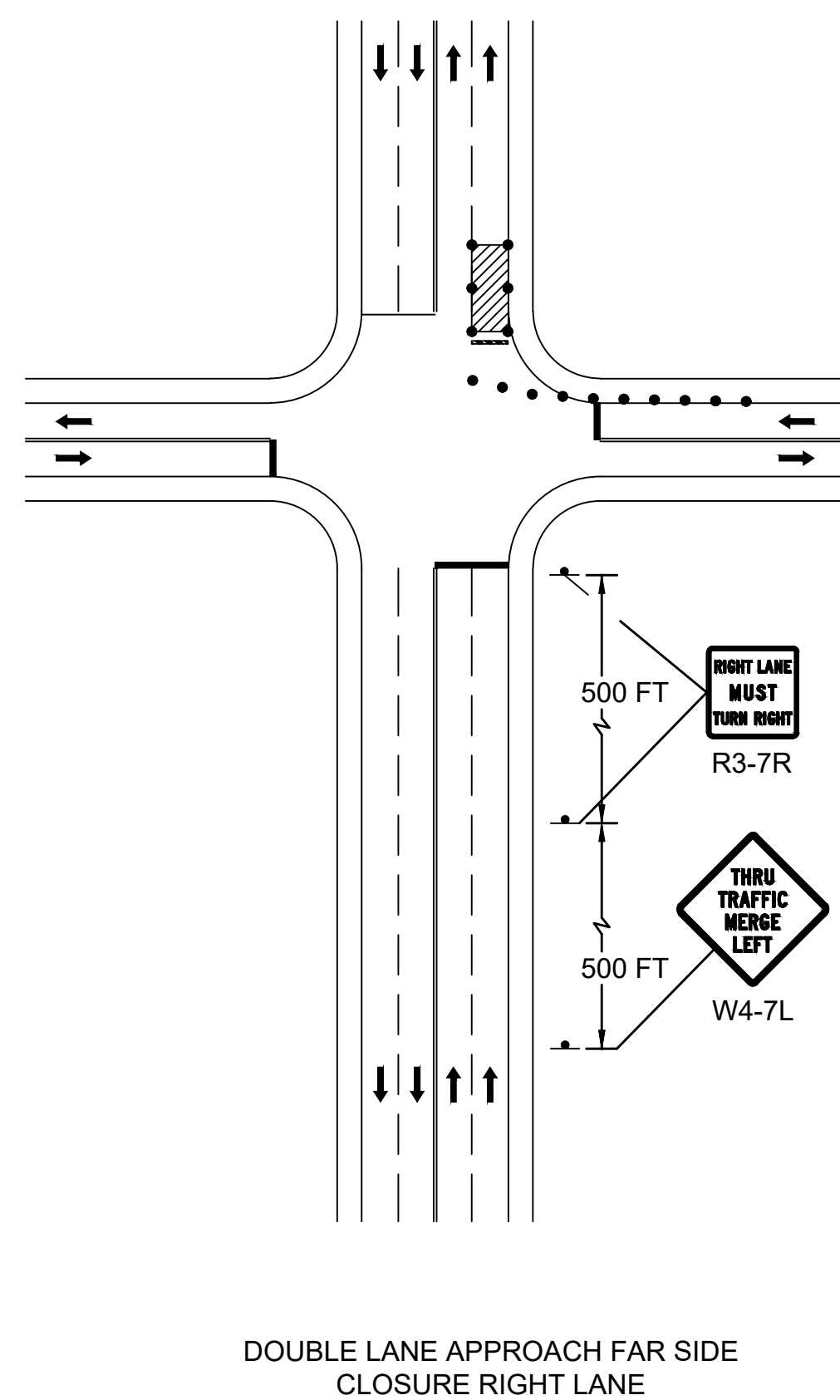
The seal of the Municipality of Salsomaggiore is circular. It features a central shield depicting a figure holding a staff, with a ship and a lighthouse in the background. The shield is flanked by two olive branches. Above the shield is a crown. The text around the border reads "SALEM CONDITA A.D. 1626" at the top and "CIVITAS REGIMINE DONATA A.D. 1836" at the bottom.

BRIDGE STREET RECONSTRUCTION PROJECT

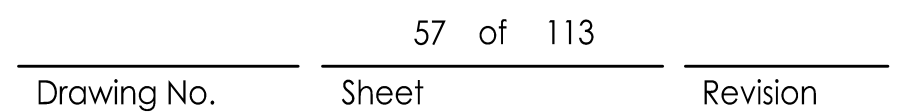
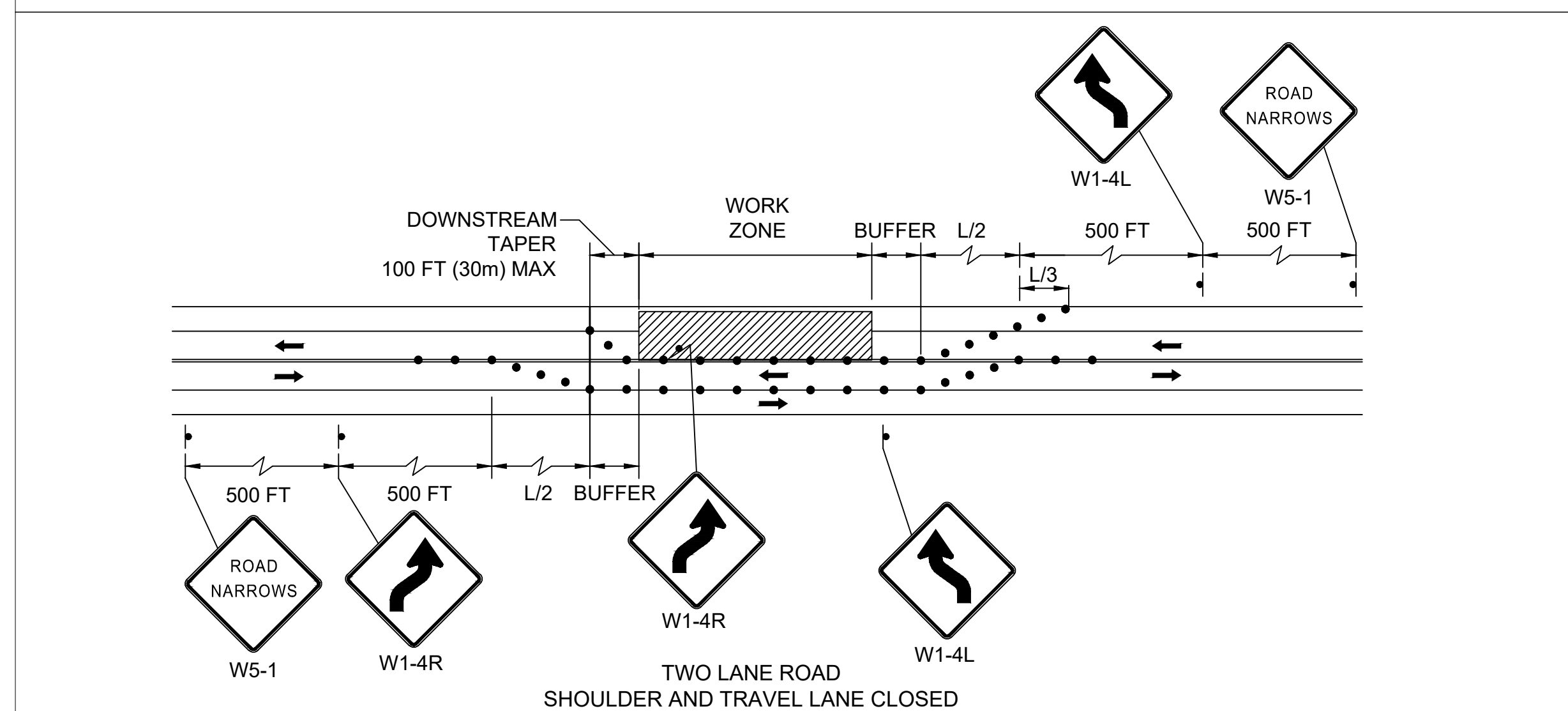
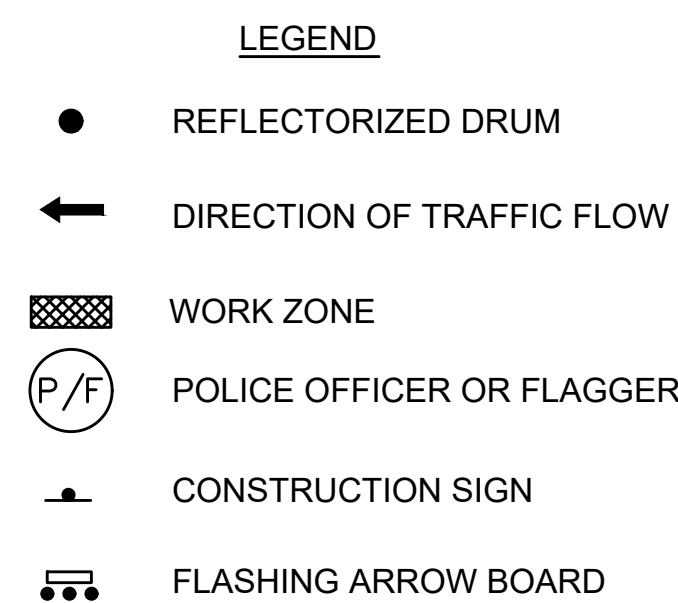
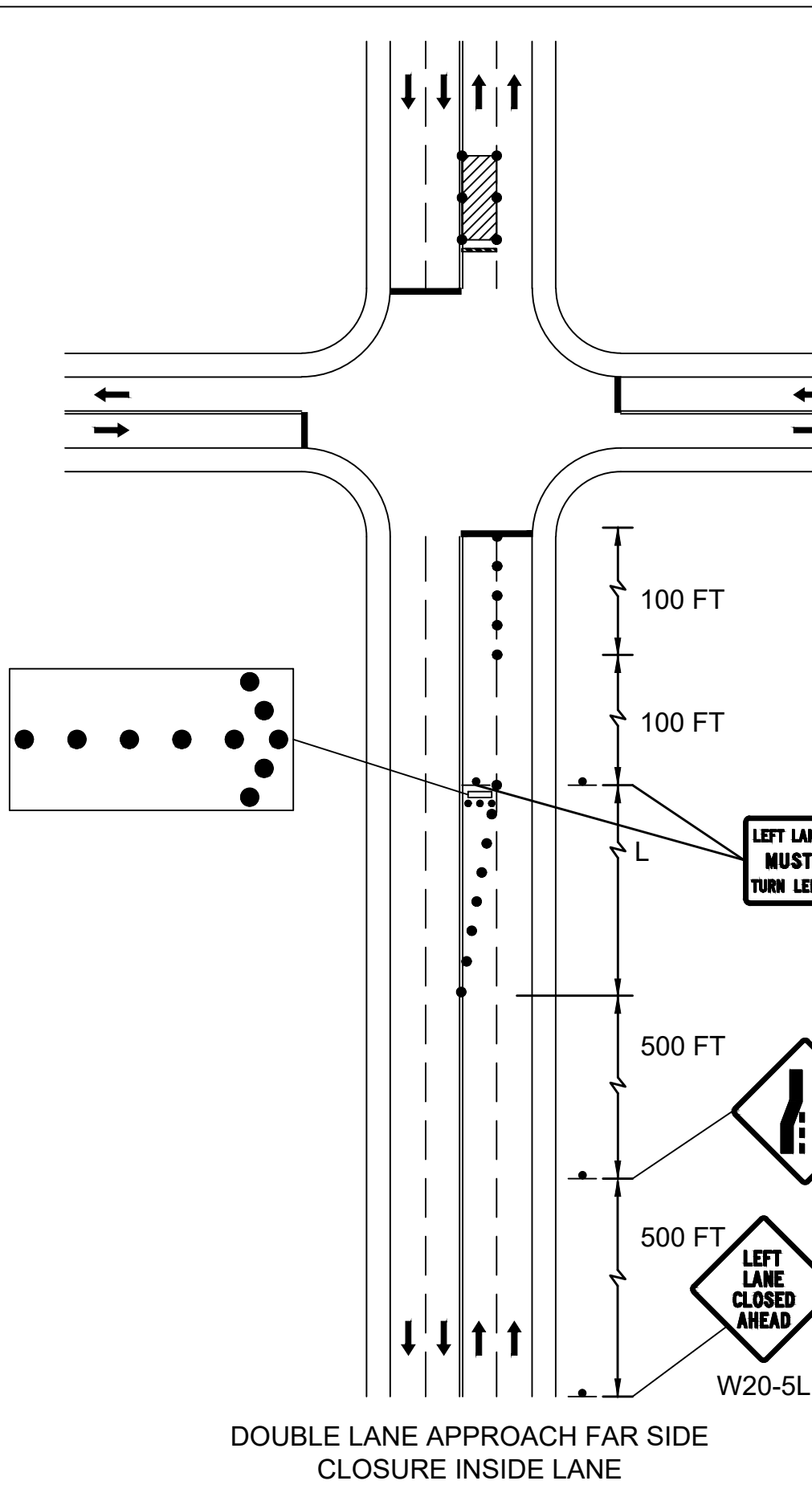
Title	TEMPORARY TRAFFIC CONTROL PLAN TYPICAL DETAILS PART 1 OF 2
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179410455	NOT TO SCALE	
Project No.	Scale	
	56 of 113	
Drawing No.	Sheet	Revision

\\US152-F01\SHARED_PROJECTS\179410455\transportation\drawing\dwg_traffic_tcp_typicals.dwg
2020/02/05 12:57 PM Bv: Borenstein, Sarah



- | SPEED LIMIT (S) | TAPER LENGTH (L) |
|-----------------|-----------------------|
| 40 MPH OR LESS | $L = \frac{WS^2}{60}$ |
| 45 MPH OR MORE | $L = WS$ |



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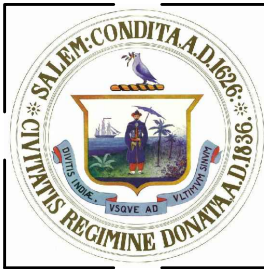
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File Name:	dwg_construction_sign_summary.dwg	Dwn.	Chkd.	Dsgn.	YY.MM.DD
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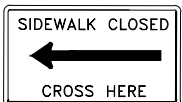

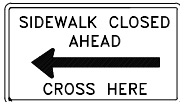
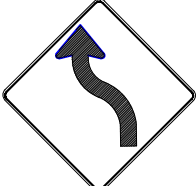
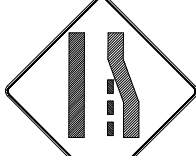




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BRIDGE STREET RECONSTRUCTION PROJECT

Title
TEMPORARY TRAFFIC CONTROL PLAN
CONSTRUCTION SIGN SUMMARY

179410455	
Project No.	Scale
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IDENTIFI- CATION NUMBER	SIZE OF SIGN		SIGN	LEGEND DIMENSIONS			NUMBER OF SIGNS REQUIRED	COLOR			SUPPORT AND NUMBER REQUIRED	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW		BACK- GROUND	LEGEND	BORDER		
MA-R2-10a	48"	36"		SEE MASSDOT STDS.	SEE MASSDOT STDS.	SEE MASSDOT STDS.	5	SEE MASSDOT STDS.	SEE MASSDOT STDS.	SEE MASSDOT STDS.		60.00
MA-R2-10e	36"	48"		SEE MASSDOT STDS.	SEE MASSDOT STDS.	SEE MASSDOT STDS.	7	SEE MASSDOT STDS.	SEE MASSDOT STDS.	SEE MASSDOT STDS.		84.00
MA-W20-7b	24"	18"		SEE MASSDOT STDS.	SEE MASSDOT STDS.	SEE MASSDOT STDS.	2	SEE MASSDOT STDS.	SEE MASSDOT STDS.	SEE MASSDOT STDS.		6.00
R4-7c	18"	30"		SEE MUTCD	SEE MUTCD	SEE MUTCD	1	SEE MUTCD	SEE MUTCD	SEE MUTCD		3.75
R9-9	24"	12"					1					2.00
R9-11aR	24"	18"					2					6.00
R9-11aL	24"	18"					3					9.00
R9-11R	24"	24"					1					4.00
R9-11L	24"	12"					1					2.00
W1-4L	36"	36"					1					9.00
W4-2R	36"	36"					1					9.00
W5-1	36"	36"					3					27.00
W20-1c	36"	36"					5					45.00
W20-5cR	36"	36"					1					9.00
W30-8R	36"	36"		▼	▼	▼	1	▼	▼	▼		9.00

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BRIDGE STREET RECONSTRUCTION PROJECT

Title

LANDSCAPING PLAN PART 1 OF 5

179410455

Project No.

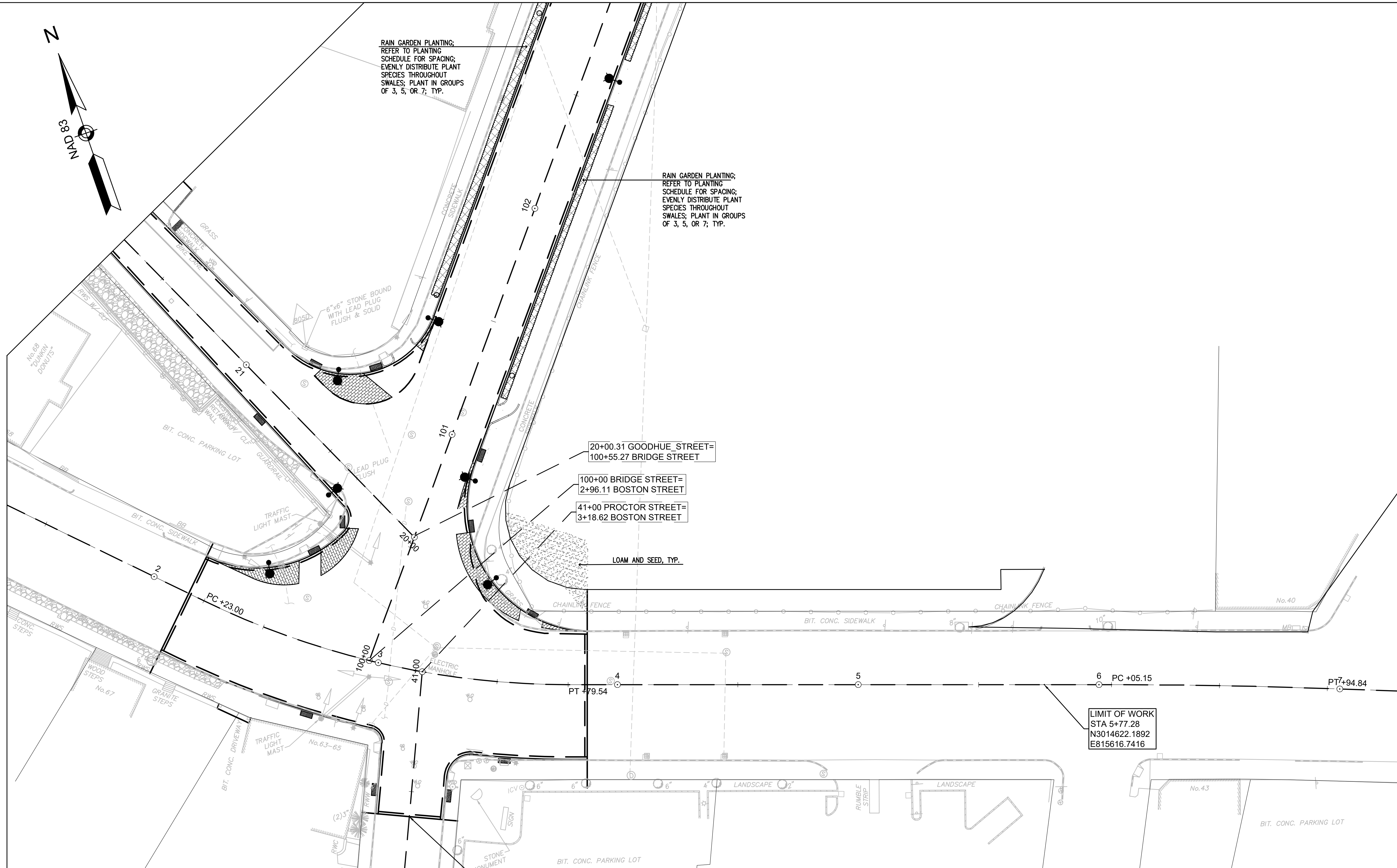
Drawing No.

1" = 20'

Scale

Sheet

Revision



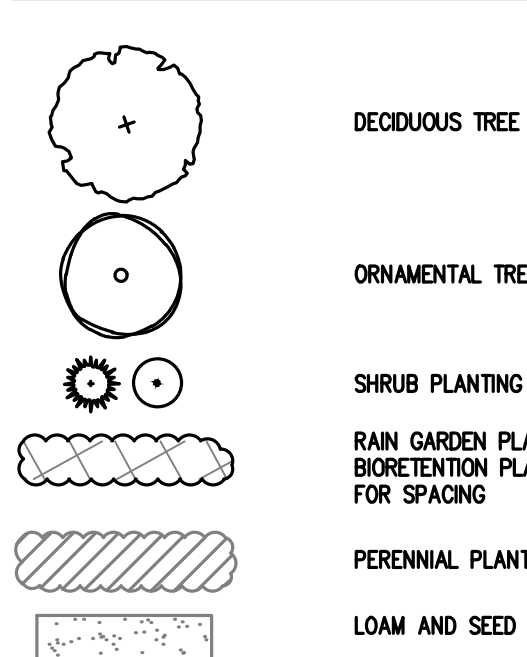
PLANT SCHEDULE

SYMBOL	BOTANICAL NAME	COMMON NAME	QTY	SIZE	COMMENT
DB	QUERCUS BICOLOR	SWAMP WHITE OAK	7	3 - 3 1/2" GAL.	
OC	CERIS CANADENSIS	EASTERN REDBUD	5	3 - 3 1/2" GAL.	
IC	ILEX GLABRA 'COMPACTA'	COMPACT HIBERNY	52	24"-30" HT.	
IV	ILEX VERTICILLATA 'RED SPRITE'	RED SPRITE WINTERBERRY	101	#7	9 SHALL BE MALE
AM	ARONIA MELANOCARPA 'MORTON'	MORTON BLACK CHOKEBERRY	51	#2	
CA	CLETHRA ALNIFOLIA 'SIXTEEN CANDLES'	SUMMERSWEET 'SIXTEEN CANDLES'	51	#7	
RA	RHUS AROMATICA 'GROW LOW'	AROMATIC GROW LOW SUMAC	119	#5	
PV	PANICUM VIRGATUM	SWITCH GRASS	29	#2	

RAIN GARDEN PLANT SCHEDULE

SYMBOL	BOTANICAL NAME	COMMON NAME	QTY	SIZE	COMMENT
MIXED PERENNIALS					
ACHILLEA MILLEFOLIUM 'APPLERBLOSSOM'	APPLERBLOSSOM YARROW	230	1 GAL.	18" O.C.	
ASTER NOVAE-ANGIAE 'PURPLE DOME'	NEW ENGLAND ASTER	230	1 GAL.	18" O.C.	
ECHINACEA PURPUREA 'WHITE SWAN'	PURPLE CONEFLOWER	230	1 GAL.	18" O.C.	
ECHINACEA P. 'KIM'S KNEE HIGH'	PURPLE CONEFLOWER	230	1 GAL.	18" O.C.	
EUPATORIUM D. 'BABY JOE'	DWARF JOE PYE WEED	230	1 GAL.	18" O.C.	
NEPETA 'WALKERS LOW'	CATMINT	230	1 GAL.	18" O.C.	
PENNSETUM ALOPECUROIDES 'HAMELI'	FOUNTAIN GRASS	230	2 GAL.	18" O.C.	
RUDBECKIA FULGIDA 'GOLDSTURM'	BLACK EYED SUSAN	230	2 GAL.	18" O.C.	
SCHIZACHYRIUM SCORPARIUM 'THE BLUES'	LITTLE BLUESTEM	230	2 GAL.	18" O.C.	
SEDUM 'AUTUMN JOY'	STONECROP	230	1 GAL.	18" O.C.	
SEDUM EMERSII, TETRACITUM, RUPESTRE	MIXED CREEPING SEDUM	22	FLATS	LOCATED ON SITE WITH L.A.	
HEMEROCALLIS 'HAPPY RETURNS'	HAPPY RETURNS DAYLILIES	230	#5 POT	18" O.C.	
IRIS VERSICOLOR	BLUE FLAG IRIS	230	1 GAL.	18" O.C.	
OSMUNDA REGALIS	ROYAL FERN	230	#1 POT	18" O.C.	

LEGEND



PLANTING NOTES

- THE LOCATIONS OF UNDERGROUND UTILITIES SHOWN ARE BASED ON THE SURVEY REFERENCED ABOVE. THE CONTRACTOR SHALL CONTACT DIGSAFE AND THE PROPER LOCAL AUTHORITIES OR RESPECTIVE UTILITY COMPANIES TO CONFIRM THE LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. ANY DAMAGE DUE TO FAILURE OF THE CONTRACTOR TO CONTACT THE PROPER AUTHORITIES SHALL BE BORNE BY THE CONTRACTOR.
- CONTRACTOR SHALL BEGIN MAINTENANCE IMMEDIATELY AFTER PLANTING AND WILL CONTINUE UNTIL FINAL WRITTEN ACCEPTANCE OF PLANT MATERIAL.
- CONTRACTOR SHALL VERIFY ALL TREE REMOVALS AND/OR TRANSPLANTS WITH OWNER'S REPRESENTATIVE PRIOR TO CONSTRUCTION START.
- THE LANDSCAPE CONTRACTOR SHALL SUPPLY ALL PLANT MATERIALS IN QUANTITIES SUFFICIENT TO COMPLETE ALL PLANTINGS SHOWN ON THIS DRAWING.
- ALL MATERIALS SHALL CONFORM TO THE GUIDELINES ESTABLISHED BY THE AMERICAN NURSERY AND LANDSCAPE ASSOCIATION.
- ALL PLANTS SHALL BE BALLED IN BURLAP OR CONTAINERIZED.
- ALL PLANTS TO BE BALLED IN BURLAP OR CONTAINERIZED.
- MULCH FOR PLANTED AREAS TO BE AGED PINE BARK: PARTIALLY DECOMPOSED, DARK BROWN IN COLOR AND FREE OF WOOD CHIPS THICKER THAN 1/4 INCH.
- PLANTING SOIL MIX: LOAM THOROUGHLY INCORPORATED WITH ROTTED MANURE PROPORTIONED 5 C.Y. TO 1 C.Y. OR EQUIVALENT. FERTILIZER ADDED PER RECOMMENDED RATES OF SOILS ANALYSIS.
- THE LANDSCAPE CONTRACTOR SHALL GUARANTEE ALL PLANT MATERIALS FOR ONE (1) FULL YEAR FROM DATE OF ACCEPTANCE.
- ALL PLANT MATERIALS ARE SUBJECT TO THE APPROVAL OF THE LANDSCAPE ARCHITECT, AT THE NURSERY, AND AT THE SITE.
- ALL AREAS OF THE SITE WHICH HAVE BEEN DISTURBED AND NOT OTHERWISE DEVELOPED SHALL BE LOAMED AND SEEDING WITH A MINIMUM DEPTH OF 6" DEPTH TOPSOIL.

NOTE: REFER TO SHEET 63 FOR PLANTING DETAILS



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BRIDGE STREET RECONSTRUCTION PROJECT

Title

LANDSCAPING PLAN PART 2 OF 5

179410455

Project No.

Drawing No.

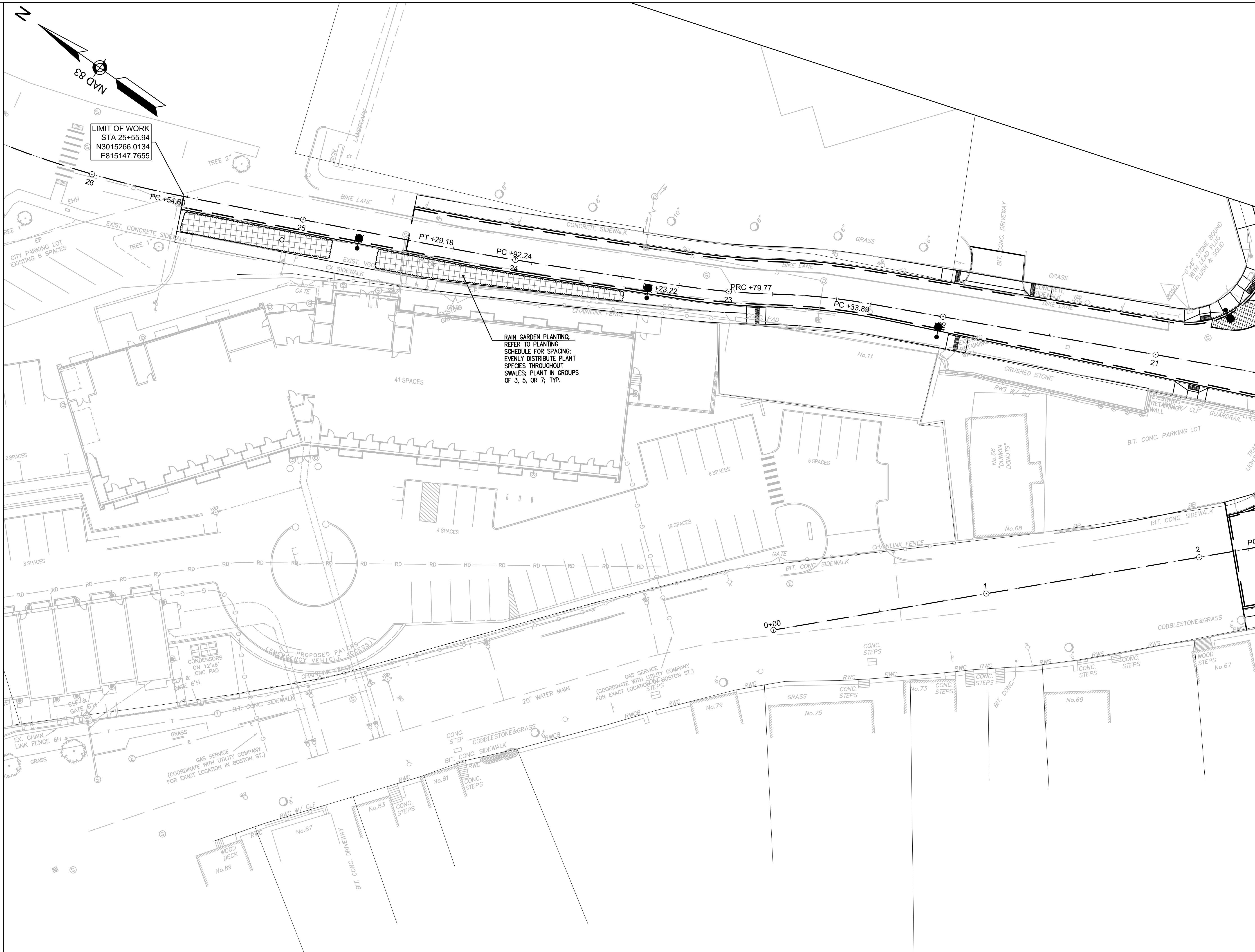
1" = 20'

Scale

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Sheet

Revision



NOTE: REFER TO SHEET 59 FOR PLANTING NOTES, LEGEND AND SCHEDULE;
REFER TO SHEET 63 FOR PLANTING DETAILS



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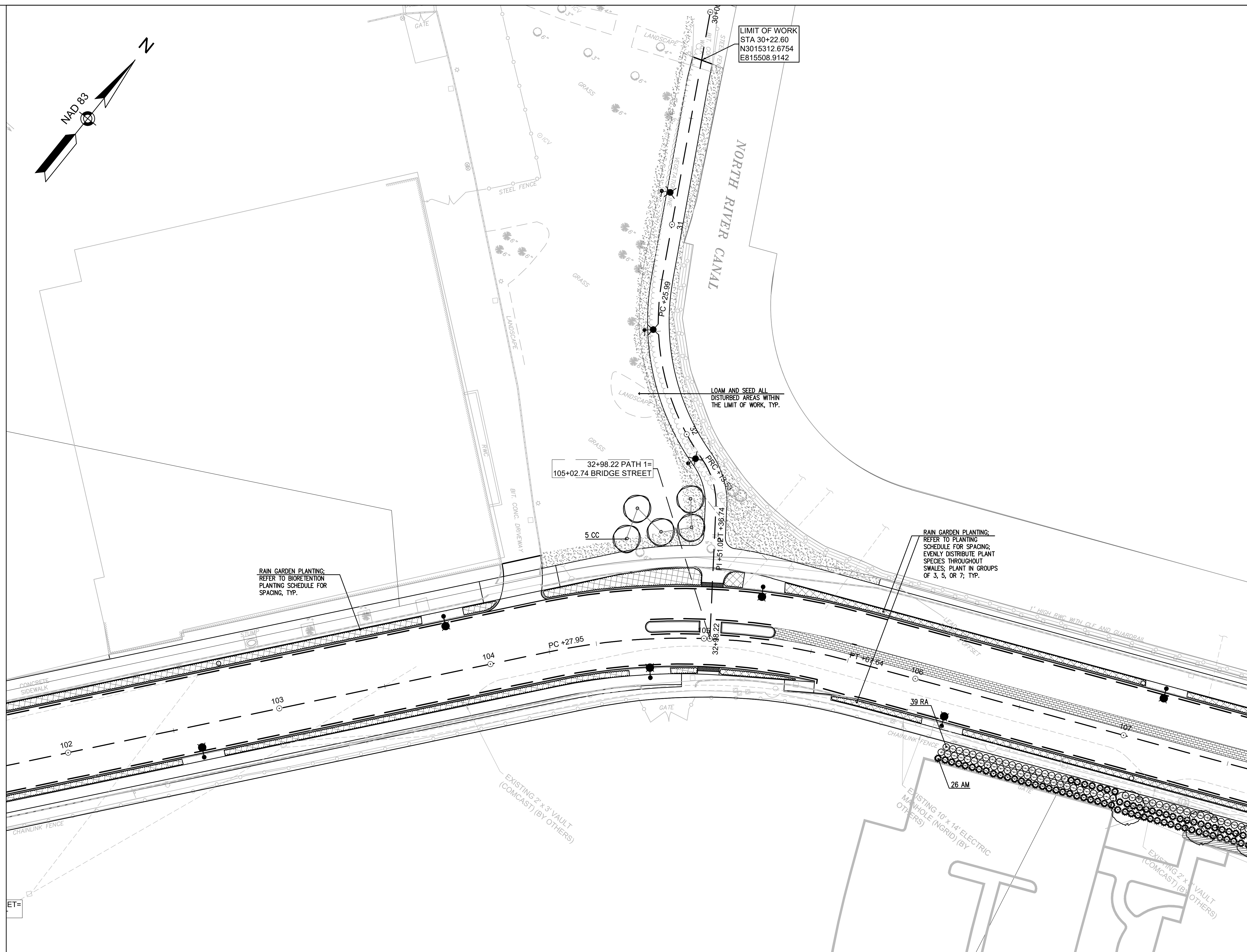


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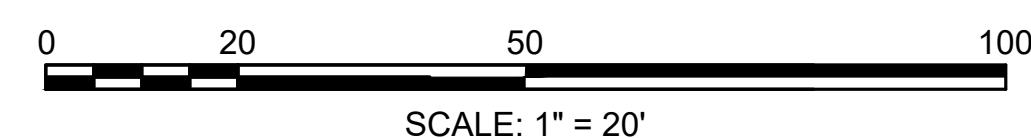
BRIDGE STREET RECONSTRUCTION PROJECT

Title
LANDSCAPING PLAN PART 3 OF 5

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Project No.	Scale	
	61 of 113	
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NOTE: REFER TO SHEET 59 FOR PLANTING NOTES, LEGEND AND SCHEDULE;
REFER TO SHEET 63 FOR PLANTING DETAILS



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2020/02/05 1:15 PM By: Sedler, Chris



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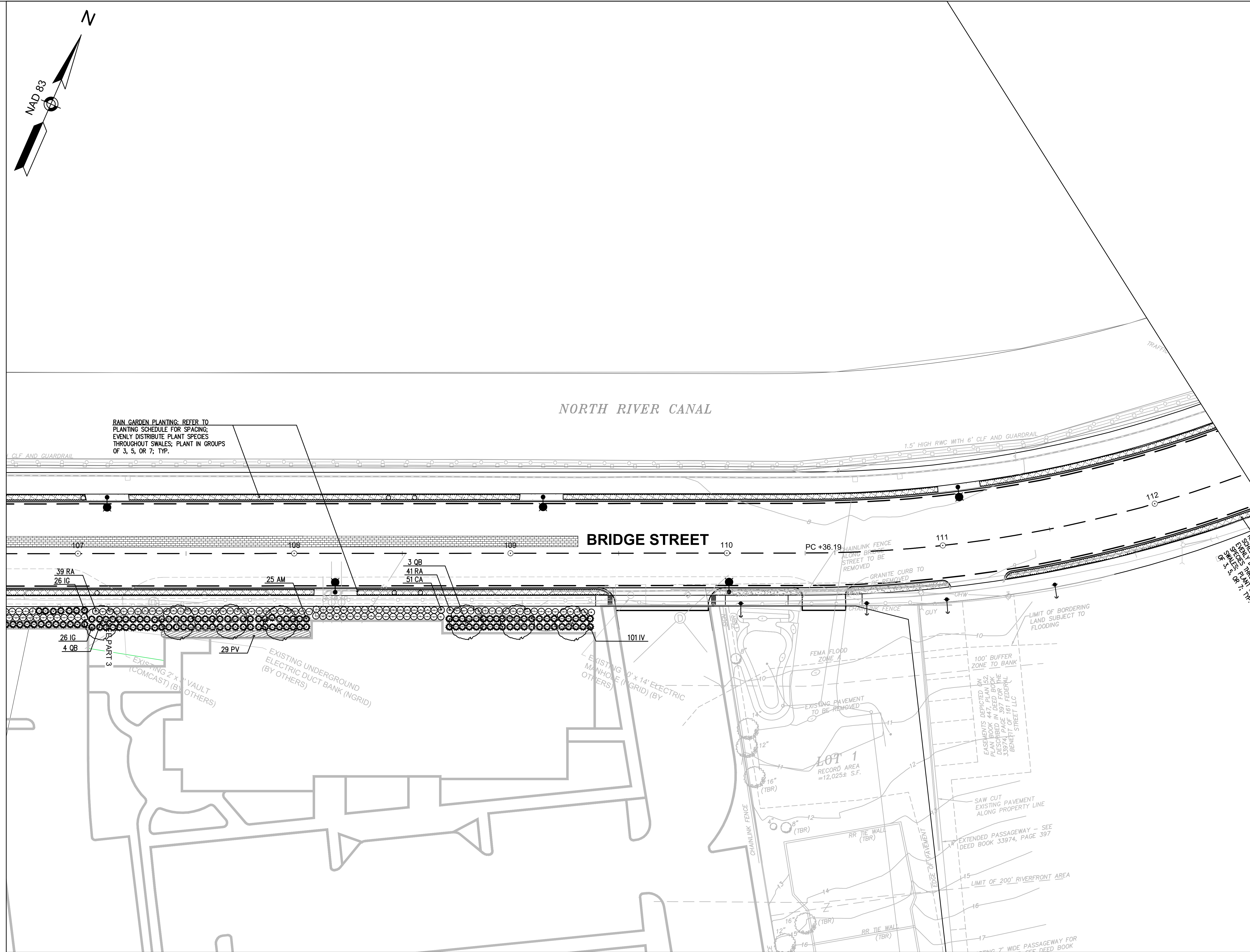


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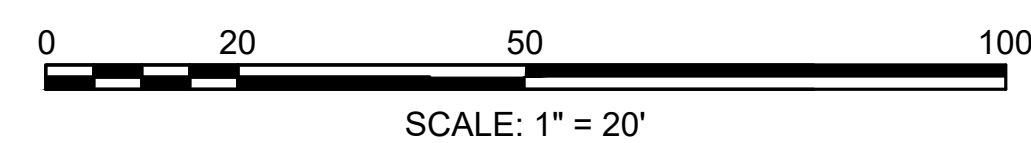
BRIDGE STREET RECONSTRUCTION PROJECT

Title
LANDSCAPING PLAN PART 4 OF 5

179410455	1" = 20'	
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NOTE: REFER TO SHEET 59 FOR PLANTING NOTES, LEGEND AND SCHEDULE;
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SCALE: 1" = 20'



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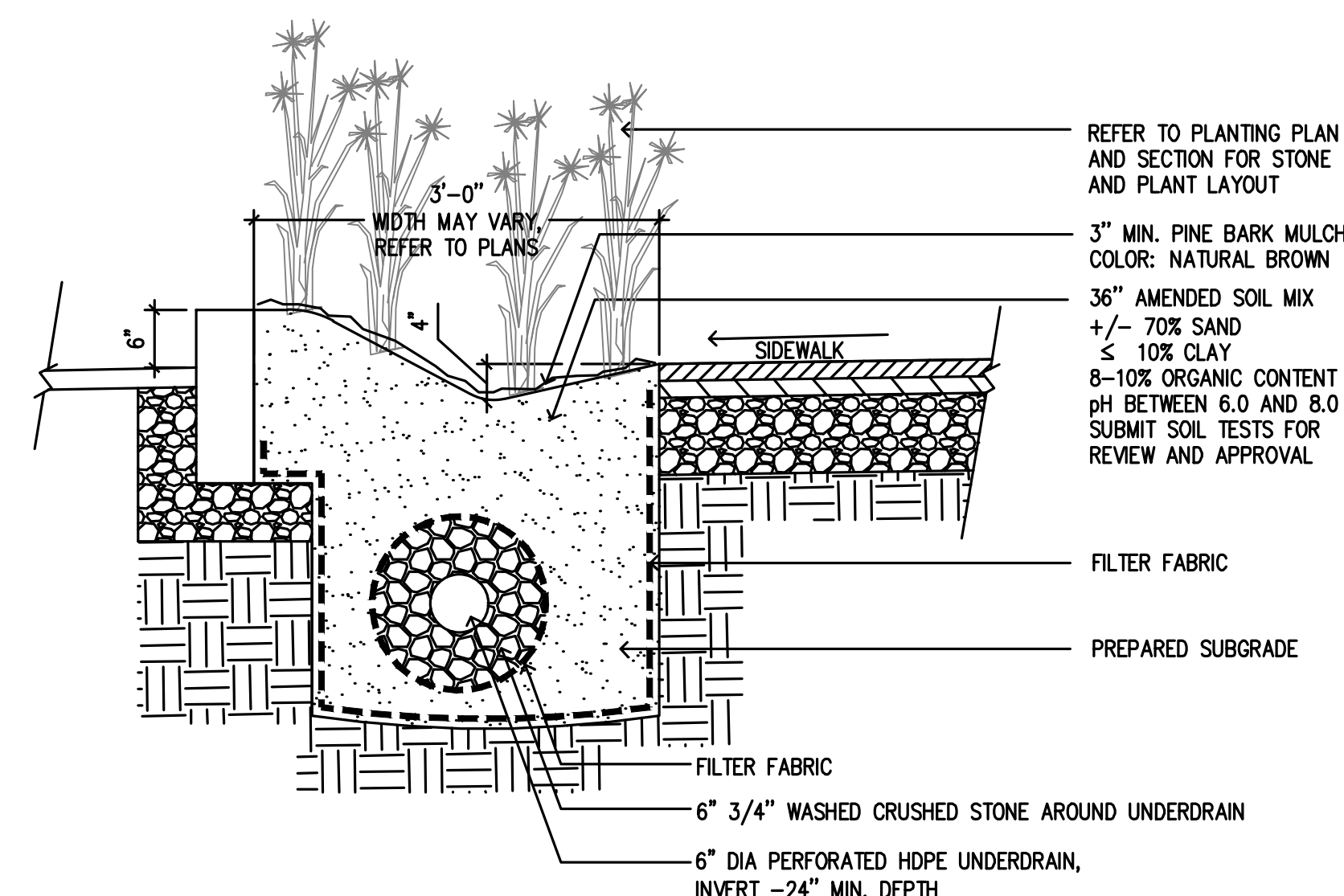
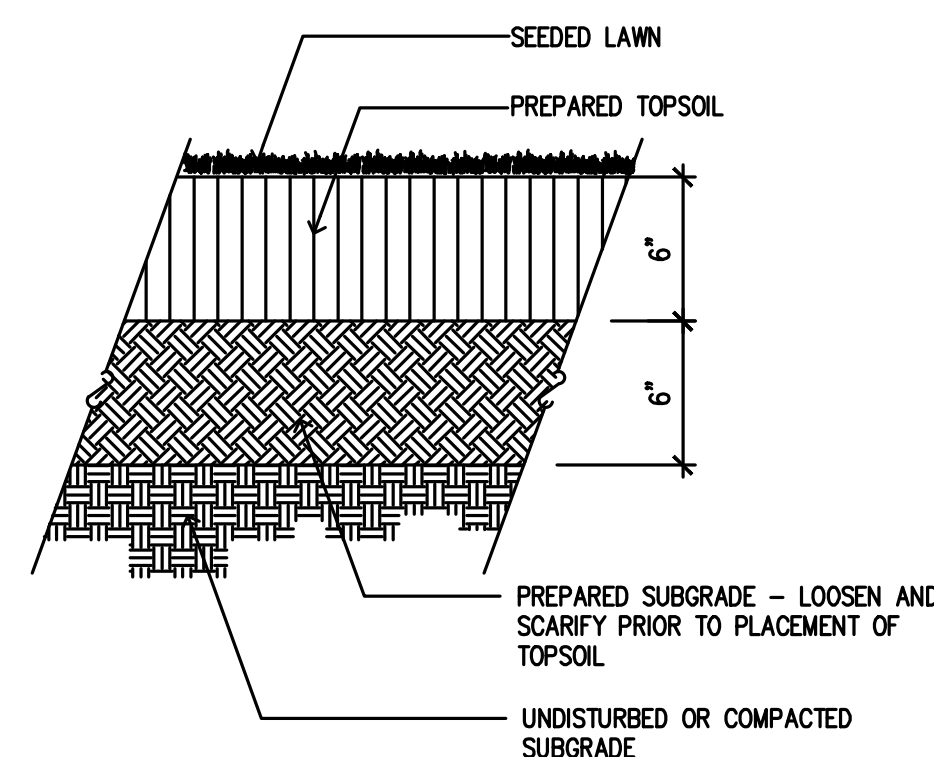


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Title
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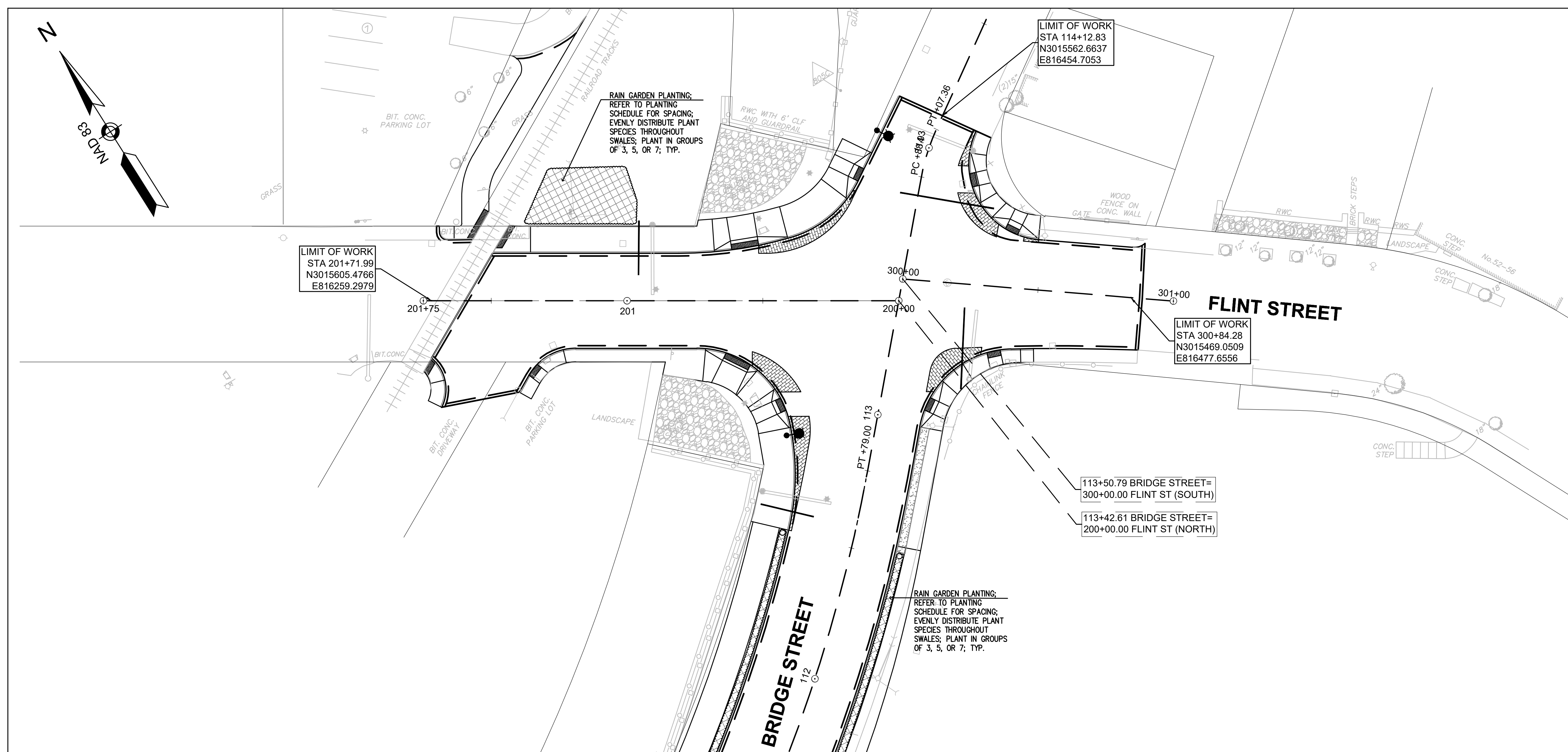
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NOTE: REFER TO SHEET 59 FOR PLANTING NOTES, LEGEND AND SCHEDULE;
REFER TO SHEET 63 FOR PLANTING DETAILS

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2020/02/05 1:16 PM By: Sadtler, Chris

Revision	By	Appd.	YY.MM.DD

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File Name: dwg_lighting_details.dwg	Dwn.	Chkd.	Dsgn.	YY.MM.DD

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BRIDGE STREET RECONSTRUCTION PROJECT

Title
LIGHTING LEGEND, ABBREVIATIONS & NOTES

179410455 NOT TO SCALE
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ELECTRICAL LEGEND:

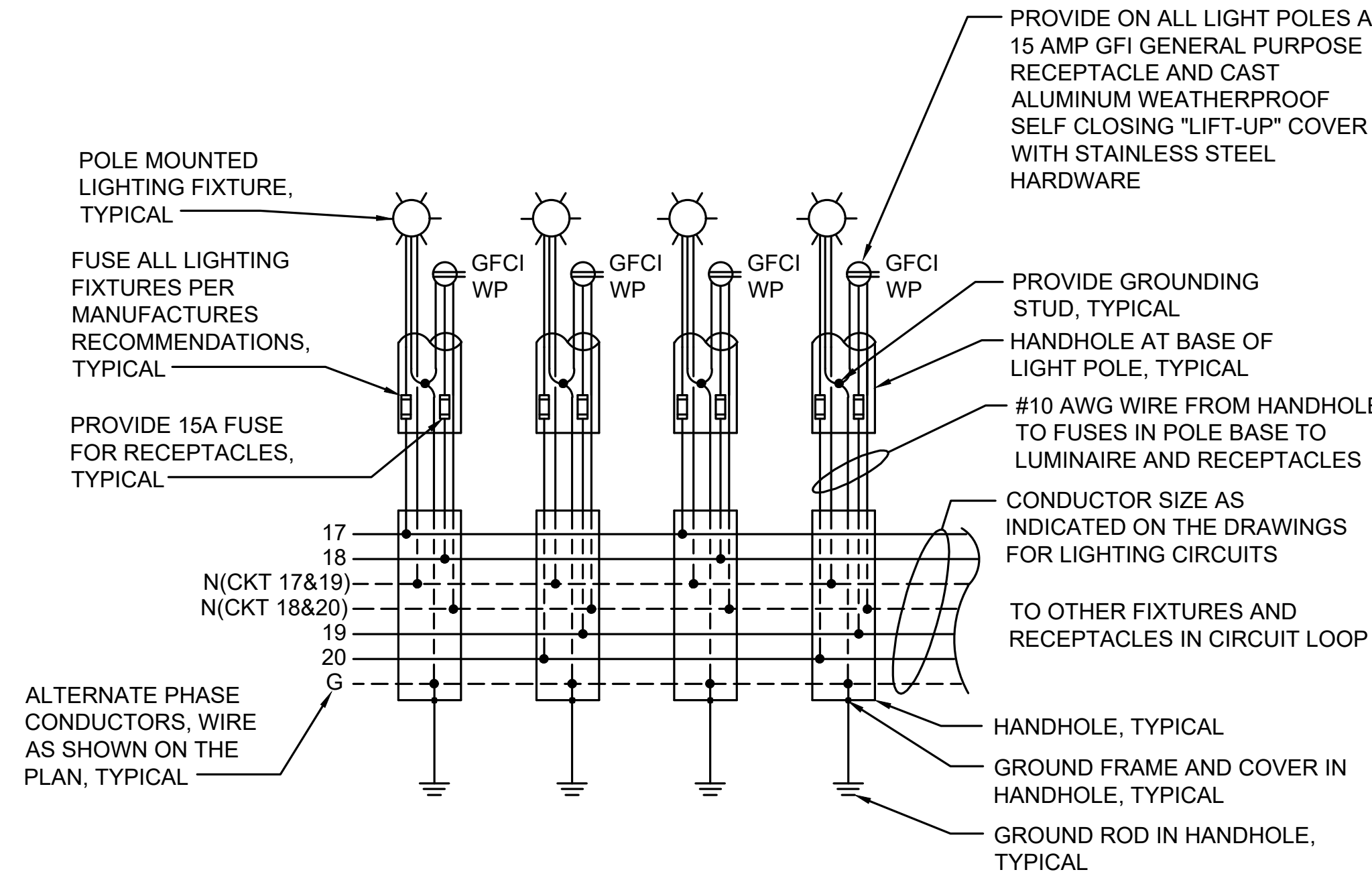
	LIGHTING CONDUIT DIRECT BURIED IN THE EARTH
	CONDUIT, CONCRETE ENCASED
	CONDUIT TURNING DOWN
	SINGLE LUMINAIRE WITH LIGHT POLE AND CONCRETE POLE FOUNDATION
	LIGHTING CONTROL CABINET
	PRE-CAST HANDHOLE

ABBREVIATIONS:

A	AMPERES
AFG	ABOVE FINISHED GRADE
AWG	AMERICAN WIRE GAUGE
C	CONDUIT
CB	CIRCUIT BREAKER
CKT	CIRCUIT
CM	CIRCULAR MIL
FBO	FURNISHED BY OTHERS
Fc	FOOT CANDLES
GAL	GALVANIZED
GND	GROUND
GFI	GROUND FAULT INTERRUPTER
IESNA	ILLUMINATING ENGINEERING SOCIETY OF NORTH AMERICA
KVA	KILO VOLT AMPERES
LL	"L" LEFT
LT	LIQUID TIGHT FLEXIBLE METAL CONDUIT
LTG	LIGHTING
NIC	NOT IN CONTRACT
PVC	POLYVINYL CHLORIDE
RCPT	RECEPTACLE
RGS	RIGID GALVANIZED STEEL
RNC	RIGID NON-METALLIC CONDUIT
RT	RAIN TIGHT
SCH	SCHEDULE
SW	SIDEWALK
UF	UNFUSED
V	VOLTS
W	WATTS
WP	WEATHERPROOF

GENERAL NOTES:

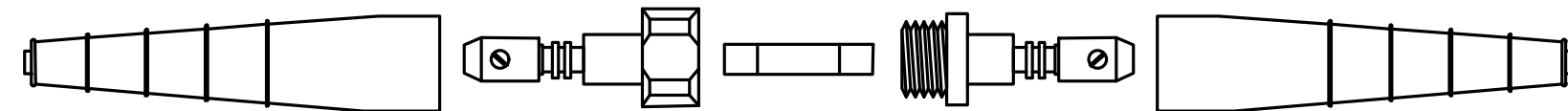
- CONDUIT RUNS ARE DIAGRAMMATIC. COORDINATE CONDUIT RUNS WITH ALL OTHER EXISTING OR PROPOSED UNDERGROUND UTILITIES AND STRUCTURES.
- PRIOR TO THE INSTALLATION OF PROPOSED LIGHTING, THE CONTRACTOR SHALL EXCAVATE TEST PITS AT LOCATIONS OF UTILITY CROSSINGS TO VERIFY DEPTHS AND LOCATIONS OF EXISTING PIPES, CONDUITS OR OTHER FACILITIES AND AS REQUIRED BY THE ENGINEER. WORK SHALL BE PAID FOR UNDER ITEM 141.1 TEST PIT FOR EXPLORATION.
- COORDINATE ELECTRIC SERVICE WITH ELECTRIC UTILITY COMPANY (NATIONAL GRID). ALL WORK ASSOCIATED WITH THE ELECTRIC SERVICE MUST BE COMPLETED IN ACCORDANCE WITH ELECTRIC COMPANY STANDARDS.
- ALL UTILITY BACK CHARGES ARE THE RESPONSIBILITY OF THE CONTRACTOR FOR ALL WORK PERTAINING TO THIS PROJECT. COORDINATE ALL PERMANENT AND TEMPORARY POWER WITH THE UTILITY.
- ALL ELECTRICAL EQUIPMENT SHALL BE UL LISTED FOR ITS INTENDED USE.
- PROVIDE GROUND CONDUCTOR IN EACH ELECTRICAL RACEWAY IN ACCORDANCE WITH THE CODE.
- THE HOT LINE AND NEUTRAL CONDUCTORS IN THE POLE HANDHOLE SHALL HAVE AN APPROVED STREET LIGHT FUSE CONNECTOR.



TYPICAL LIGHT POLE CIRCUITING ONE-LINE DIAGRAM

LIGHT POLE WIRING

NOT TO SCALE

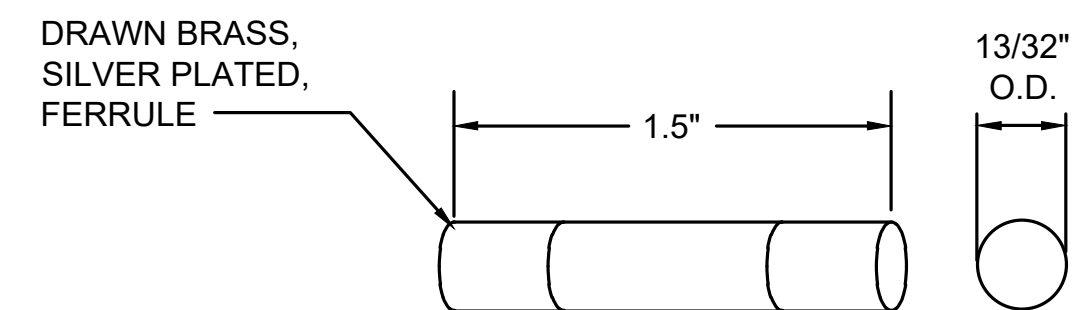


RATING: 600 VOLTS - 20 AMPS.	
FUSE SIZE: 1 1/2" x 13/32" O.D. - FERRULE 3/8"	
CONNECTOR SIZE: 8 1/2" x 7/8" O.D.	
CONNECTOR TYPE:	FUSE - QUICK DISCONNECT SET SCREW TYPE CONNECTION
CONNECTOR MATERIAL:	MOLDED PLASTIC BODY WITH RUBBER INSULATING BOOTS.
CONDUCTOR SIZE:	LINE AND LOAD #2 AWG THROUGH #12 AWG
REQUIRED FEATURES:	WATERTIGHT NO TAPING OR SEALANT REQUIRED
-FUSE HELD IN LOAD SIDE WHEN CONNECTOR IS DISCONNECTED. AASHTO COMPLIANT BREAKAWAY FUSE HOLDER.	
-PROVIDE APPROVED FUSE HOLDER WITH "DUMMY" ROD BLANK FUSE FOR NEUTRALS	

FUSE HELD IN LOAD SIDE WHEN CONNECTOR IS DISCONNECTED.

STRAIGHT THRU CONNECTOR

NOT TO SCALE



SIZE: MIDGET
TYPE: CURRENT LIMITING FUSE
VOLTAGE RATING: 120/240 VOLTS
USE: PROTECTION OF AREA LIGHTING EQUIPMENT

FUSES

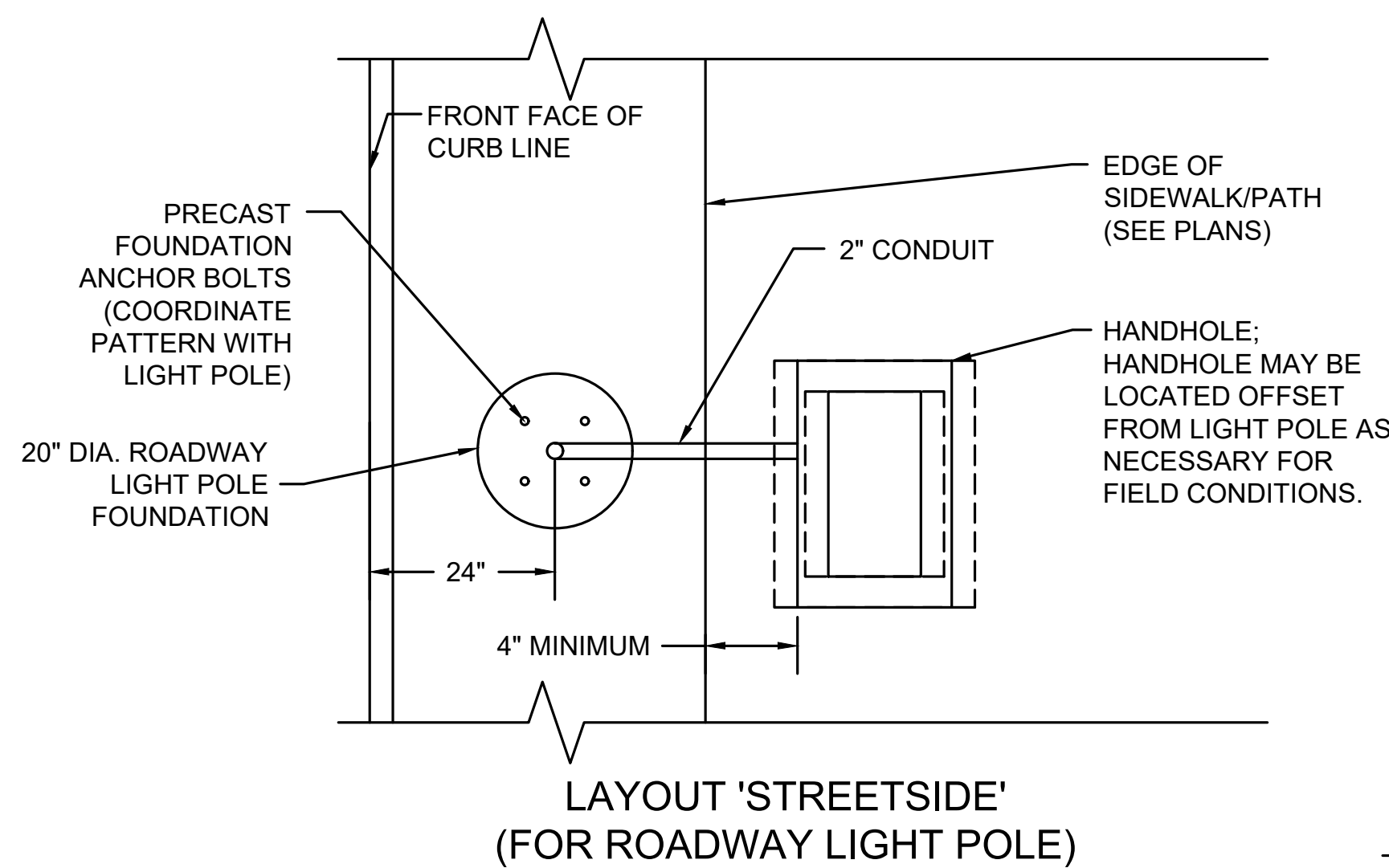
NOT TO SCALE

LUMINAIRE INFORMATION BOX

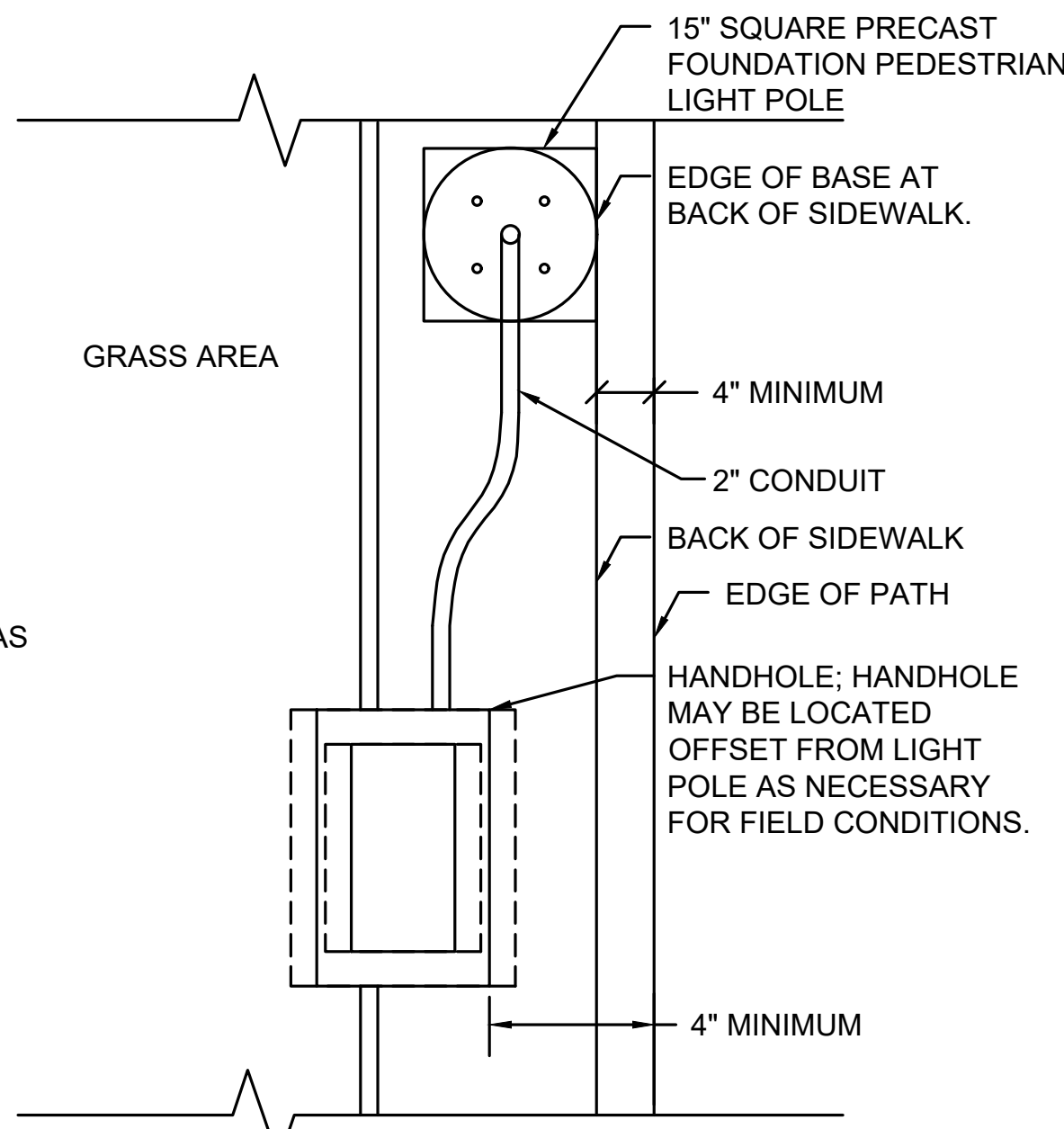
POLE IDENTIFICATION NUMBER. 'LC1' INDICATES LOAD CENTER '1' POWERS LIGHT POLE AND '01' INDICATES POLE NUMBER
STATION NUMBER
ARM LENGTH
POLE HEIGHT

LC1-01	17,R18
STA	44+98
6'	TYPE 3
27'	13000

INDICATES CIRCUITS TO LUMINAIRE AND RECEPTACLE. LUMINAIRE IS POWERED FROM CIRCUIT 17. RECEPTACLE IS POWERED FROM CIRCUIT 18.
IESNA LUMINAIRE DISTRIBUTION TYPE
LUMINAIRE OUTPUT



LAYOUT 'STREETSIDE'
(FOR ROADWAY LIGHT POLE)



LAYOUT 'BEHIND MULTI-USE PATH'
(FOR PEDESTRIAN LIGHT POLE)

TYPICAL LIGHT POLE AND PULLBOX LAYOUT DETAILS

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CITY OF SALEM

Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title

LIGHTING PLAN PART 1 OF 5

179410455

Project No.

Drawing No.

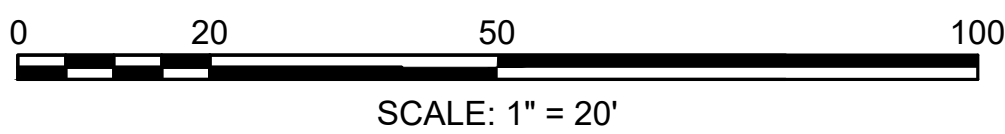
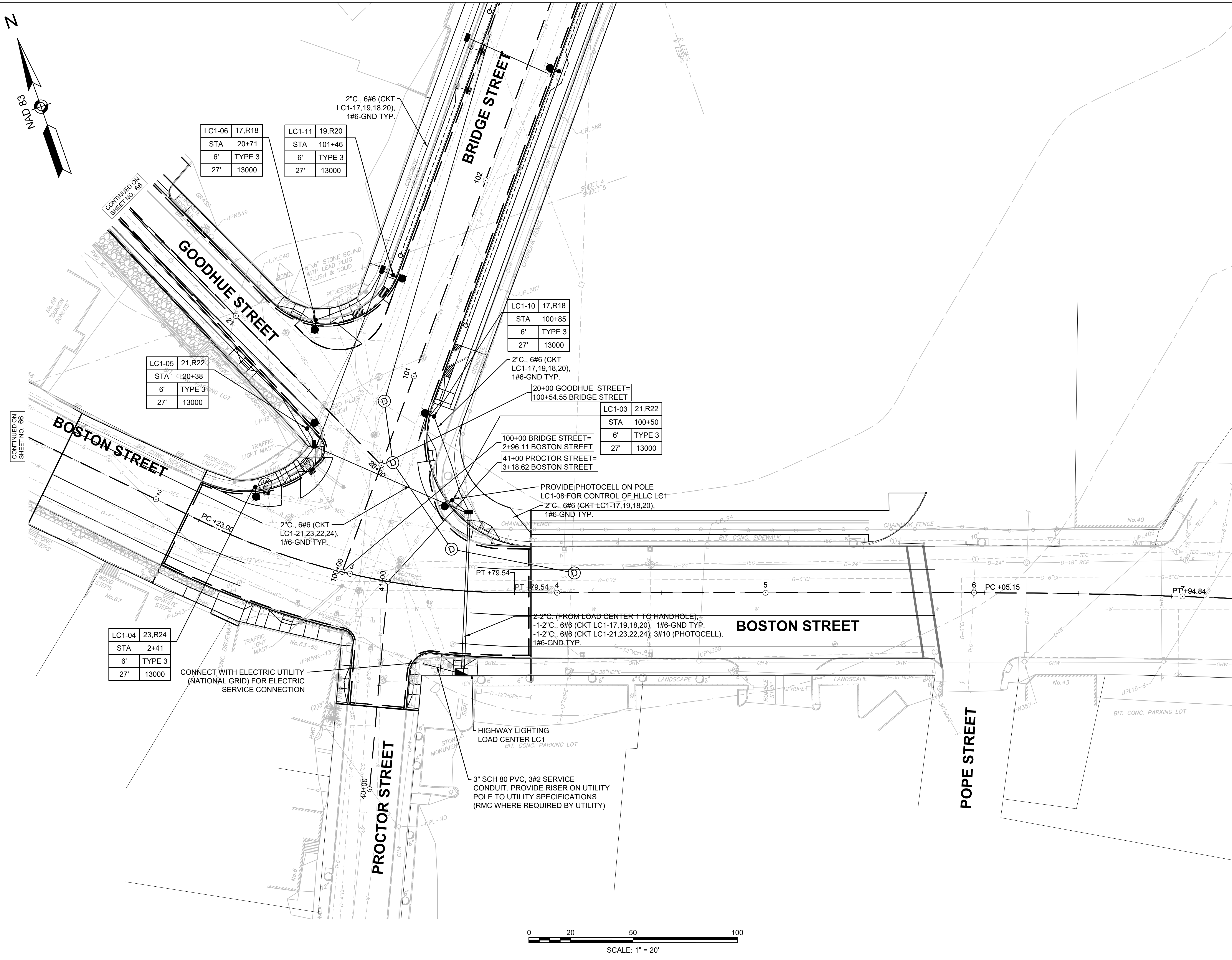
1" = 20'

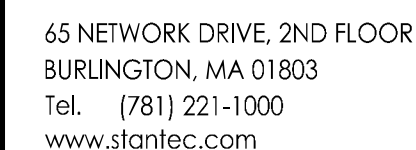
Scale

65 of 113

Sheet

Revision





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File Name: dwg_lighting_plan.dwg	Dwnl.	Chkd.	Dsgn.
			YY.MM.DD

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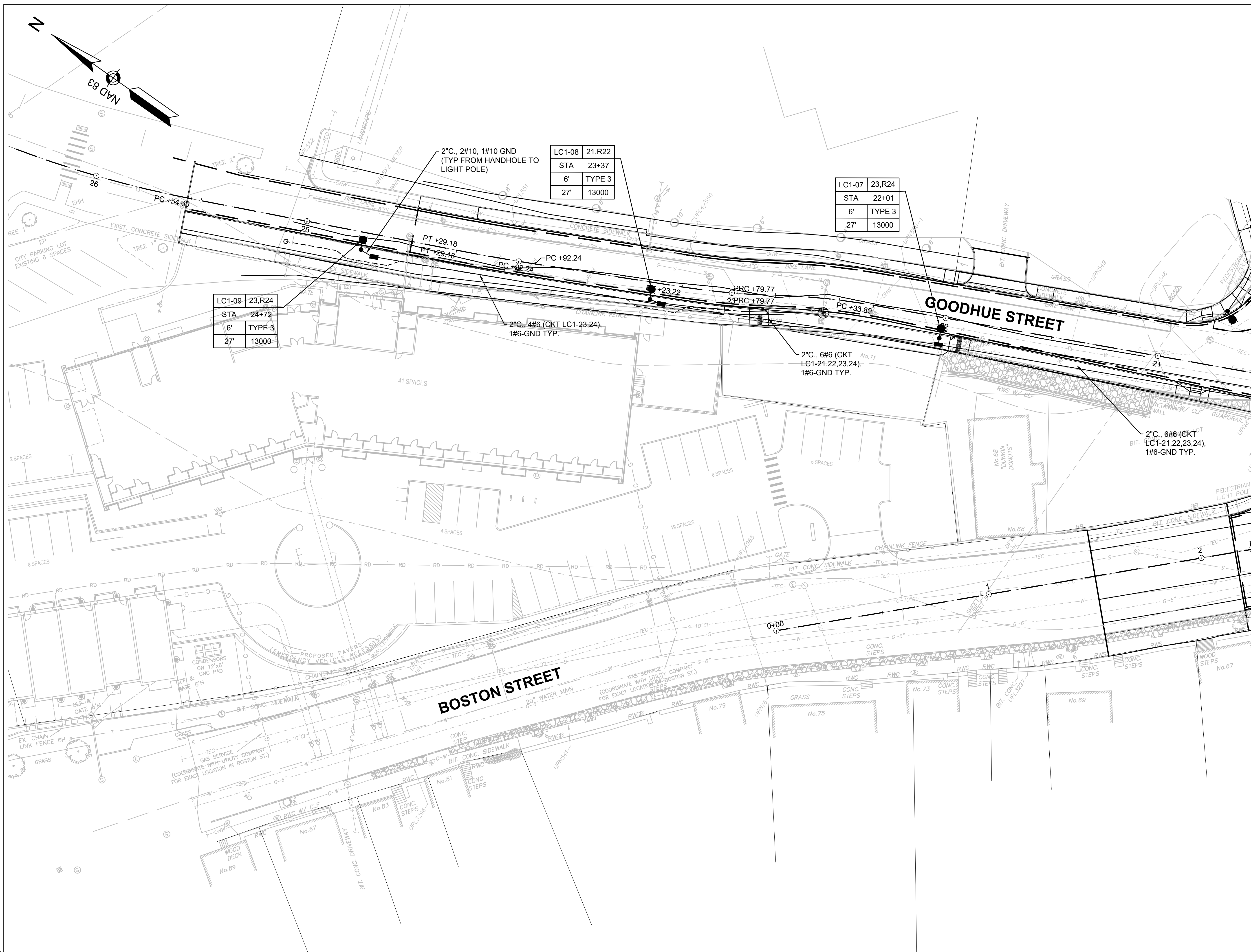


CITY OF SALEM
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BRIDGE STREET RECONSTRUCTION PROJECT

Title
LIGHTING PLAN PART 2 OF 5

179410455	1" = 20'	
Project No.	Scale	
	66 of 113	
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CONTINUED ON
SHEET NO. 65

CONTINUED ON
SHEET NO. 65

J:\17941045\transportation\drawing\dwg_fighting_plan.dwg
2020/02/05 1:17 PM By: Siedler, Chris

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Issued	By	Appd.	YY.MM.DD
File Name: dwg_lighting_plan.dwg	Dwn.	Chkd.	Dsgn.
			YY.MM.DD

Permit-Seal



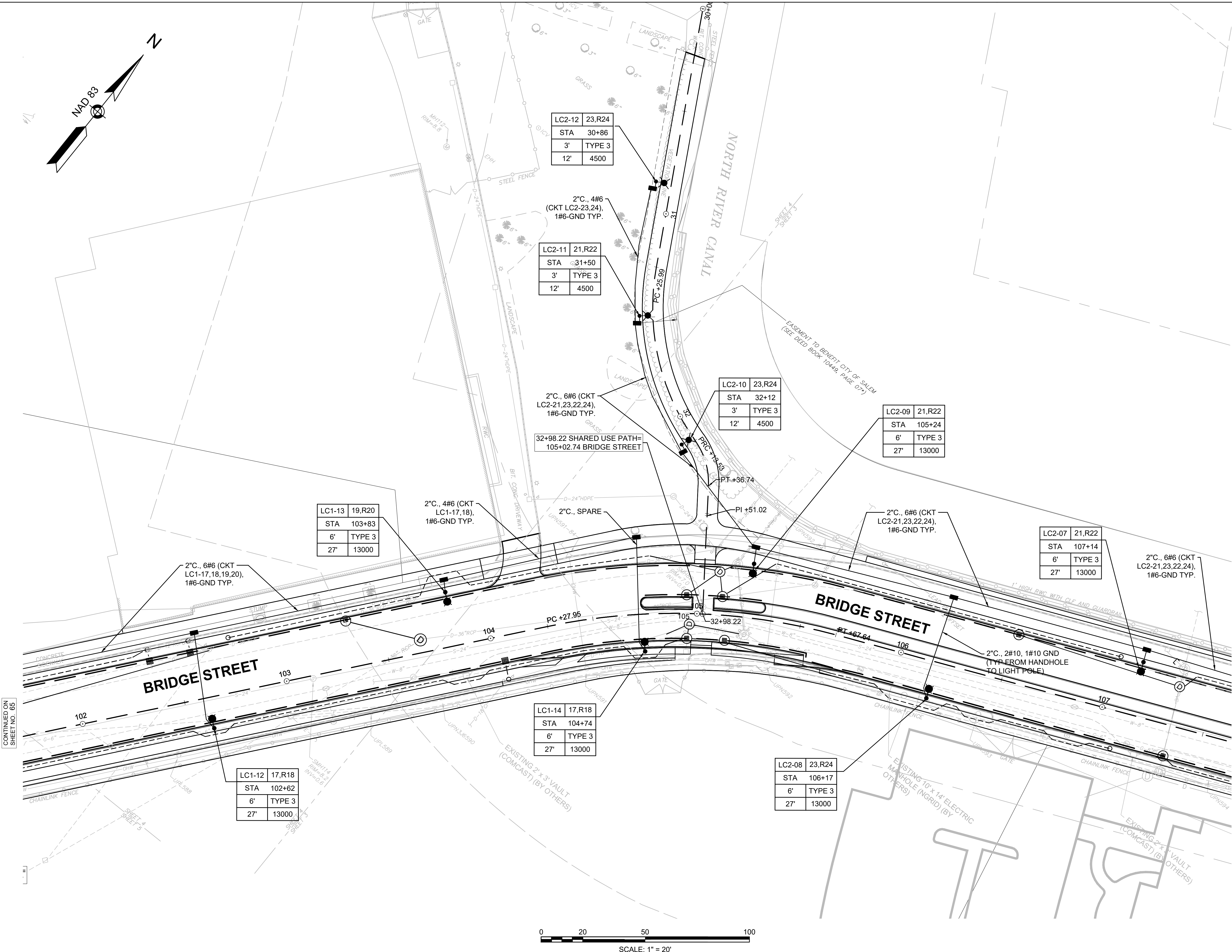
CITY OF SALEM

Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title
LIGHTING PLAN PART 3 OF 5

179410455	1" = 20'
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	67 of 113
Drawing No.	Sheet
	Revision





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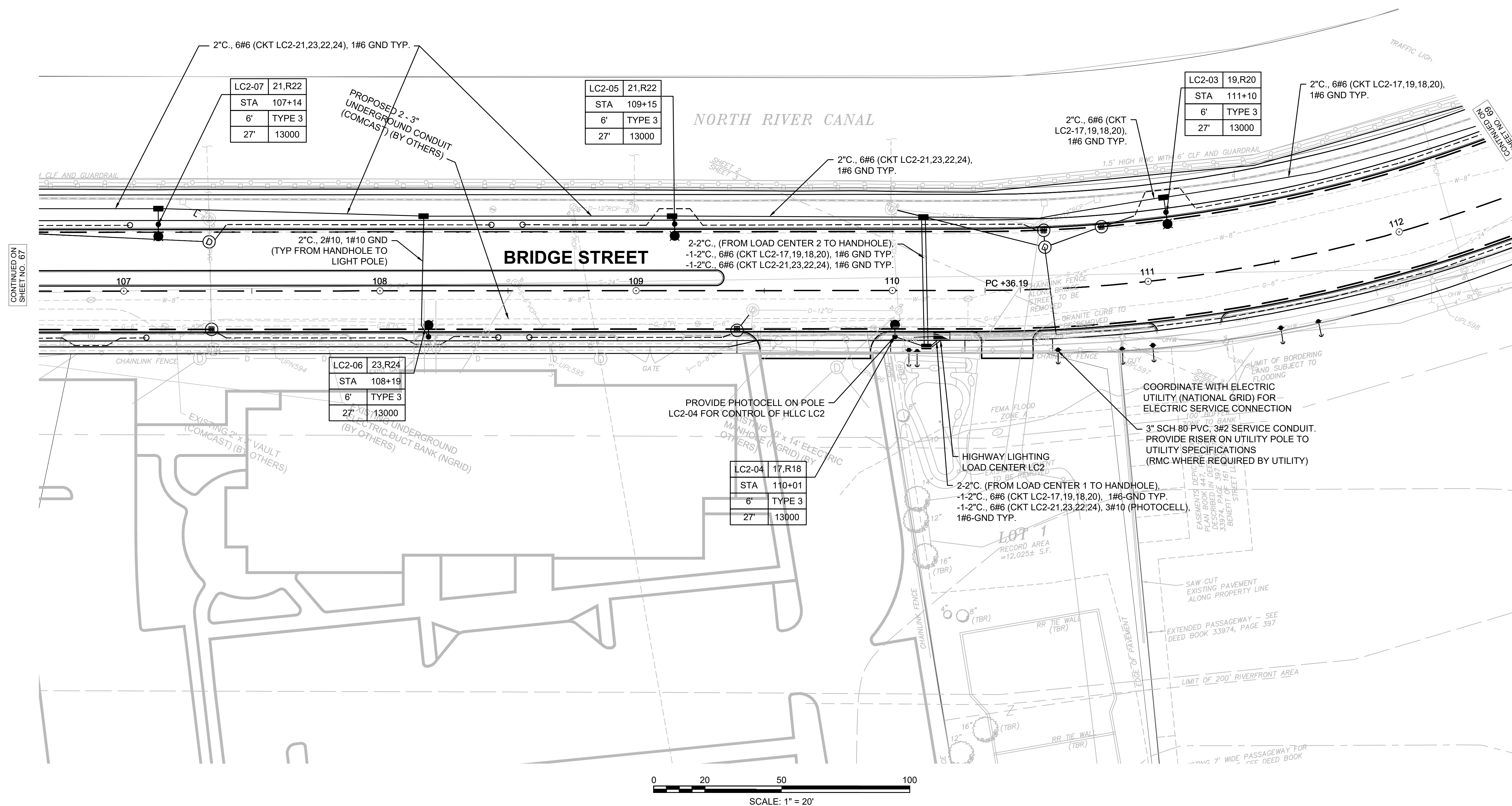
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Title
LIGHTING PLAN PART 4 OF 5

179410455	1" = 20'	
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Drawing No.	Sheet	Revision



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2020/02/05 1:18 PM By: Sadtler, Chris



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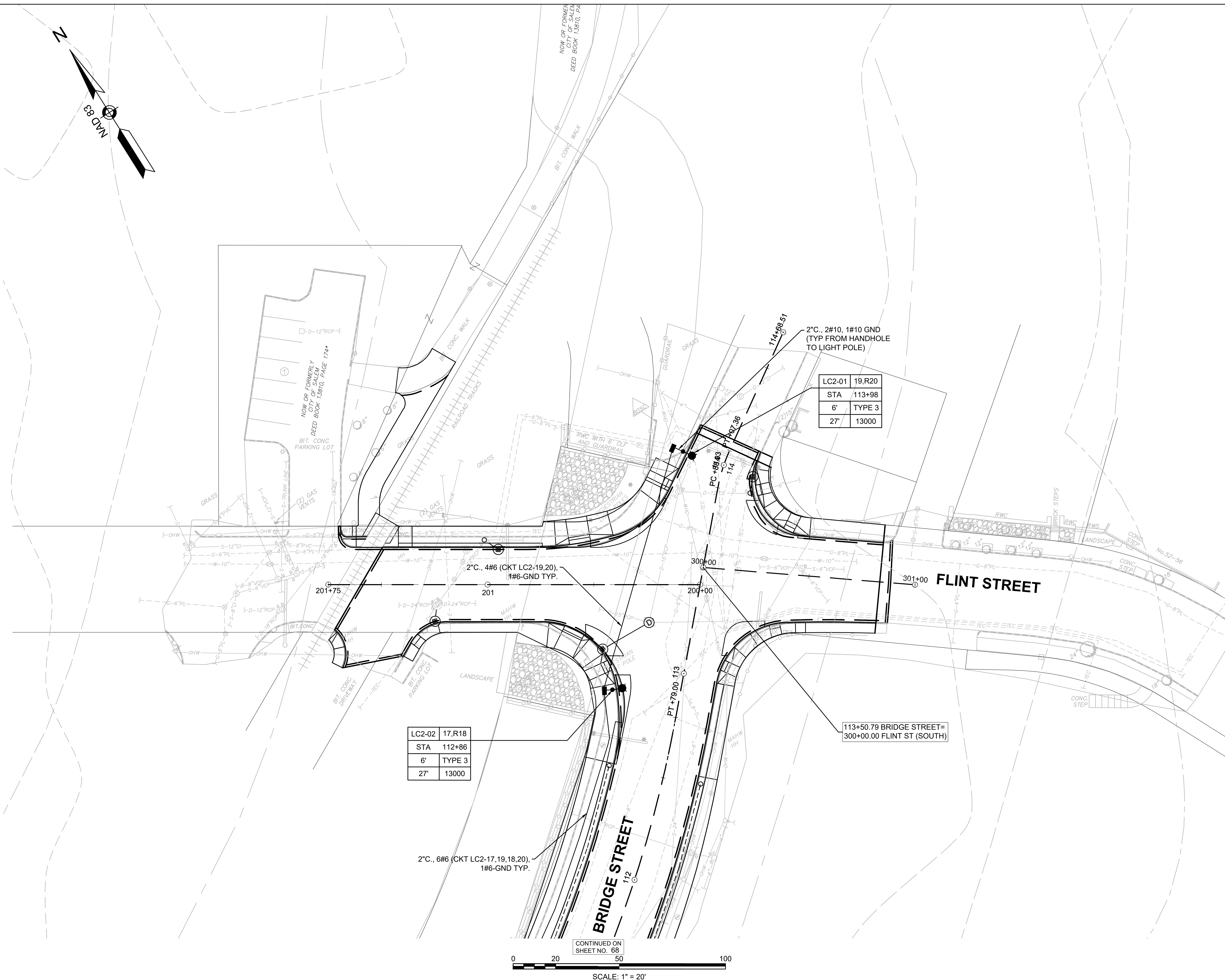
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Issued	By	Appd.	YY.MM.DD

File Name: <u>dwg_lighting_plan.dwg</u>				
	Dwn.	Chkd.	Dsgn.	YY.MM.DD

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Client/Project

Title
LIGHTING PLAN PART 5 OF 5

179410455	1" = 20'	
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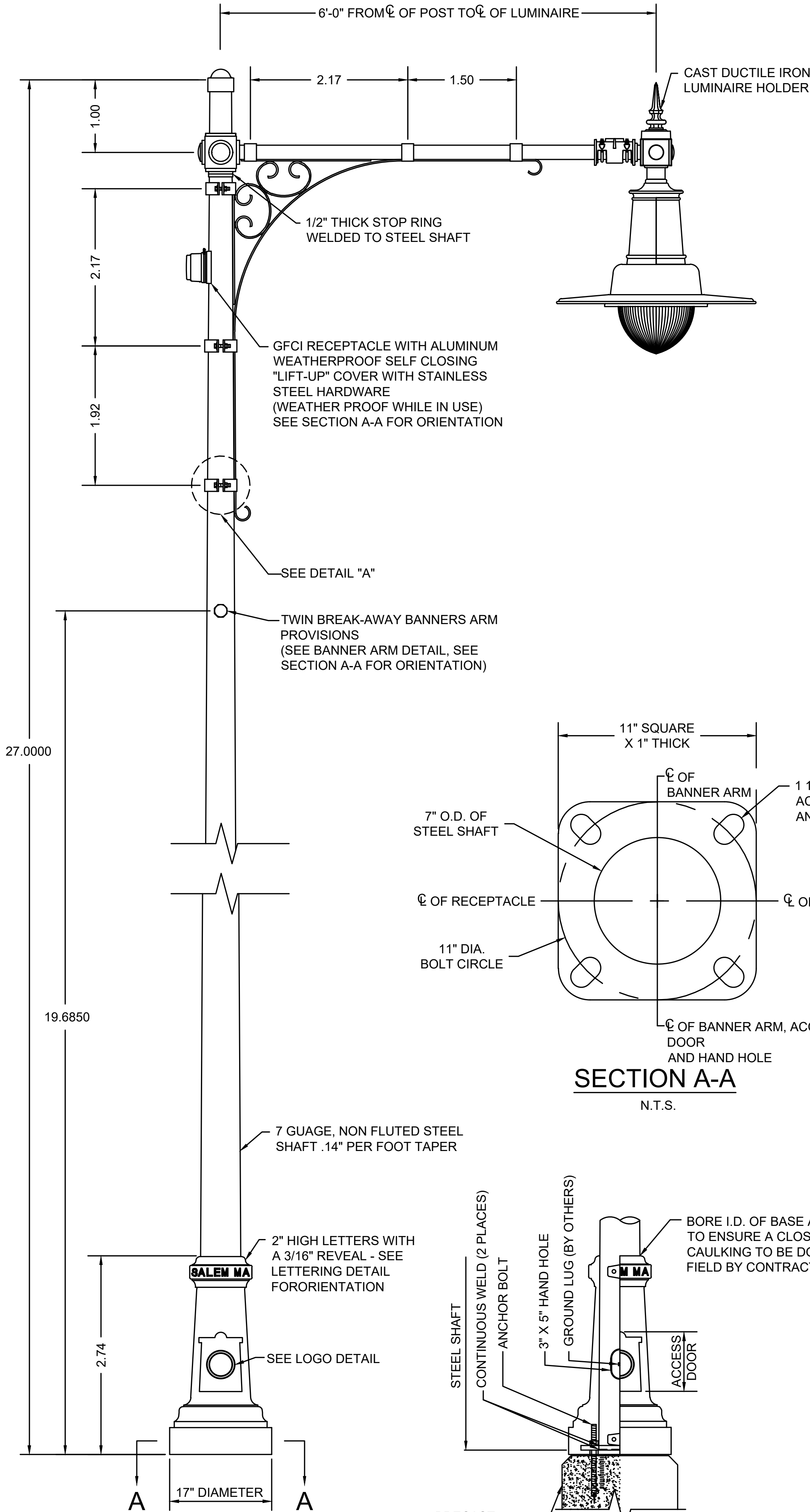


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2020/02/05 1:18 PM By: Sadtler, Chris

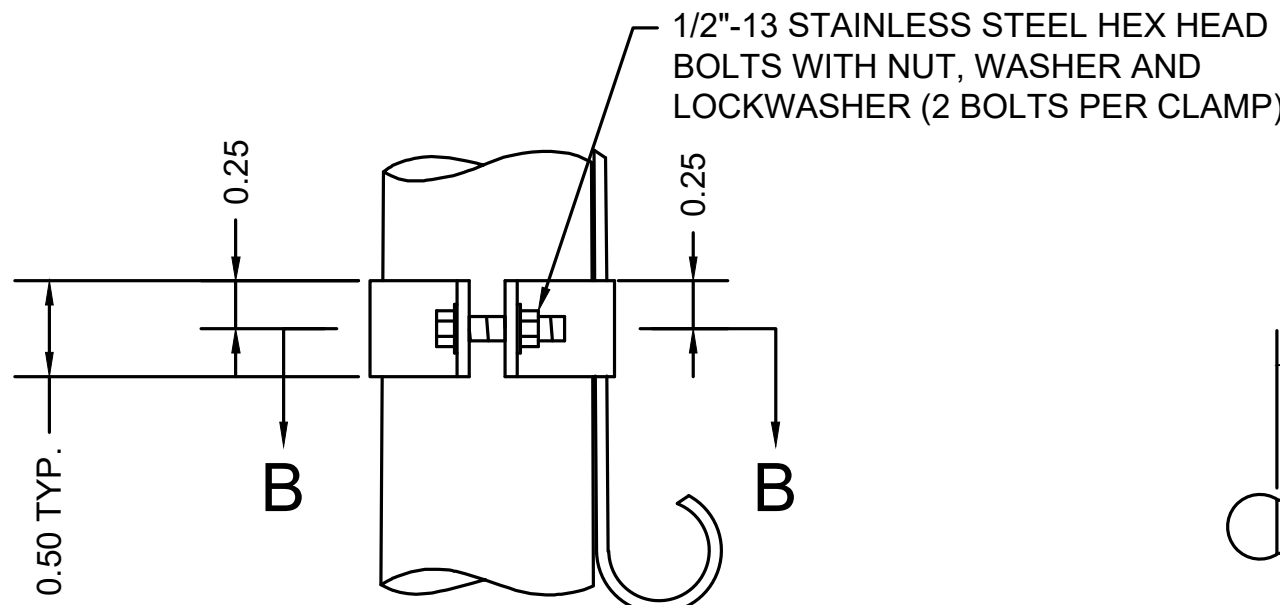
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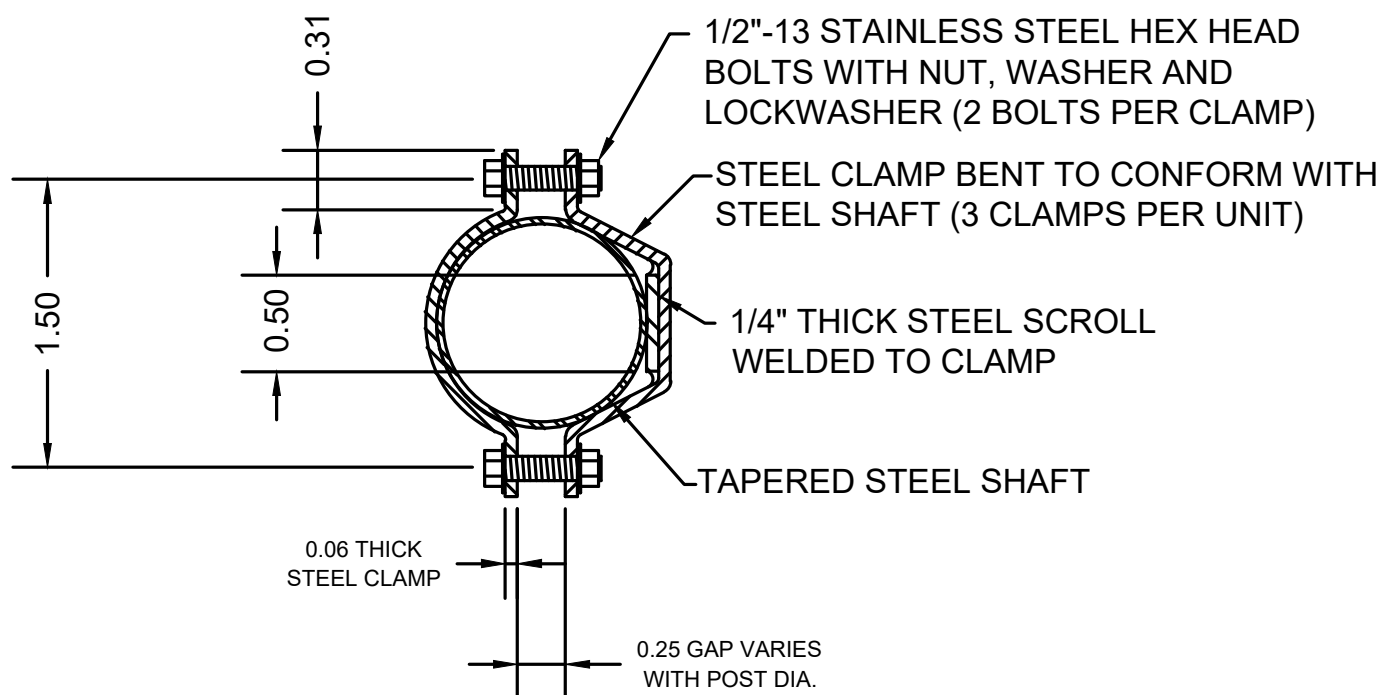
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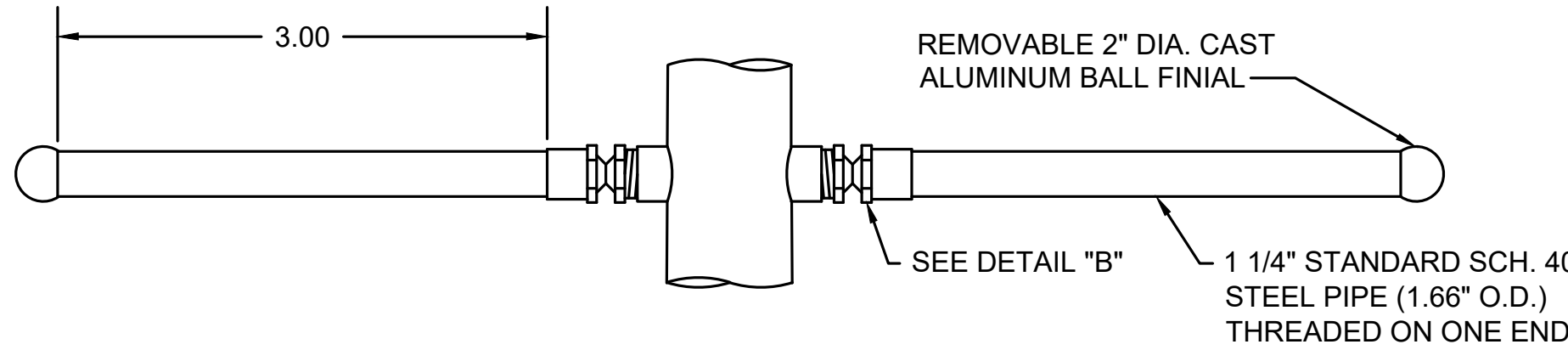
ROADWAY LIGHT POLE
NOT TO SCALE



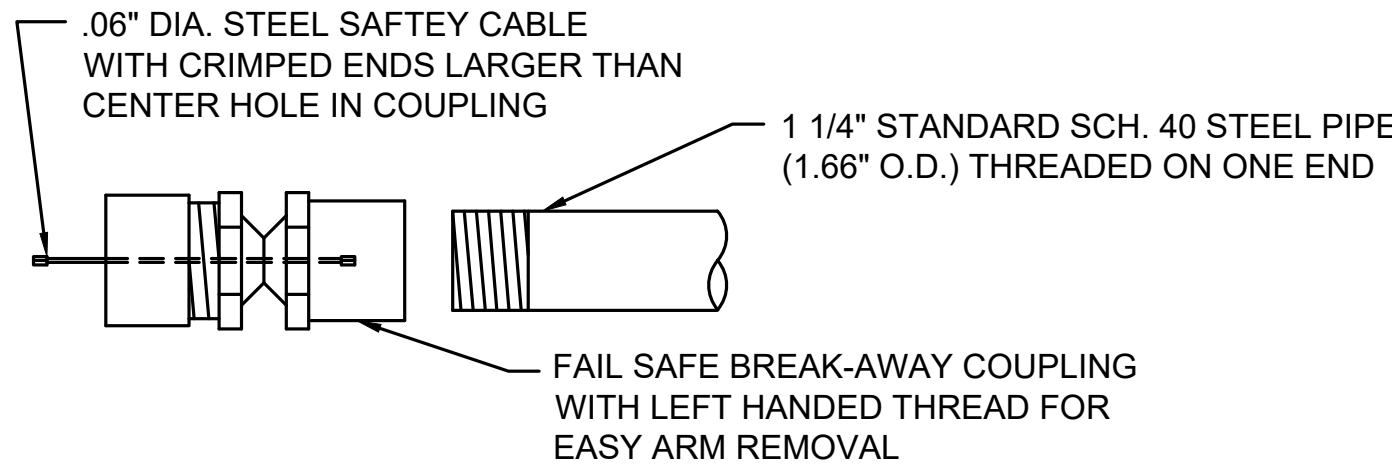
DETAIL "A"
N.T.S.



SECTION B-B
N.T.S.



BANNER ARM DETAIL
N.T.S.



DETAIL "B"
N.T.S.

LUMINAIRE SPECIFICATIONS (ITEM NUMBER: 823.101)

STYLE:	LECHMERE (ITEM NUMBER #: 823.101)
HEIGHT:	2'-4 7/8"
WIDTH:	2'9" DIA.
MATERIAL:	CAST ALUMINUM ALLOY ANSI 356 PER A.S.T.M. B26-95
GLOBE:	RIBBED GLASS
FINISH:	POWDER COAT - GLOSS BLACK
LAMPING:	155W LED
VOLTAGE:	ELECTRONIC WIRED AT 120-277
NUM. OF BOARDS:	2
COLOR TEMP:	4500K
DISTRIBUTION:	TYPE III (ASYMMETRIC DISTRIBUTION)
OPTICS:	CROSSFIRE REFRACTIVE
MODIFIER:	COLUMBIA VERTICAL ALIGNMENT
OUTPUT:	13,000 LUMEN MINIMUM

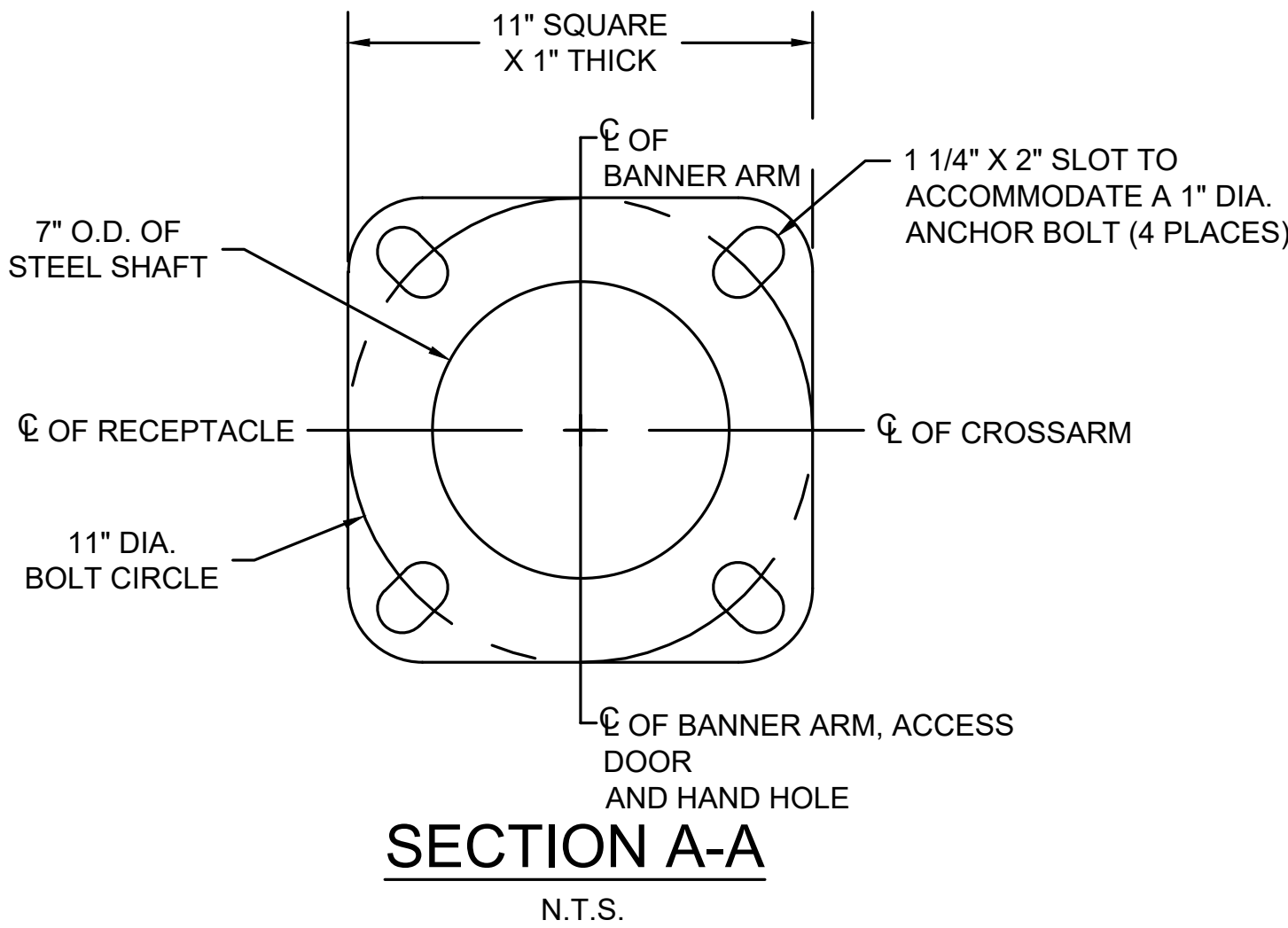
CROSS ARM SPECIFICATIONS (ITEM NUMBER: 821.3)

STYLE:	CAMBRIDGE SINGLE (ITEM NUMBER #: 821.3)
HEIGHT:	27'-0"
WIDTH:	6'-0" FROM CL OF POST TO CL OF LUMINAIRE
MATERIAL:	CAST DUCTILE IRON WITH 2" STANDARD STEEL PIPE AND STEEL SCROLL WORK
FINISH:	POWDER COAT - GLOSS BLACK

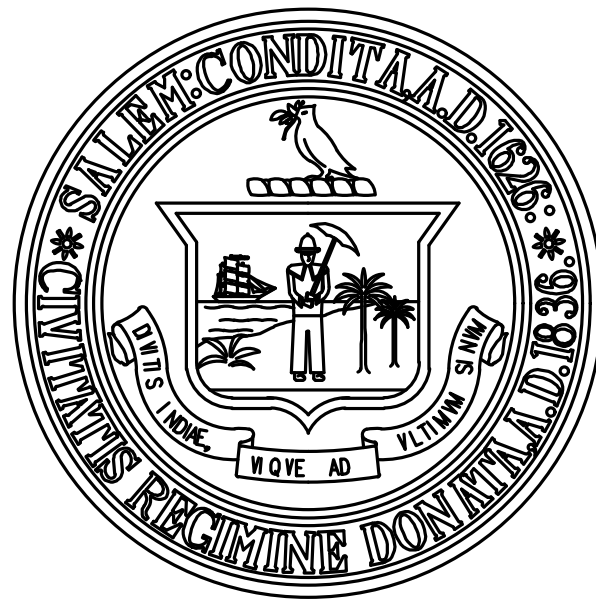
LAMP POST SPECIFICATIONS (ITEM NUMBER: 821.3)

STYLE:	CAMBRIDGE STEEL (CITY OF SALEM)
HEIGHT:	27'-0"
BASE:	17" DIAMETER
MATERIAL:	7 GAUGE, SMOOTH STEEL MONOTUBE CONSTRUCTION
SHAFT:	DUCTILE IRON
FINISH:	HOT DIPPED GALVANIZED & POWDER COAT - GLOSS BLACK
SHAFT:	POWDER COAT - GLOSS BLACK
BASE:	LOCATED IN BASE TO COINCIDE WITH HAND HOLE
ACCESS DOOR:	IN STEEL SHAFT SECURED WITH TAMPER PROOF HEX SOCKET SECURITY MACHINE SCREWS

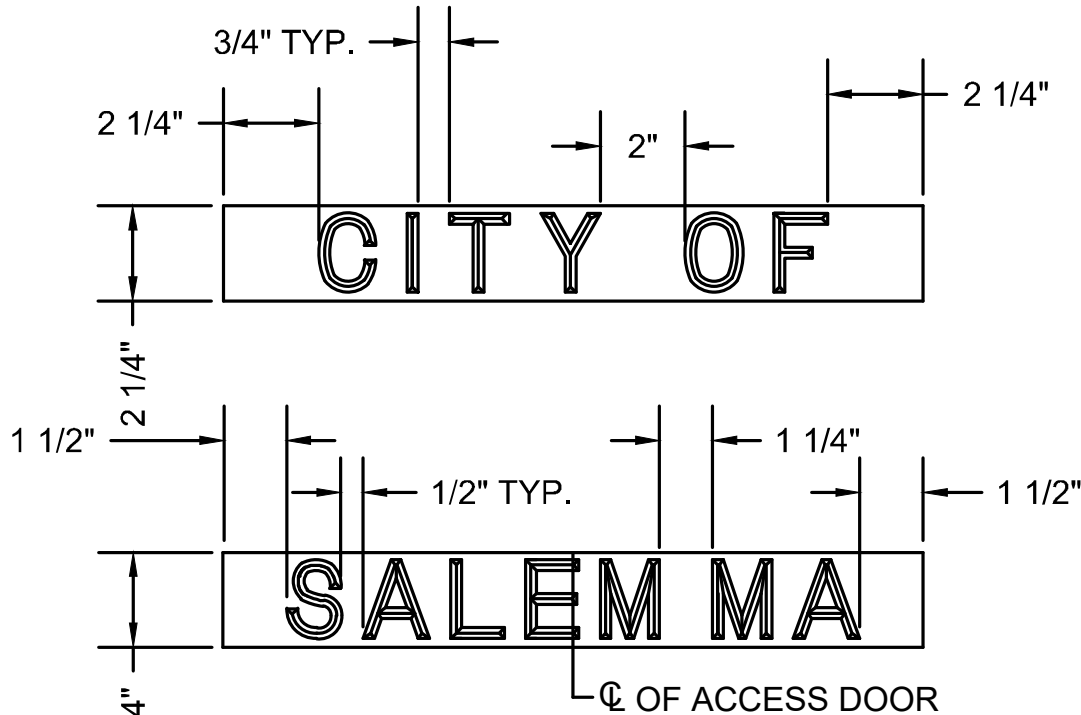
GROUND STUD PROVISIONS:	1/4"-20 SQUARE NUT WELDED TO INSIDE WALL OF SHAFT OPPOSITE HAND HOLE FOR GROUND STUD (STUD BY OTHERS)
ANCHOR BOLTS:	(4) 1" DIA. X 36" + 4" HOOK (FULLY GALVANIZED WITH 2 GALVANIZED NUTS AND 2 GALVANIZED WASHERS PER BOLT)
BOLT PROJECTION:	5" REQUIRED



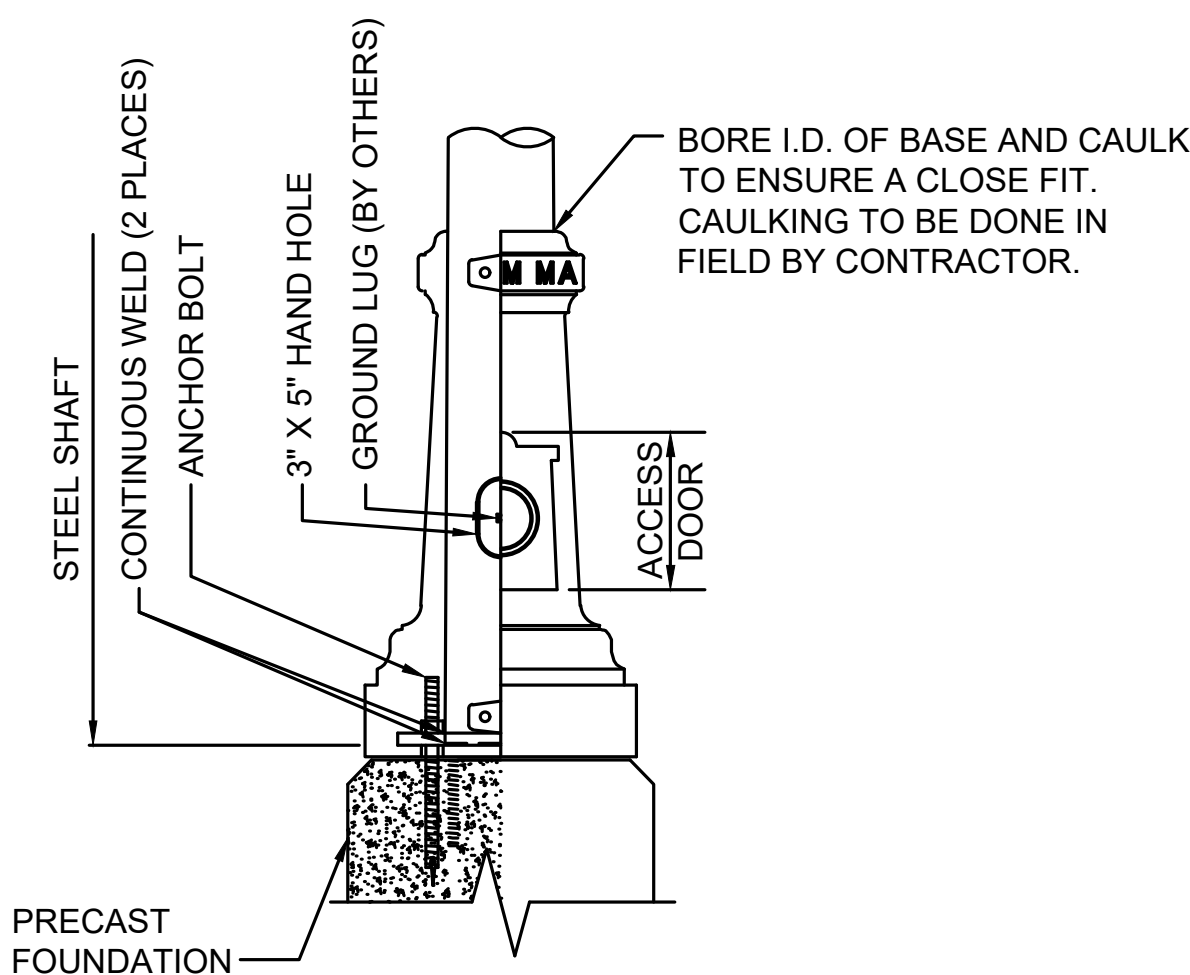
SECTION A-A
N.T.S.



LOGO DETAIL
N.T.S.



LETTERING DETAIL
N.T.S.



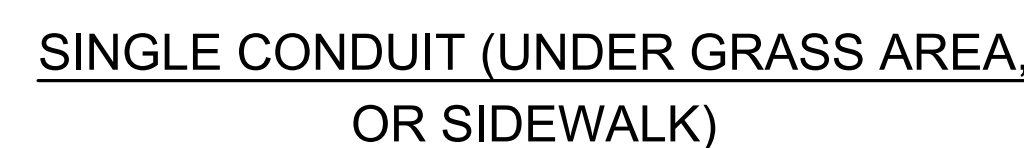


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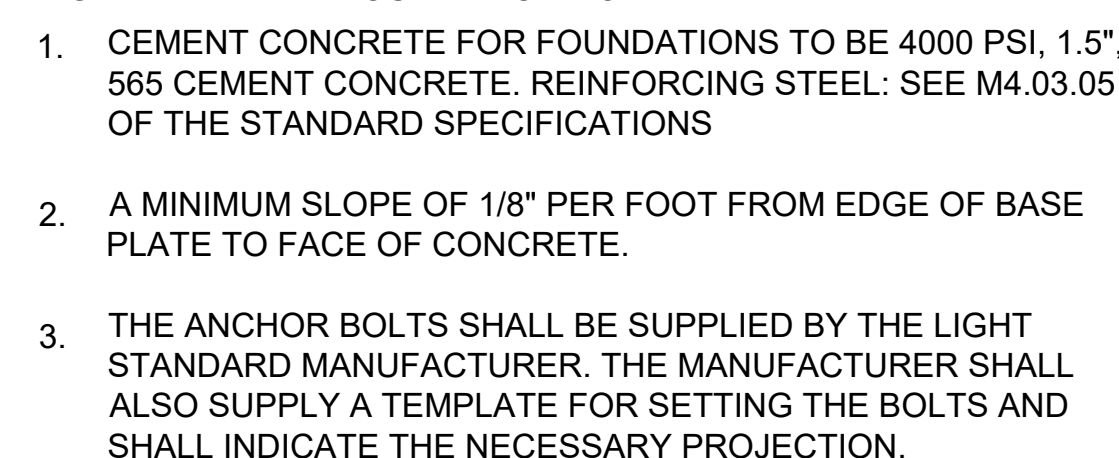
Legend
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Notes
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NOT TO SCALE

1. FOR TRENCH CUTS WITHIN PAVED SURFACES, (MAINLINE AND LOCAL ROADS) WHERE SUCH TRENCH CUTS HAVE A DEPTH OF 4' OR LESS, CONTROLLED DENSITY FILL (CDF) FOR BACKFILL IS MANDATORY. CONTROLLED DENSITY FILL TYPE 2E, (FLOWABLE-EXCAVATABLE) SHALL MEET THE REQUIREMENTS OF SECTION M4.08.0



NOT TO SCALE

1. ALL CONDUIT ENTERING HANDHOLES SHALL EXTEND INTO THE HANDHOLE CAVITY BY AT LEAST 2".
2. ALL HANDHOLES SHALL BE PRECAST CONCRETE REINFORCED WITH STEEL, AND SHALL BE FREE OF CRACKS OR OTHER DEFECTS. CONCRETE FOR PRECAST CONCRETE HANDHOLES SHALL BE 5000 PSI, 3/8 IN., 660 CEMENT CONCRETE MASONRY. REINFORCING BARS SHALL CONFORM TO AASHTO DESIGNATION M31 (ASTM DESIGNATION A 615) GRADE 60. AND SHALL BE EPOXY COATED.
3. CAST IRON FRAME SHALL BE MORTARED TO THE SURFACE OF THE PRECAST HANDHOLE.
4. ALL CONDUIT INSTALLED IN HANDHOLES SHALL BE INSTALLED IN KNOCKOUTS PROVIDED IN THE BOX AND NO EXCESS KNOCKOUTS SHALL BE MADE. THE KNOCKOUTS ARE DESIGNED TO BE MADE PRIOR TO BACKFILLING AROUND THE HANDHOLES. AFTER THE CONDUIT HAS BEEN INSTALLED IN THE HANDHOLE, THE OPEN SPACE BETWEEN THE BOX AND THE CONDUIT WILL BE SEALED WITH CLASS B CEMENT CONCRETE MASONRY. ANY CONDUIT INSTALLED IN SUCH A MANNER AS TO BLOCK COMPLETE ACCESS TO ANY OTHER CONDUIT SHALL BE REMOVED AND RESET.
5. FOR THE EXACT NUMBER, SIZE, AND ORIENTATION OF THE CONDUITS ENTERING THE HANDHOLE, SEE PLAN SHEETS.
6. CAST IRON HANDHOLE COVERS AND FRAMES SHALL BE PRODUCED BY THE SAME MANUFACTURER AND IN ACCORDANCE WITH MASSDOT SPECIFICATIONS.
7. METAL HANDHOLE COVER AND FRAME ARE TO BE BONDED (GROUNDED). DRILL AND TAP FRAME FOR MECHANICAL GROUND LUG CONNECTION. INSTALL A SERVIT POST GROUNDING CONNECTOR SUITABLE TO ACCOMMODATE ONE BARE #6 CONDUCTOR. EXTEND #6 GROUND CONDUCTOR THROUGH FRAME LUG TO BOND COVER. PROVIDE 42" OF SLACK FROM FRAME LUG TO BOND COVER. EXOTHERMICALLY WELD GROUND CONDUCTOR TO COVER.
8. FUNCTION DESIGNATION ON THE HANDHOLE SHALL BE LABELED AS FOLLOWS PER NEC ARTICLE 314.30D:

"LIGHTING" FOR LIGHTING CONDUIT

[illegible]

CITY OF SALEM
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BRIDGE STREET RECONSTRUCTION PROJECT

Title
LIGHTING DETAILS
PART 2 OF 4

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BRIDGE STREET RECONSTRUCTION PROJECT

Title
LIGHTING DETAILS
PART 3 OF 4

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—HANDHOLE

10 1/2" DIA.
X 3/4" THICK

4" O.D. OF
STEEL SHAFT

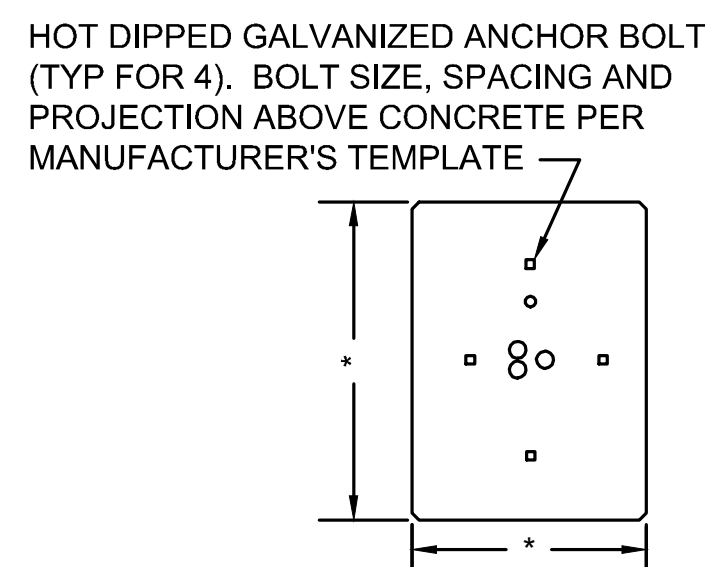
8" DIA.
BOLT CIRCLE

1/2" OF CROSS ARM

BASE PLATE DETAIL
N.T.S.

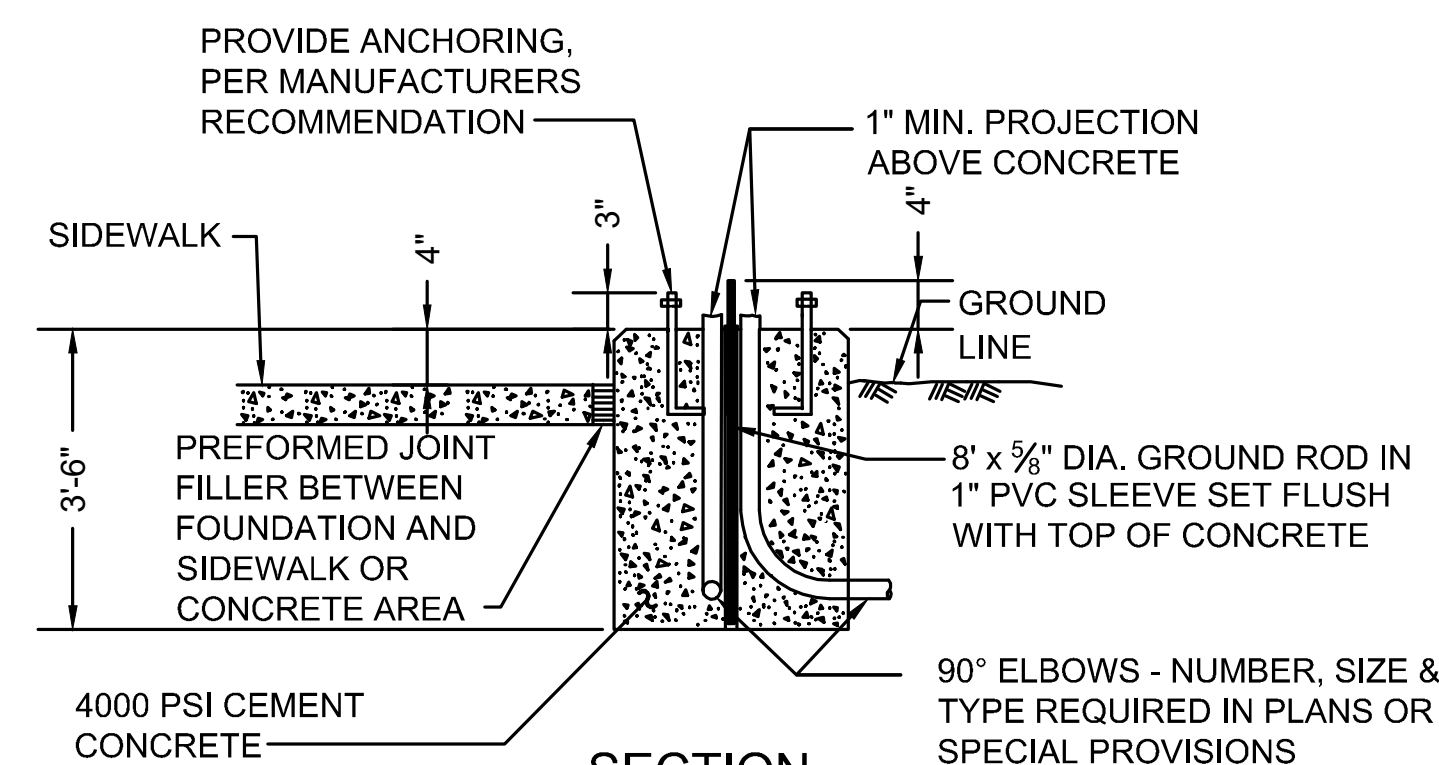
PEDESTRIAN LIGHT POLE

NOT TO SCALE



* FOUNDATION SHALL BE 6" WIDER AND LONGER THAN CABINET BASE TO BE INSTALLED

PLAN



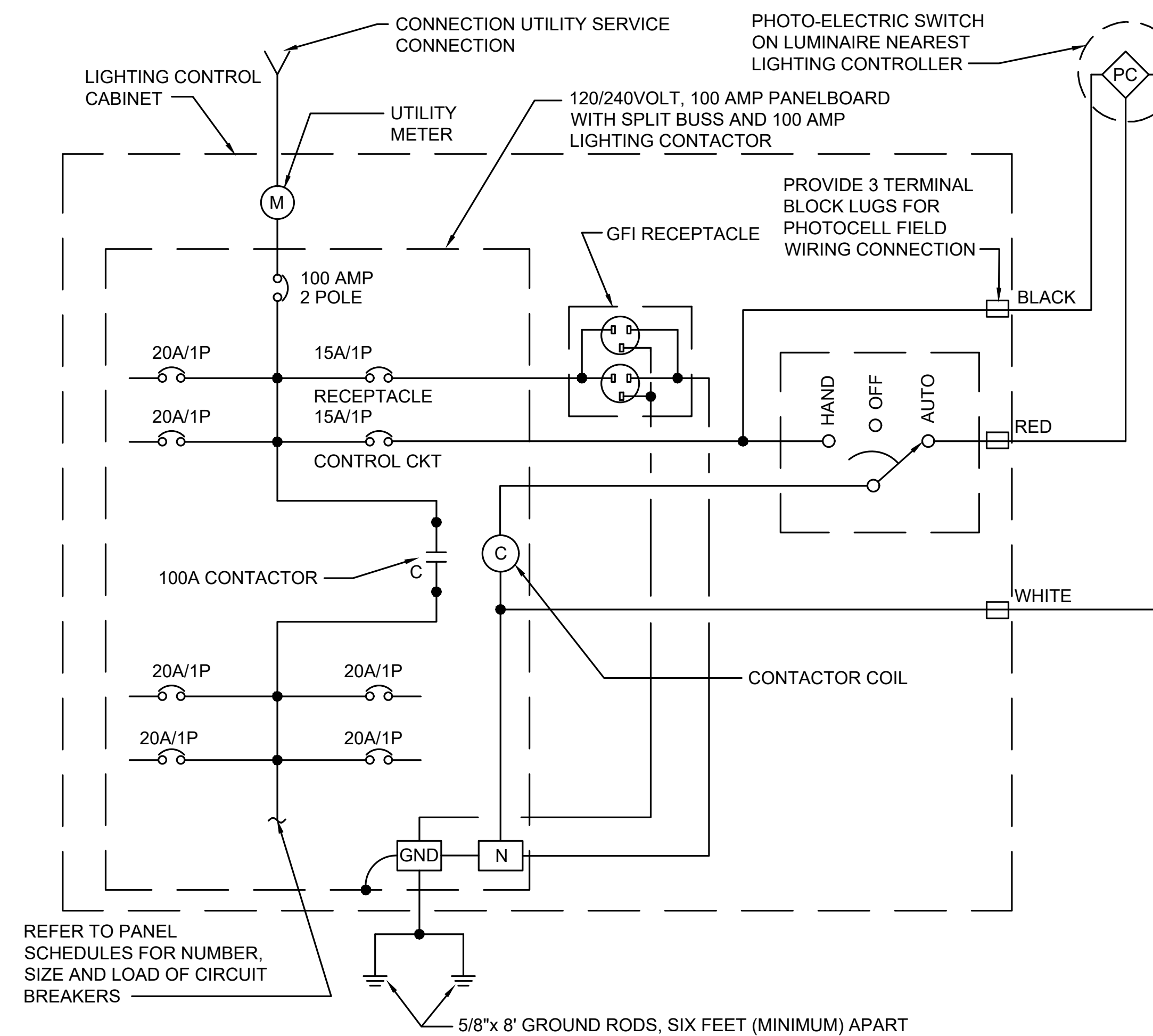
SECTION

NOTES:

1. ALL EXPOSED EDGES SHALL BE CHAMFERED $\frac{3}{4}$ ".
2. CONTRACTOR SHALL MAINTAIN ACCESS TO BOTH SIDES OF LOAD CENTER. FRONT FOR CUSTOMER ACCESS, BACK FOR UTILITY ACCESS. CONTRACTOR MAY ROTATE LOAD CENTER 90 DEGREES IF NECESSARY TO MAINTAIN ACCESS.
3. CONTRACTOR MUST MAINTAIN SIDEWALK CLEARANCE OF 3' MINIMUM AROUND CONCRETE FOUNDATION

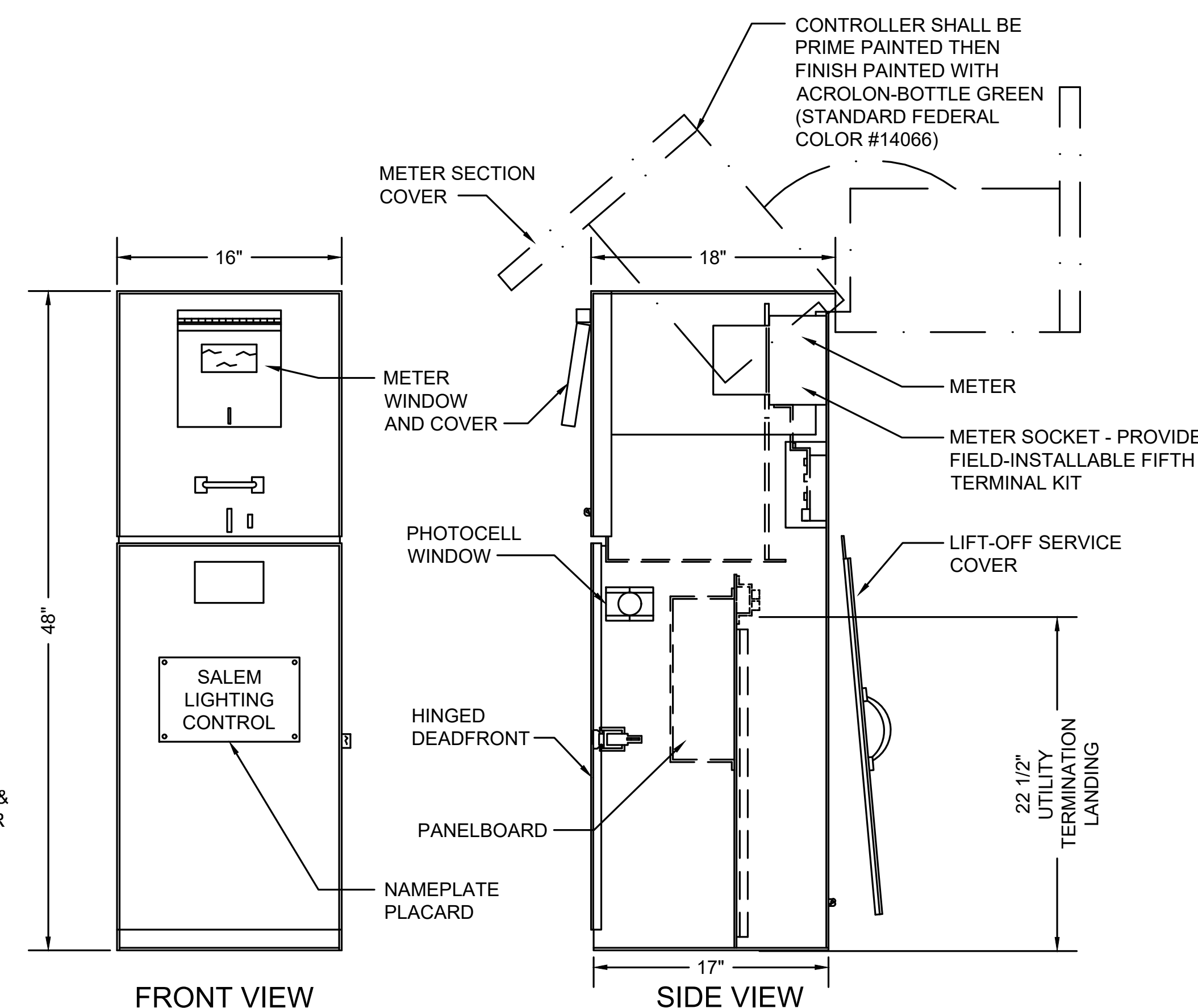
CONCRETE FOUNDATION FOR LIGHTING CONTROL CABINET

NOT TO SCALE



ONE LINE DIAGRAM

NOT TO SCALE



FRONT VIEW

SIDE VIEW

LIGHTING CONTROL CABINET DETAIL

NOT TO SCALE

NOTES:

1. PROVIDE MILBANK CP3B51110A22SL1 MODIFIED WITH 100 AMP CONTACTOR, PHOTOCELL TERMINALS, FIFTH TERMINAL KIT FOR USE WITH RINGLESS METER SOCKETS (COORDINATE WITH ELECTRIC UTILITY FOR METER SOCKET REQUIREMENTS), MOUNTING BASE, ANCHOR BOLT KIT, GFCI RECEPTACLE, AND INTERNAL LIGHT FIXTURE OR EQUIVALENT BY MYERS POWER PRODUCTS, OR VIT STRONG BOX.



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BRIDGE STREET RECONSTRUCTION PROJECT

Drawing No.	Sheet	Revision
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BUS: 120/240V, 1PH, 3W, 60HZ
MAIN LUGS ONLY : 100A
FEEDER SIZE: 3#2, 1#2-GND

SHORT CIRCUIT BRACING: 10-KAIC
CABINET: SURFACE
PANEL LOCATION: GROVE ST STA 104+71 LT

LOAD SERVED	HP	VA A	VA B	CKT. NO.	NEUTRAL		CKT. NO.	VA A	VA B	HP	LOAD SERVED
MAIN		-		1	100A	A B	2	50			CONTROL CKT
MAIN			-	3	100A		4		180		GFI RECEPTACLE
SPARE		-		5			6	-			SWITCHED LOAD CENTER (BELOW)
SPARE			-	7			8		-		SWITCHED LOAD CENTER (BELOW)
SPARE		-		9			10	-			SPARE
SPACE			-	11			12		-		SPACE
SPACE		-		13			14	-			SPACE
SPACE			-	15			16		-		SPACE

					GROUND BAR						
LOAD SERVED	HP	VA A	VA B	CKT. NO.	NEUTRAL A B	CKT. NO.	VA A	VA B	HP	LOAD SERVED	
LUMINAIRES - LIGHT POLES 02,10,12,14		620		17		18	720			RECEPTACLES - LIGHT POLES 02,10,12,14	
LUMINAIRES - LIGHT POLES 01,11,13			465	19		20		540			RECEPTACLES - LIGHT POLES 01,11,13
LUMINAIRES - LIGHT POLES 03, 05, 08		465		21		22	540				RECEPTACLES - LIGHT POLE 03, 05, 08
LUMINAIRES - LIGHT POLES 04, 06, 07, 09			620	23		24		720			RECEPTACLES - LIGHT POLES 04, 06, 07, 09
SPARE		-		25		26	-				SPARE
SPACE			-	27		28		-			SPACE
SPACE		-		29		30	-				SPACE
SPACE			-	31		32		-			SPACE

VA A: 2395	VA B: 2525	ALL BRANCH C/B: 20 AMP TRIP
TOTAL CONNECTED LOAD: 4920 VA		UNLESS OTHERWISE NOTED

BUS: 120/240V, 1PH, 3W, 60HZ
MAIN LUGS ONLY : 100A
FEEDER SIZE: 3#2, 1#2-GND

SHORT CIRCUIT BRACING: 10-KAIC
CABINET: SURFACE
PANEL LOCATION: GROVE ST STA 110+25 RT

LOAD SERVED	HP	VA A	VA B	CKT. NO.	NEUTRAL		CKT. NO.	VA A	VA B	HP	LOAD SERVED
MAIN		-		1	100A	A B	2	50			CONTROL CKT
MAIN			-	3	100A		4		180		GFI RECEPTACLE
SPARE		-		5			6	-			SWITCHED LOAD CENTER (BELOW)
SPARE			-	7			8		-		SWITCHED LOAD CENTER (BELOW)
SPARE		-		9			10	-			SPARE
SPACE			-	11			12		-		SPACE
SPACE		-		13			14	-			SPACE
SPACE			-	15			16		-		SPACE

					GROUND BAR						
LOAD SERVED	HP	VA A	VA B	CKT. NO.	NEUTRAL A B		CKT. NO.	VA A	VA B	HP	LOAD SERVED
LUMINIARES - LIGHT POLES 02, 04		310		17			18	360			RECEPTACLES - LIGHT POLES 02, 04
LUMINIARES - LIGHT POLES 01, 03			310	19			20		360		RECEPTACLES - LIGHT POLES 01, 03
LUMINIARES - LIGHT POLES 05, 07, 09, 11,		525		21			22	720			RECEPTACLES - LIGHT POLES 05, 07, 09, 11,
LUMINIARES - LIGHT POLES 06, 08, 10, 12			430	23			24		720		RECEPTACLES - LIGHT POLES 06, 08, 10, 12
SPARE		-		25			26	-			SPARE
SPACE			-	27			28		-		SPACE
SPACE		-		29			30	-			SPACE
SPACE			-	31			32		-		SPACE

VA A: 1915	VA B: 1820	ALL BRANCH C/B:	20 AMP TRIP
TOTAL CONNECTED LOAD: 3735 VA		UNLESS OTHERWISE NOTED	



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BRIDGE STREET RECONSTRUCTION PROJECT

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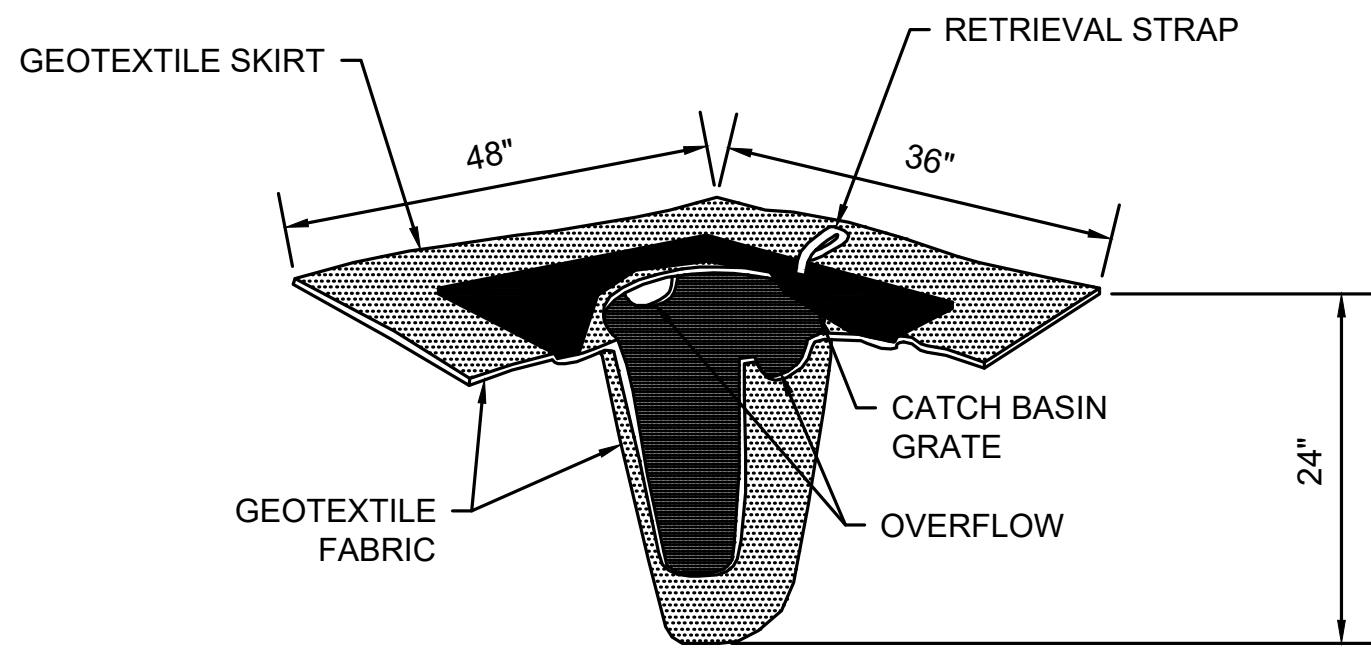
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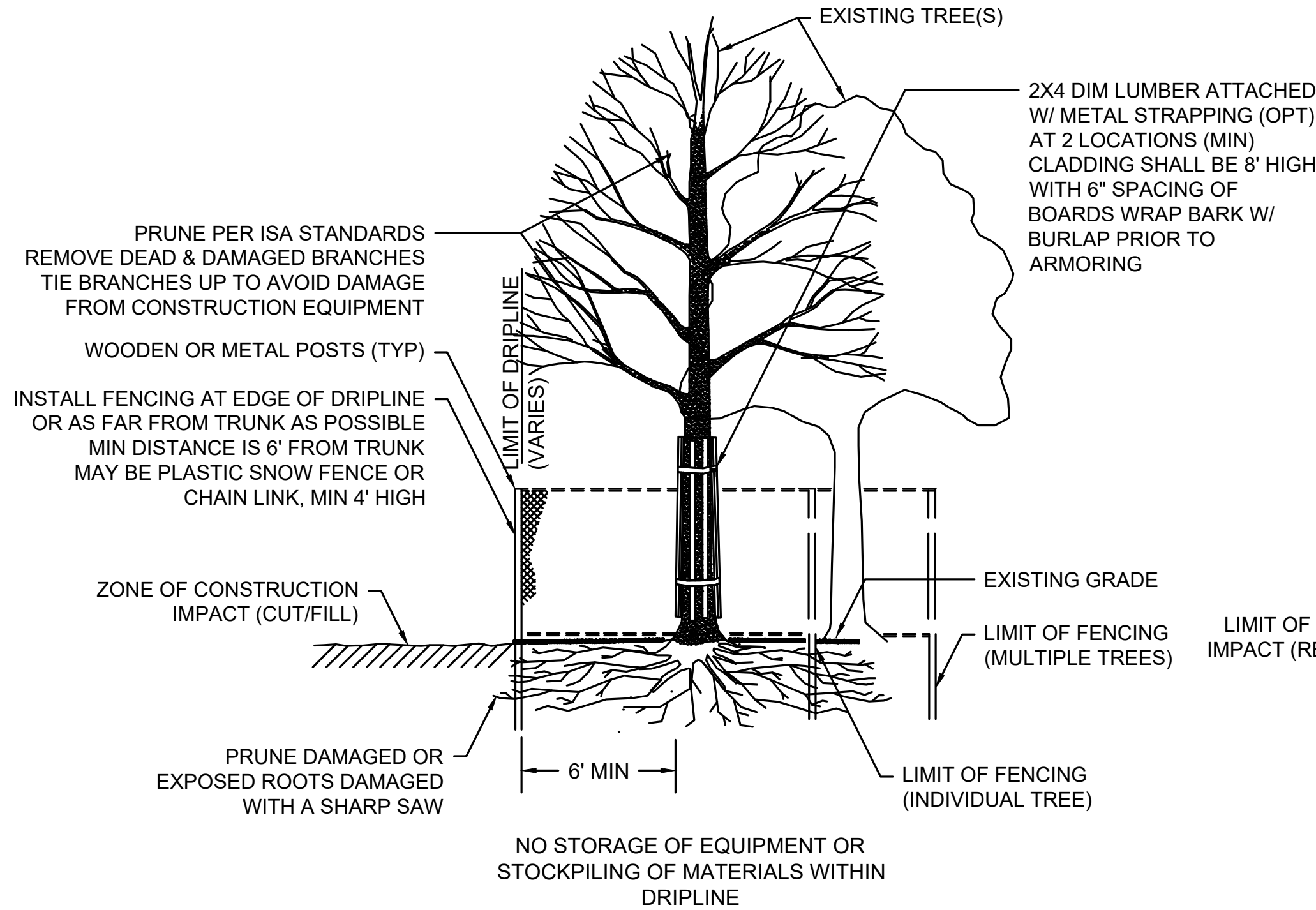
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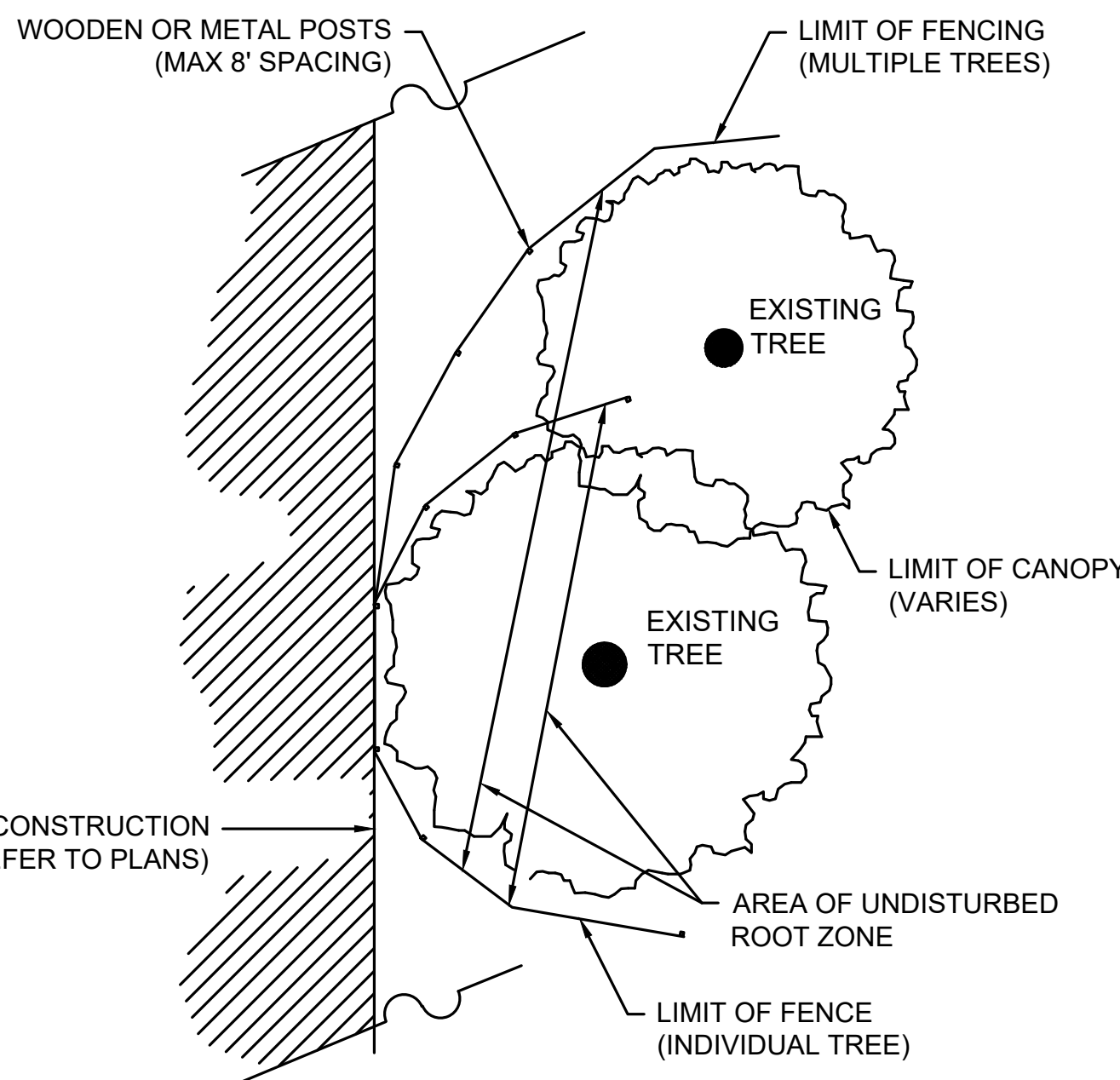
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CATCH BASIN INSERT FOR SEDIMENT CONTROL
(SILT SACK)
NOT TO SCALE



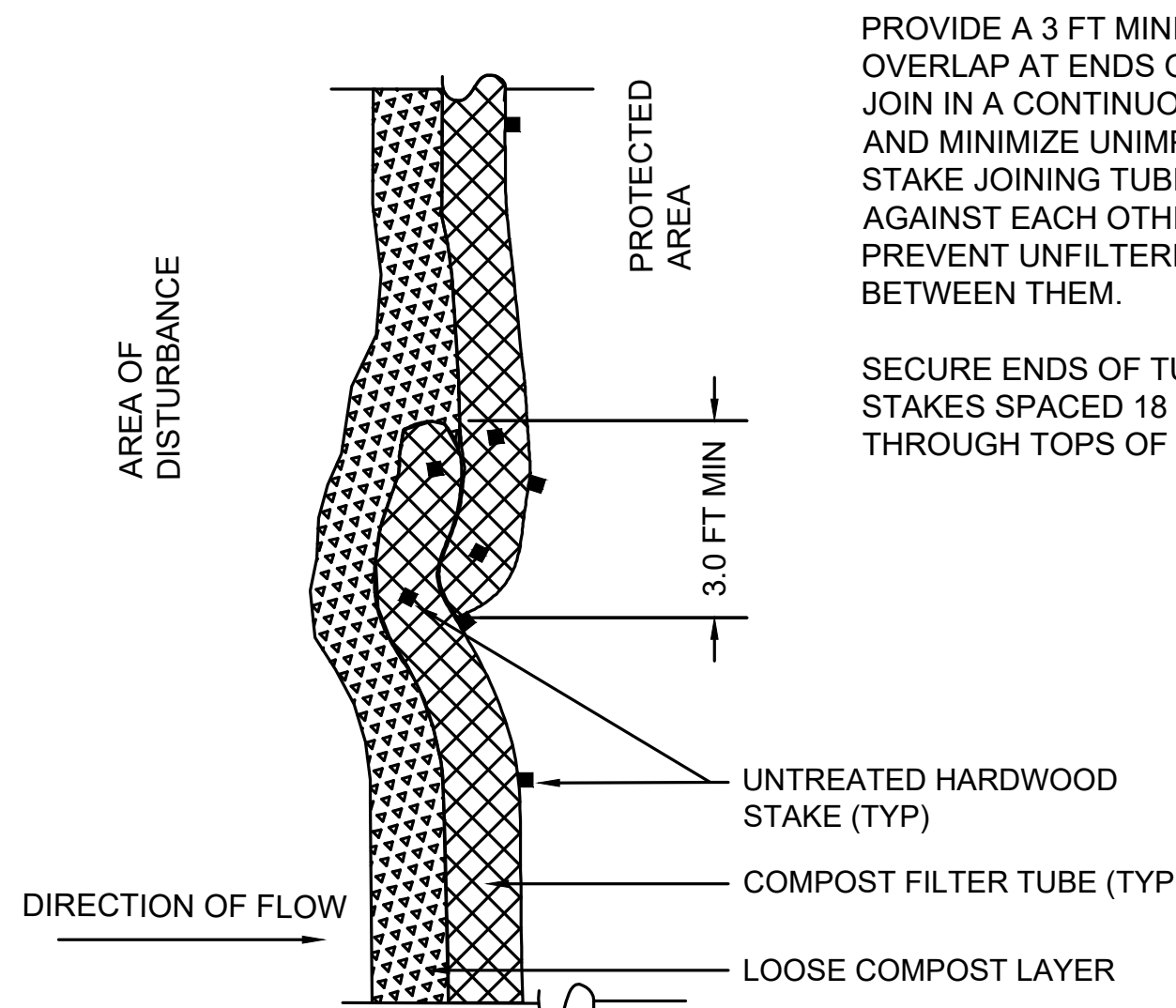
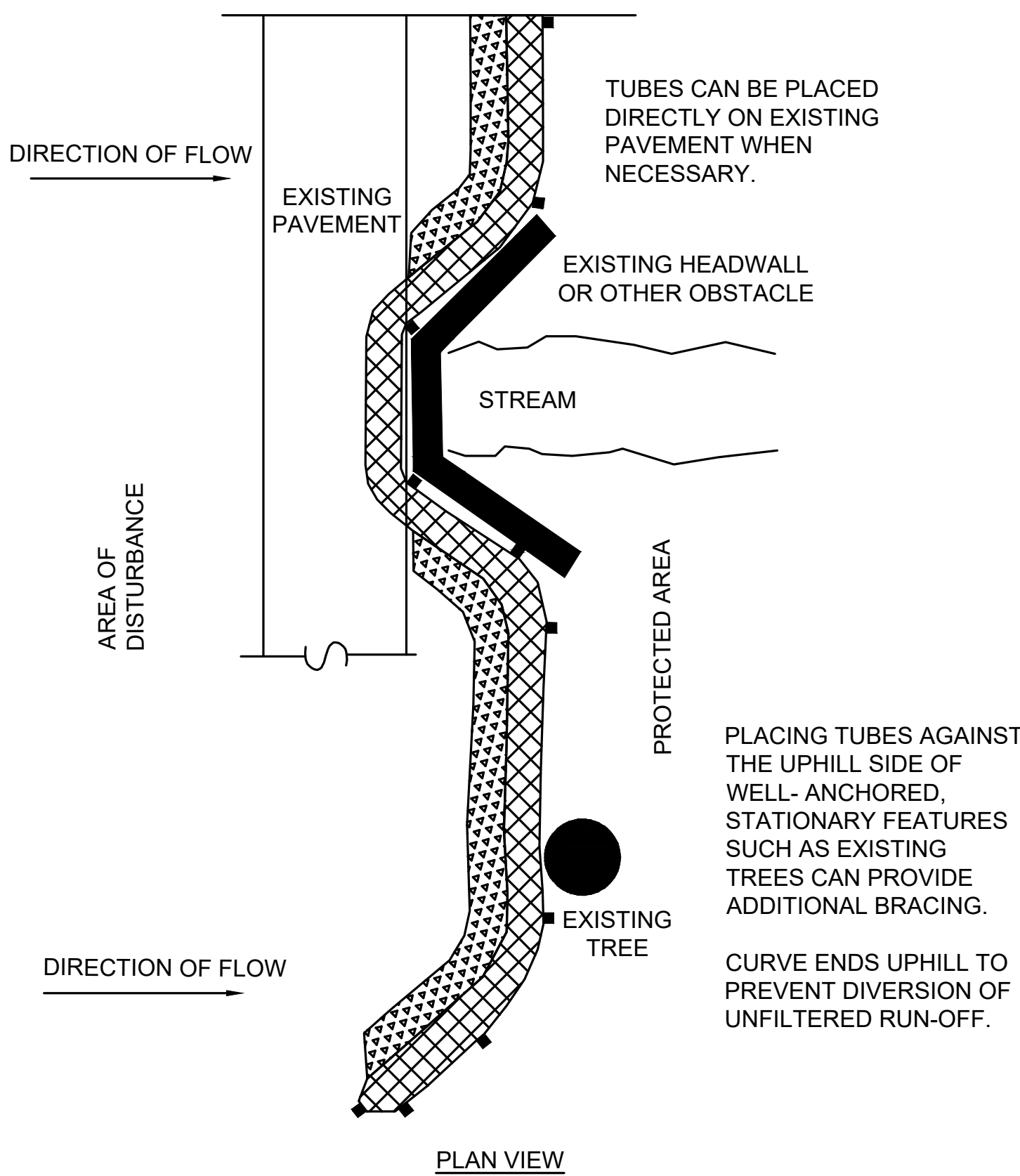
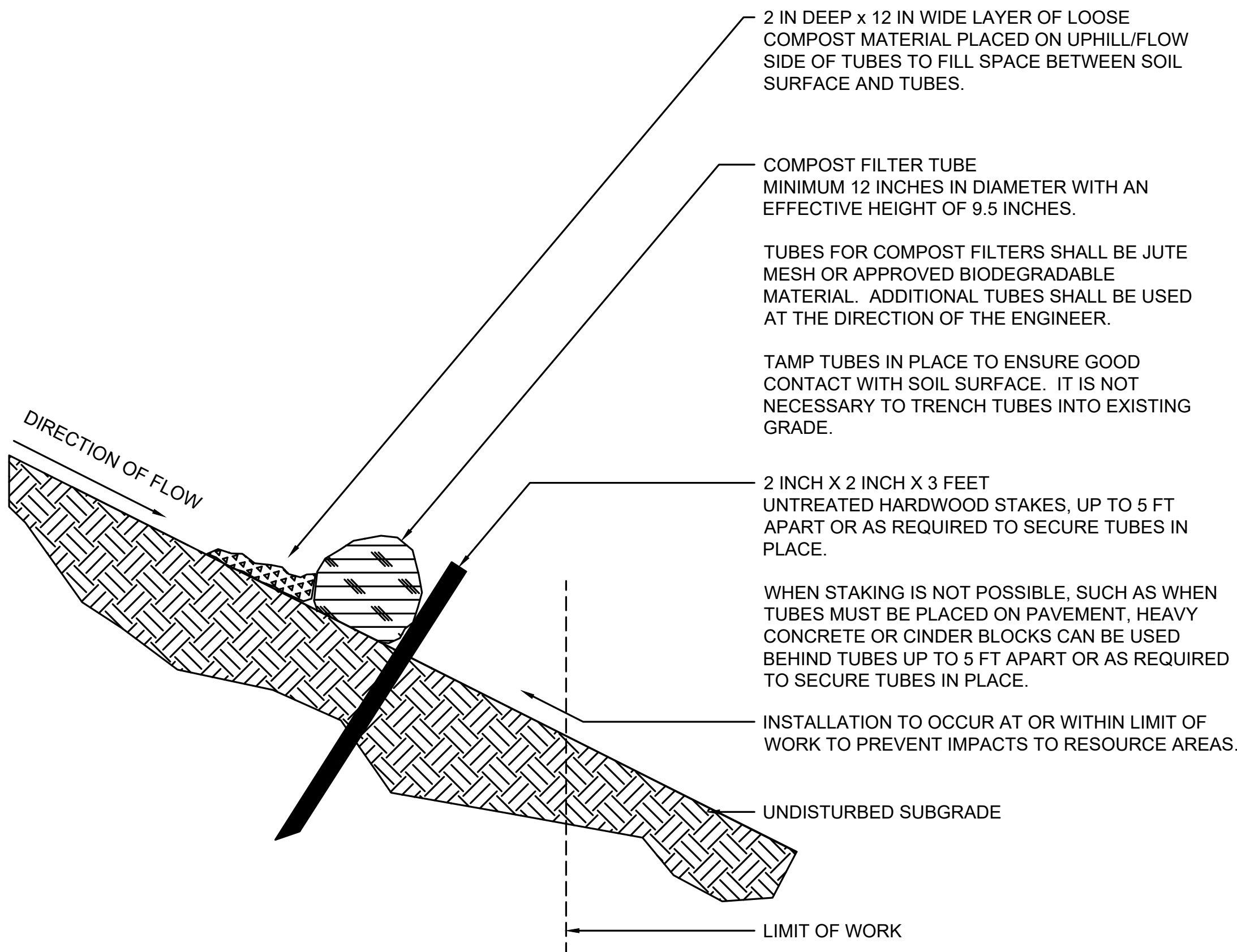
TREE PROTECTION - EXISTING TREE(S)
NOT TO SCALE



PLAN VIEW

NOTES:

1. PROVIDE A MINIMUM TUBE DIAMETER OF 12 INCHES FOR SLOPES UP TO 50 FEET IN LENGTH WITH A SLOPE RATIO OF 3H:1V OR STEEPER. LONGER SLOPES OF 3H:1V MAY REQUIRE LARGER TUBE DIAMETER OR ADDITIONAL COURSING OF FILTER TUBES TO CREATE A FILTER BERM. REFER TO MANUFACTURER'S RECOMMENDATIONS FOR SITUATIONS WITH LONGER OR STEEPER SLOPES.
2. INSTALL TUBES ALONG CONTOURS AND PERPENDICULAR TO SHEET OR CONCENTRATED FLOW.
3. DO NOT INSTALL IN PERENNIAL, EPHEMERAL OR INTERMITTENT STREAMS.
4. CONFIGURE TUBES AROUND EXISTING SITE FEATURES TO MINIMIZE SITE DISTURBANCE AND MAXIMIZE CAPTURE AREA OF STORMWATER RUN-OFF.



Revision

By

Appd.

YY.MM.DD

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YY.MM.DD

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BRIDGE STREET RECONSTRUCTION PROJECT

Title

CONSTRUCTION DETAILS
PART 2 OF 4

179410455

Project No.

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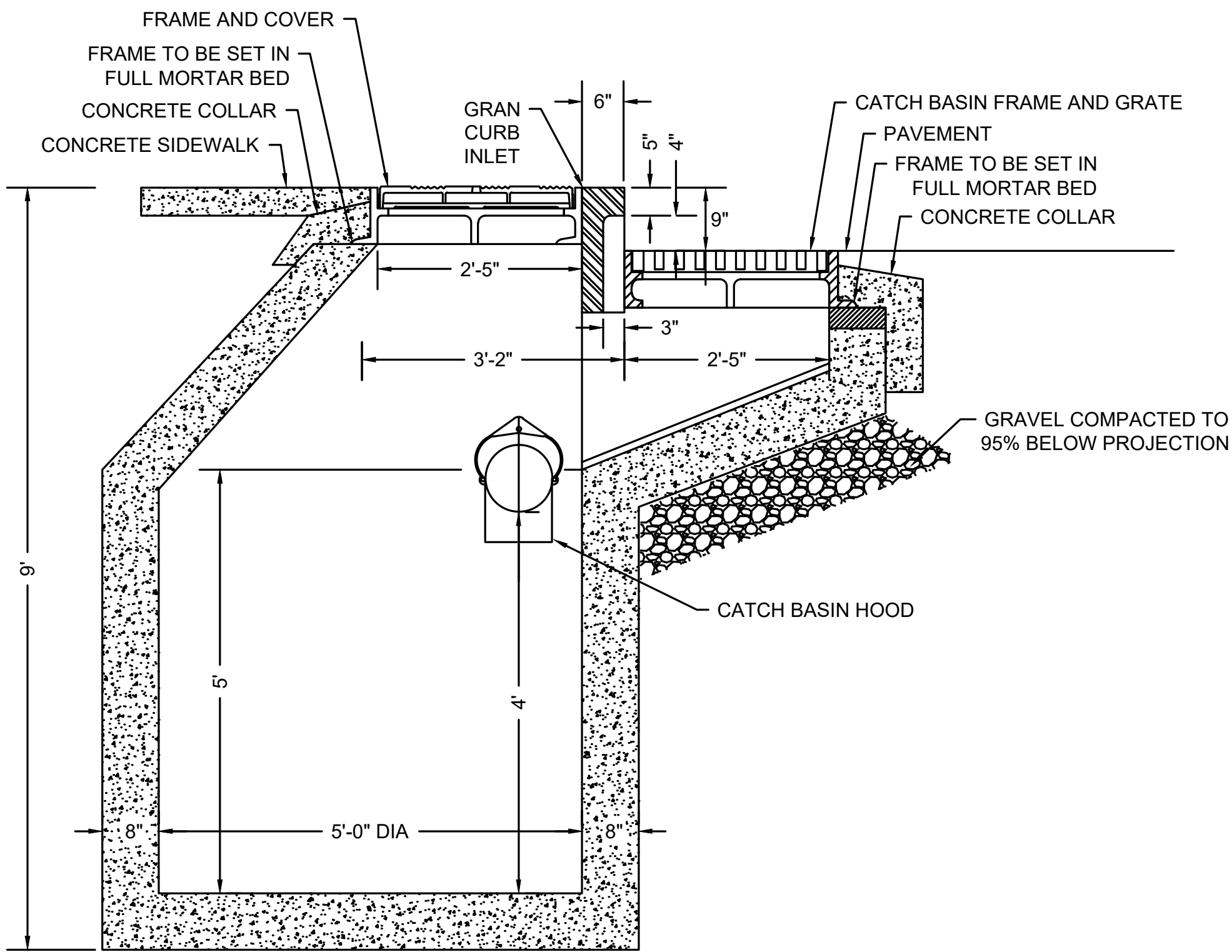
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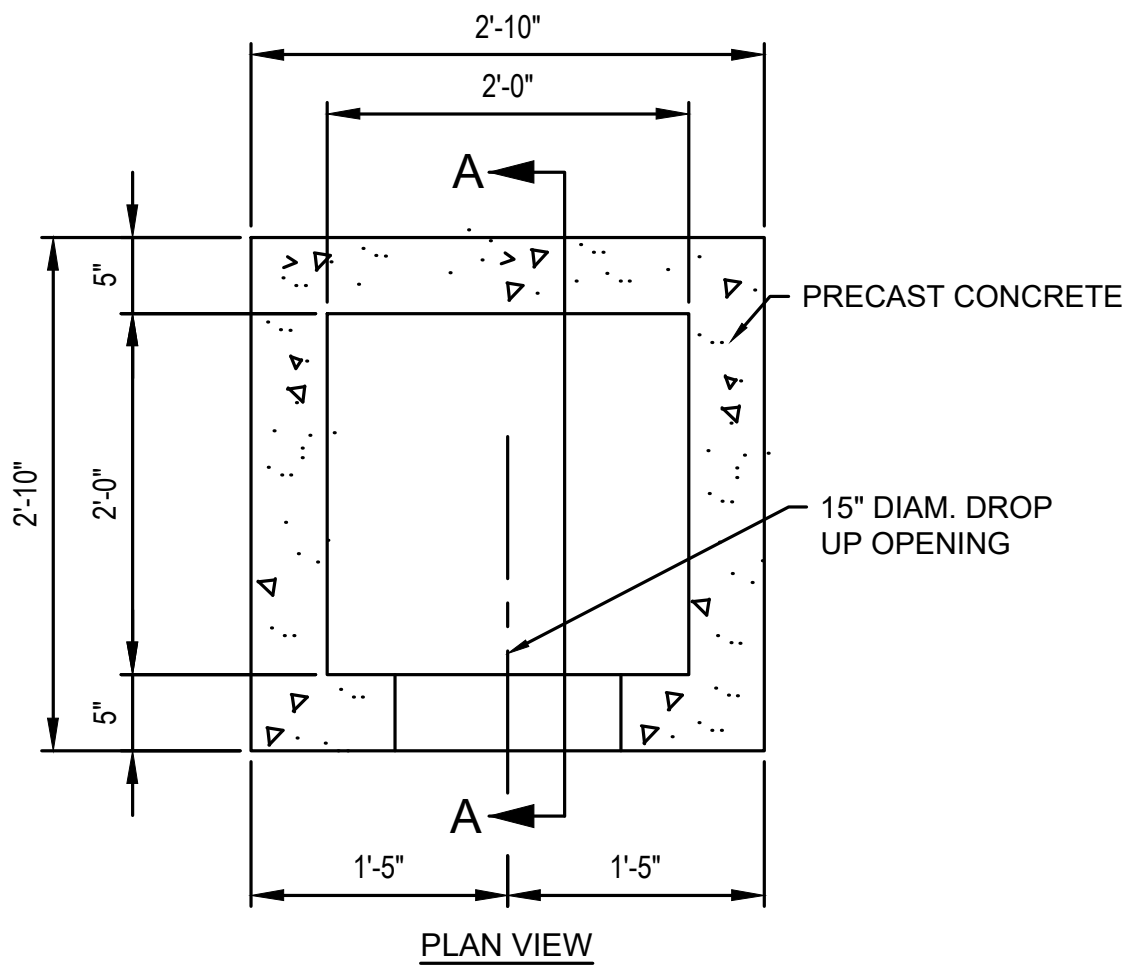
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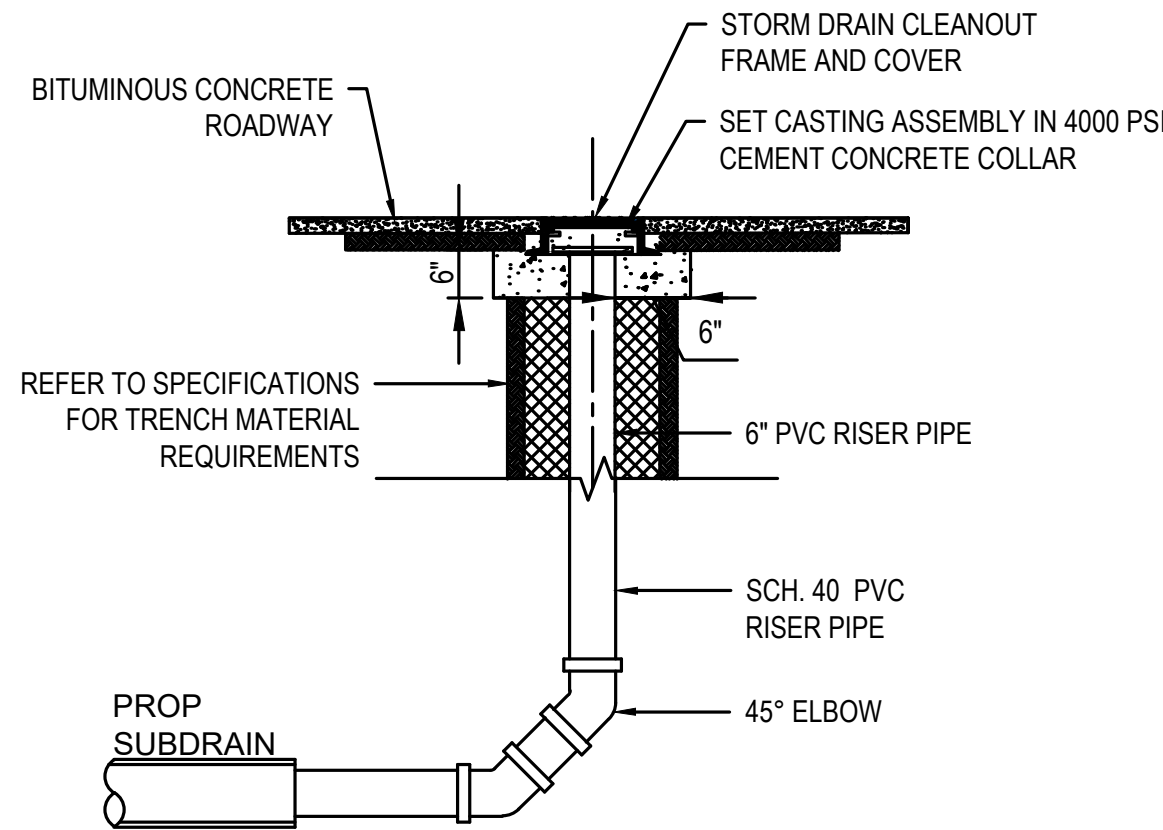


NOTE: ALL SECTIONS SHALL BE DESIGNED FOR HS-20 LOADING.

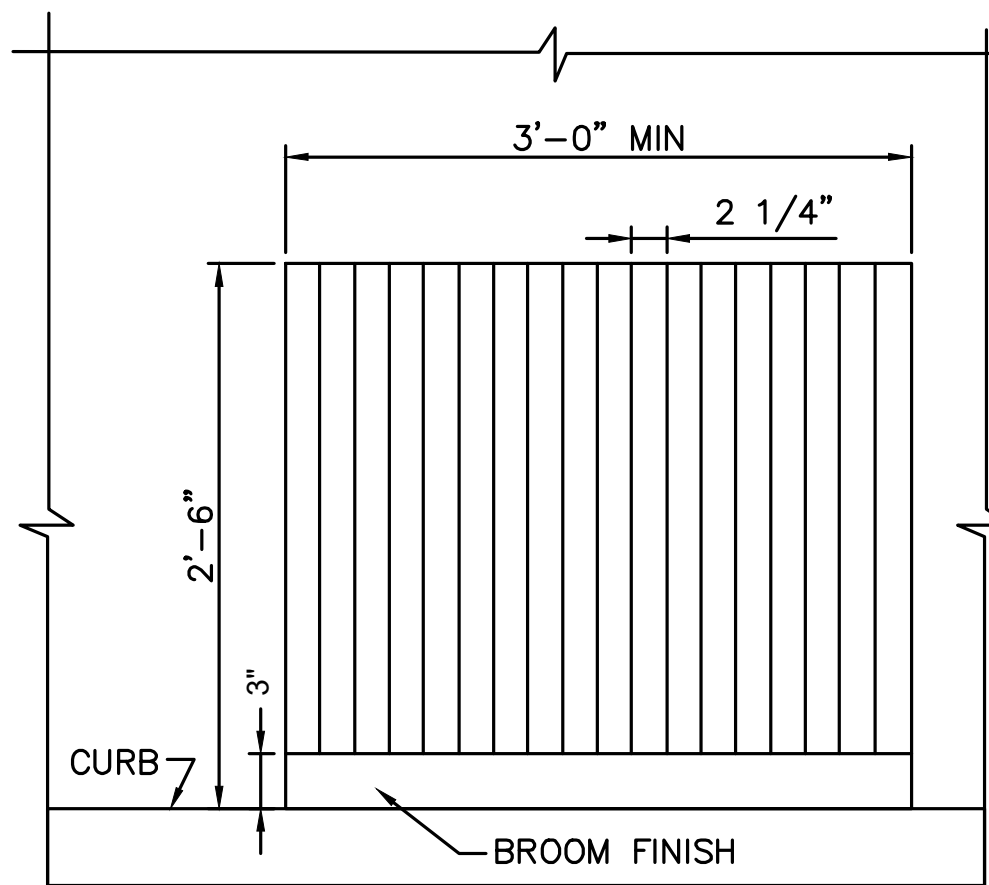
SPECIAL MANHOLE - GUTTER INLET / MANHOLE STRUCTURE
NOT TO SCALE



GUTTER INLET - SPECIAL (PRECAST)
NOT TO SCALE

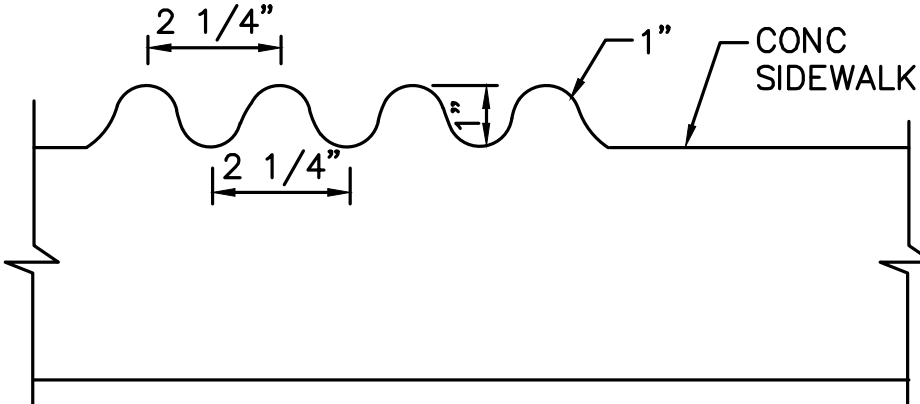


SUBDRAIN CLEANOUT DETAIL
NOT TO SCALE

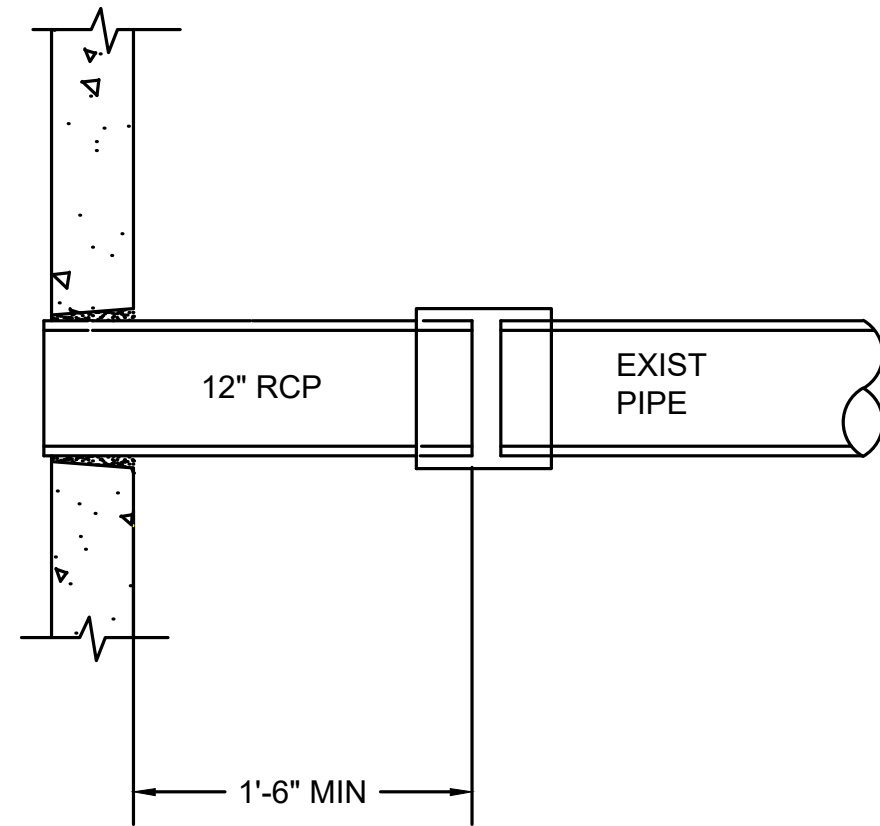


NOTES:

- REFERENCE MASSDOT CONSTRUCTION STANDARD E 105.2.0.



SCORED CONCRETE DETAIL FOR BIKE RAMPS
NOT TO SCALE



PIPE CONNECTION DETAIL
NOT TO SCALE

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BRIDGE STREET RECONSTRUCTION PROJECT

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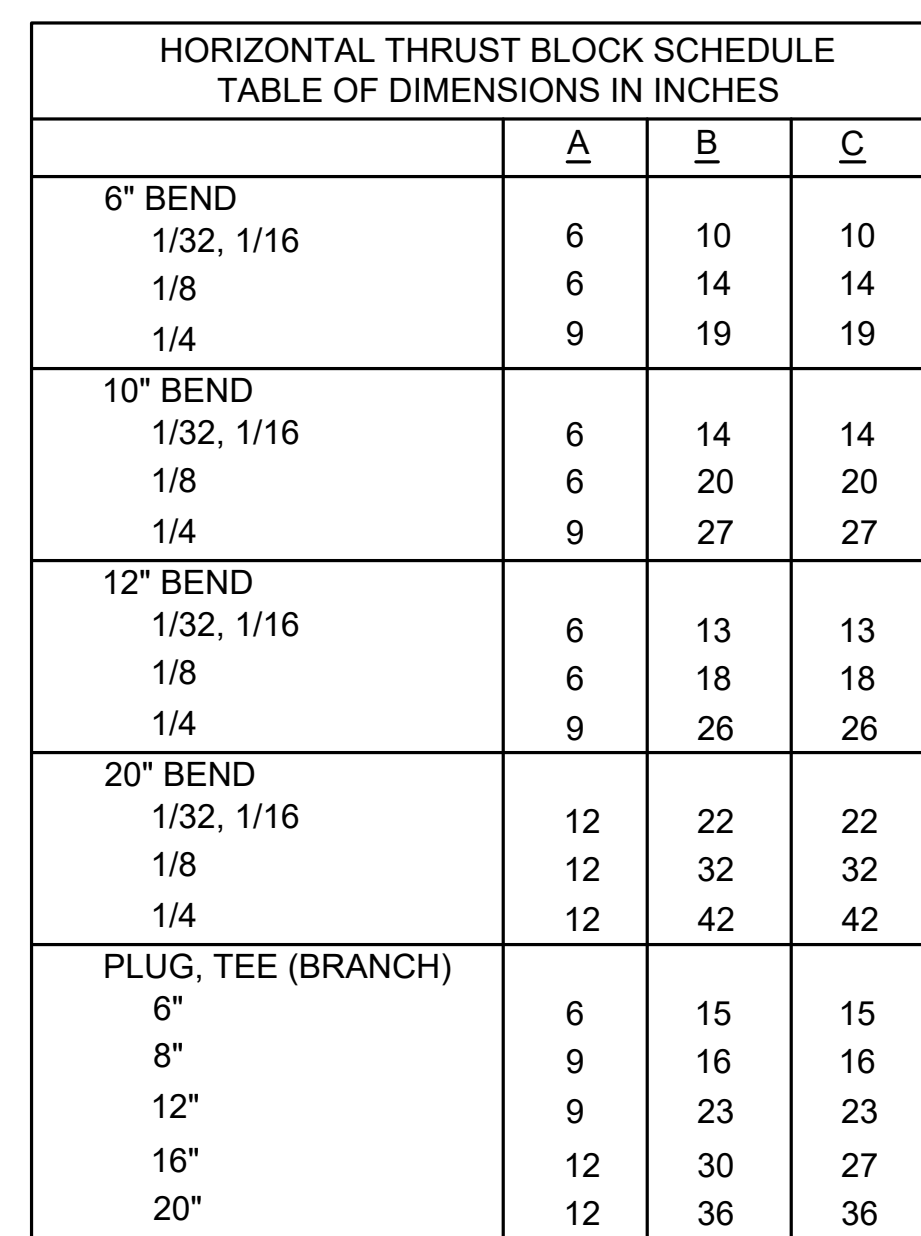
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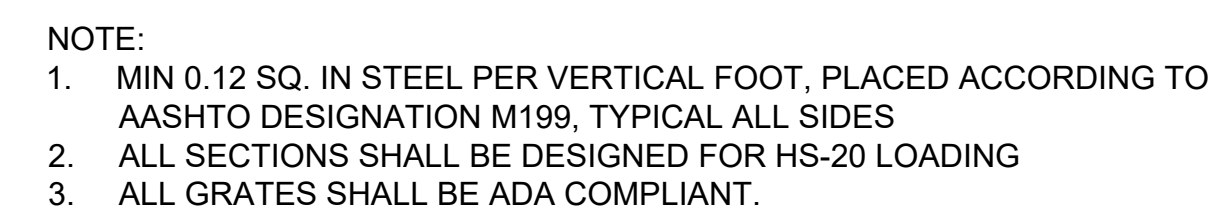
BRIDGE STREET RECONSTRUCTION PROJECT

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THRUST BLOCK DETAIL
NOT TO SCALE



SPECIAL CATCH
BASIN - DOUBLE
GRATE
NOT TO SCALE

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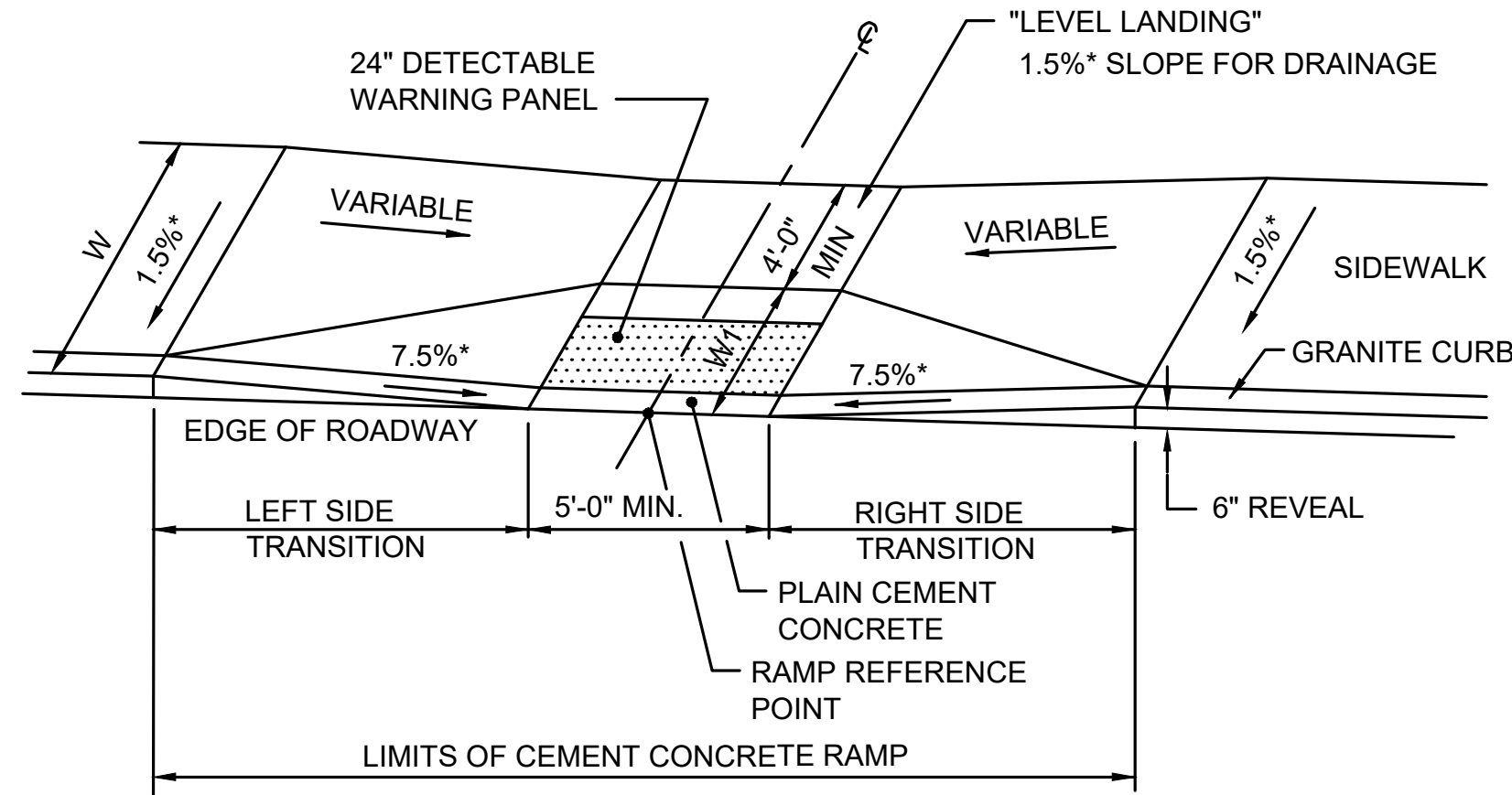
BRIDGE STREET RECONSTRUCTION PROJECT

Title
WHEELCHAIR RAMP AND DRIVEWAY DETAILS
PART 2 OF 2

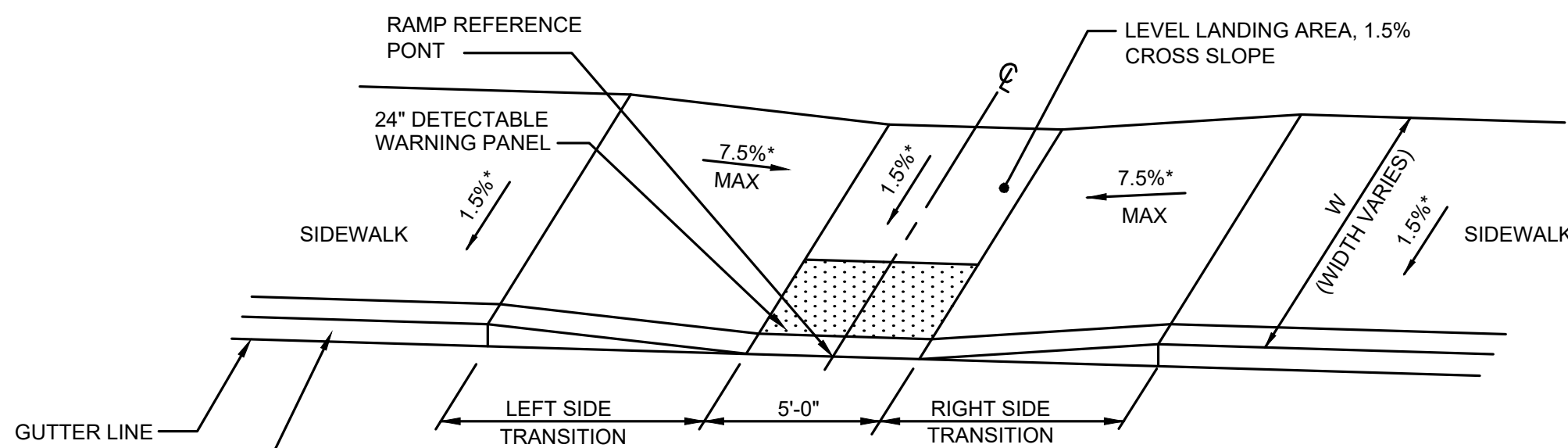
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Project No.	Scale

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Drawing No.	Sheet
	Revision

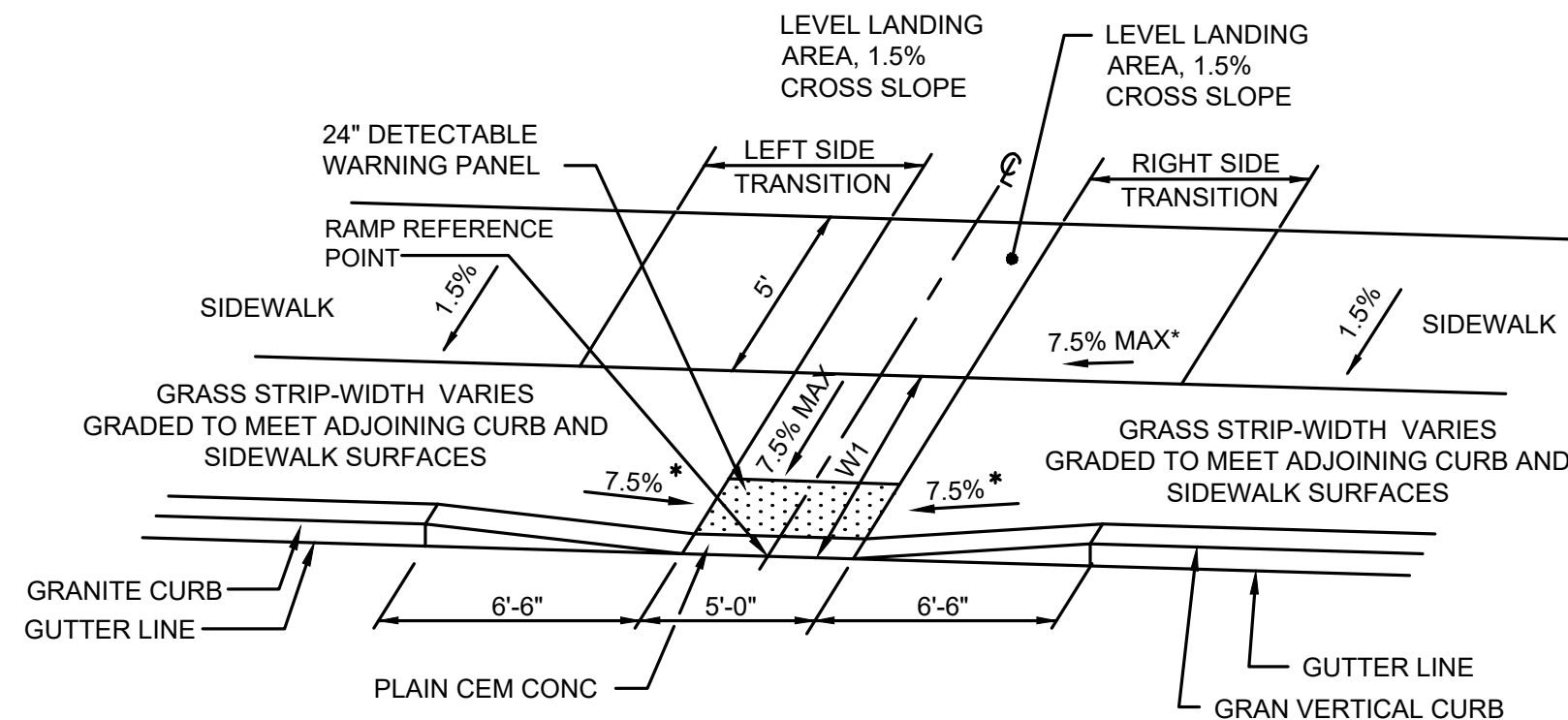
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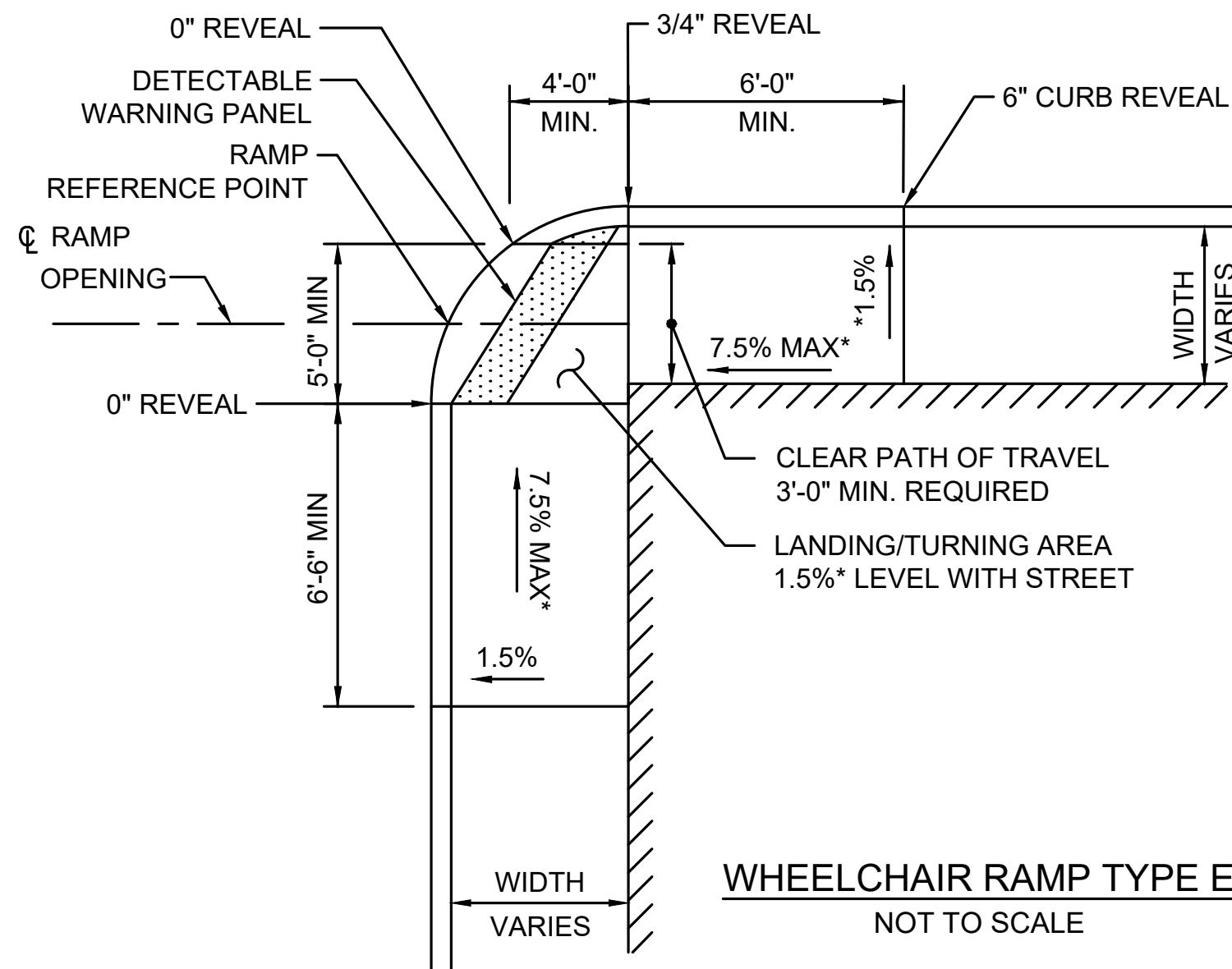
WHEELCHAIR RAMP - TYPE A
NOT TO SCALE



WHEELCHAIR RAMP TYPE C
NOT TO SCALE



WHEELCHAIR RAMP TYPE D
NOT TO SCALE



WHEELCHAIR RAMP TYPE E
NOT TO SCALE

NOTES:

- DETECTABLE WARNING PANELS SHALL BE INSTALLED ON ALL WHEELCHAIR RAMPS AND WHERE SHOWN ON THE DRAWINGS AS PER STANDARD DRAWING E 107.6.5R. DETECTABLE WARNING PANELS SHALL BE BRICK RED IN COLOR.
- UTILITY POLES, LIGHT POLE FOUNDATIONS, MAIL BOXES, AND HYDRANTS THAT ARE TO BE RETAINED OR ADJUSTED SHALL BE REMOVED AND RESET IF FIELD CONDITIONS INDICATE THE CLEAR PATH OF TRAVEL ON THE PROPOSED SIDEWALK IS LESS THAN 36" IN WIDTH.
- ADA/MA AAB REQUIREMENTS SHALL BE FOLLOWED.

WHEELCHAIR RAMPS											
WCR #	TYPE	RAMP REFERENCE POINT			WIDTH OF SIDEWALK (W)	RAMP LENGTH (W1)	WIDTH OF LEVEL LANDING	ROADWAY GUTTER SLOPE	TRANSITION LENGTH		WIDTH OF RAMP LEVELING
		BASELINE	STATION	OFFSET					LEFT SIDE	RIGHT SIDE	
1	A	BOSTON STREET	2+77.68	27.00' RT	VARIES	4.00'	5.00'	4.1%	6'-6"	7'-0" (3" REVEAL)	5'-0"
2	A	PROCTOR STREET	40+49.49	14.01' LT	5.60'	2.50'	3.16'	1.4%	6'-6"	6'-3"	5'-0"
3	A	PROCTOR STREET	40+49.61	14.01' RT	VARIES	2.50'	3.60'	2.30%	4'-8"	6'-5"	5'-0"
4	A	BOSTON STREET	3+55.29	31.00' RT	VARIES	5.00'	VARIES	3.9	6'-6"	6'-0" (3" REVEAL)	5'-0"
5	A	BOSTON STREET	3+62.74	28.18' LT	VARIES	5.00'	VARIES	1.1%	7'-11"	6'-9"	5'-0"
6	A	BRIDGE STREET	100+66.49	19.61' RT	VARIES	VARIES	VARIES	0.90%	7'-10"	9'-6"	5'-0"
7	A	BRIDGE STREET	100+34.34	36.21' LT	VARIES	VARIES	VARIES	7.0%	13'-5"	3'-3" (3" REVEAL)	5'-0"
8	A	GOODHUE STREET	20+23.65	15.18' RT	VARIES	VARIES	VARIES	5.3%	4'-6" (3" REVEAL)	4'-9" (3" REVEAL)	5'-0"
9	A	GOODHUE STREET	20+79.87	11.38' RT	6.86'	3.30'	3.50'	0.4%	3'-10" (3" REVEAL)	6'-6"	5'-0"
10	A	GOODHUE STREET	20+79.75	19.66' LT	VARIES	3.32'	VARIES	1.2%	7'-9"	6'-6"	5'-0"
11	D	BRIDGE STREET	105+02.74	13.00' RT	VARIES	VARIES	VARIES	0.1%	0'-0"	0'-0"	10'-0"
12	D	BRIDGE STREET	105+02.74	24.00' LT	18.81'	8.81'	10.00'	0.1%	0'-0"	0'-0"	10'-0"
13	A	BRIDGE STREET	112+97.57	34.97' LT	12.00'	3.50'	8.50'	1.5%	6'-6"	3'-10" (3" REVEAL)	5'-0"
14	A	FLINT STREET (N)	200+58.93	21.09' LT	12.00'	3.00'	9.00'	0.6%	3'-3" (3" REVEAL)	7'-8"	10'-0"
15	A	BRIDGE STREET	113+07.24	17.15' RT	8.11'	3.50'	VARIES	1.0%	7'-8"	6'-6"	5'-0"
16	C	FLINT STREET (S)	300+35.48	23.73' RT	5.67'	N/A	5.67'	0.60%	7'-6"	9'-10"	5'-0"
17	C	FLINT STREET (N)	201+34.72	25.25' LT	5.00'	N/A	5.00'	0.7%	11'-0"	7'-3"	5'-0"
18	C	FLINT STREET (N)	201+68.52	25.50' LT	5.50'	N/A	5.50'	N/A	10'-3"	N/A	N/A
19	C	FLINT STREET (N)	201+61.87	24.50' RT	VARIES	N/A	N/A	N/A	4'-9"	N/A	N/A
20	C	FLINT STREET (N)	201+42.03	23.50' RT	9.27'	N/A	N/A	N/A	N/A	13'-8"	N/A
21	A	FLINT STREET (N)	200+58.93	19.14' RT	12.00'	3.00'	9.00'	0.2%	6'-6"	11'-0"	10'-0"
22	A	BRIDGE STREET	113+85.68	22.00' LT	12.00'	4.00'	8.00'	0.1%	7'-8"	6'-6"	5'-0"
23	A	BRIDGE STREET	113+91.61	18.06' RT	VARIES	2.60'	VARIES	0.2%	7'-8"	6'-6"	5'-0"
24	A	FLINT STREET (S)	300+38.46	20.45' LT	VARIES	4.00'	VARIES	2.6%	6'-6"	8'-0"	5'-0"
25	C	GOODHUE STREET	21+91.79	7.11' LT	VARIES	N/A	N/A	2.2%	4'-0"	3'-4"	N/A
26	C	GOODHUE STREET	22+86.49	6.00' LT	VARIES	N/A	N/A	7.6%	9'-11"	9'-4"	N/A
27	C	GOODHUE STREET	21+96.48	16.28' RT	VARIES	N/A	N/A	0.9%	6'-6"	N/A	N/A
28	C	GOODHUE STREET	21+61.29	16.53' RT	VARIES	N/A	N/A	0.7%	N/A	7'-8"	N/A

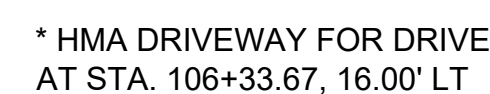
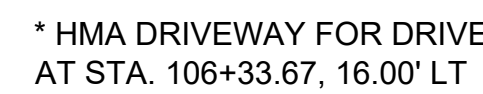


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Notes
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NOTES:

1. LEFT SIDE/RIGHT SIDE DETERMINED WHEN STANDING IN ROADWAY LOOKING AT RAMP OR DRIVEWAY.
2. SEE CONSTRUCTION PLANS FOR LOCATIONS OF DETECTABLE WARNING PANELS AT DRIVEWAYS. COST SHALL BE INCIDENTAL TO ITEM 701.1 CEMENT CONCRETE SIDEWALK AT DRIVEWAYS. DETECTABLE WARNING PANELS SHALL BE BRICK RED IN COLOR.

* = TOLERANCE FOR CONSTRUCTION = 0.5%±

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2000/09/05 1:18 PM Ray Seyler Chris



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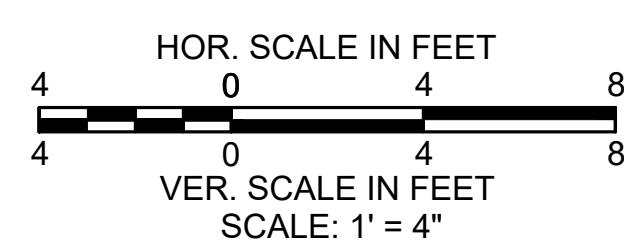
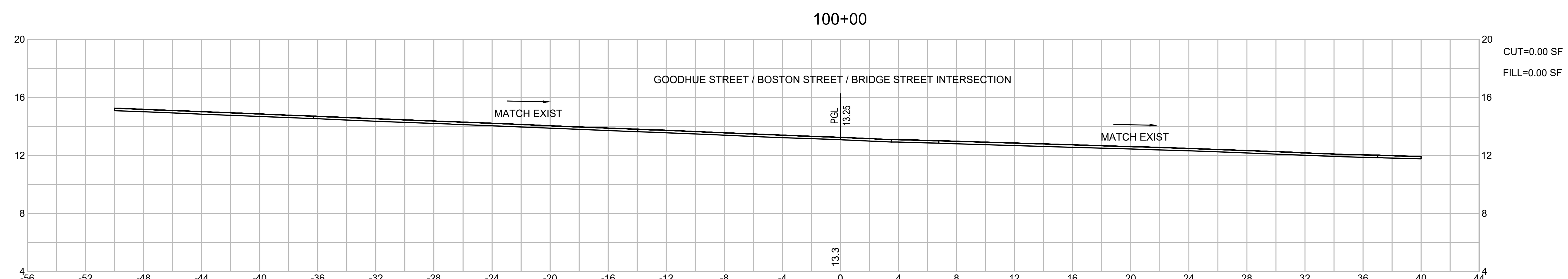
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BRIDGE STREET RECONSTRUCTION PROJECT

179410455	1" = 4'	
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Drawing No.	Sheet	Revision



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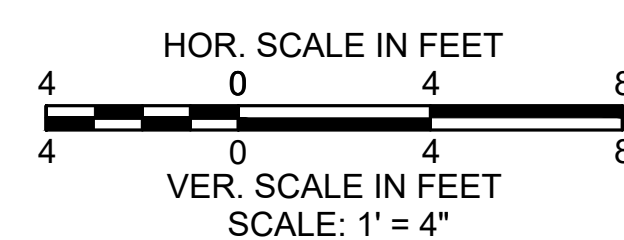
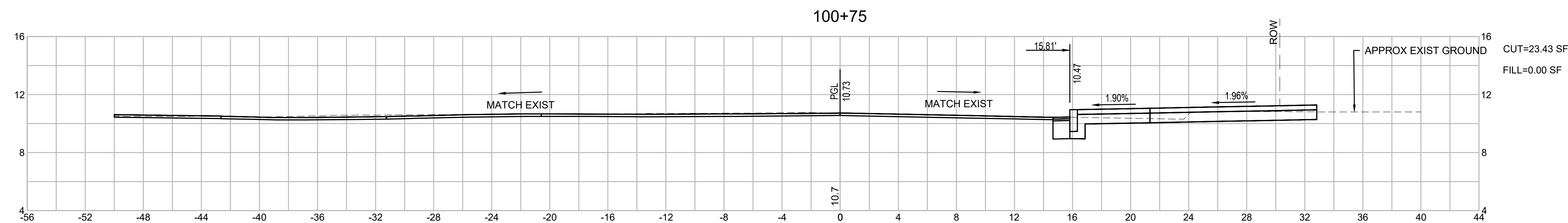
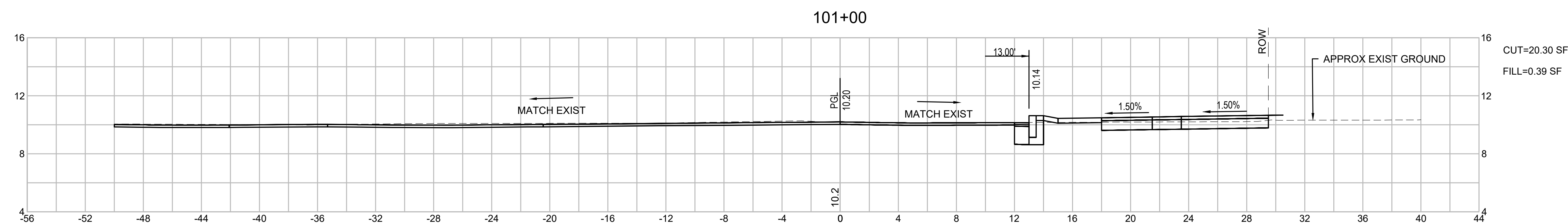
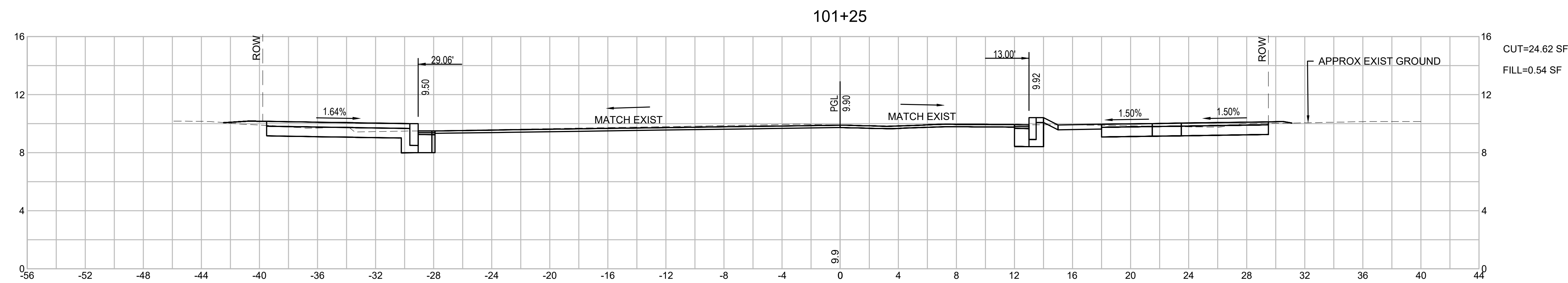


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BRIDGE STREET RECONSTRUCTION PROJECT

Title
CROSS SECTIONS BRIDGE STREET
PART 2 OF 18

179410455	1" = 4'	
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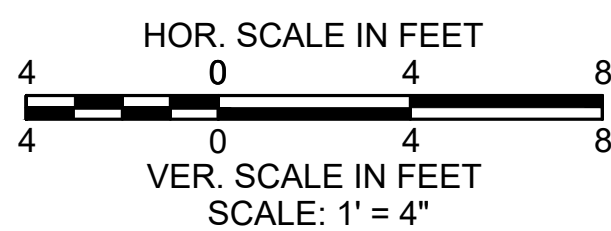
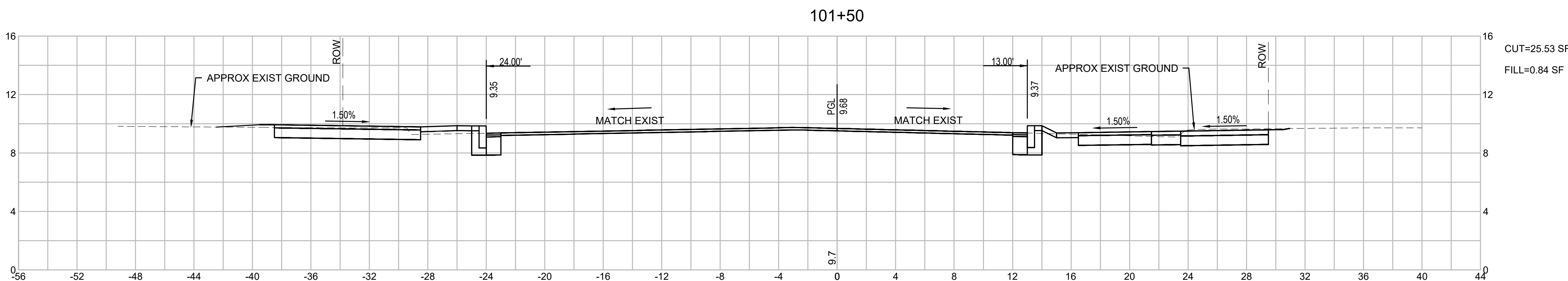
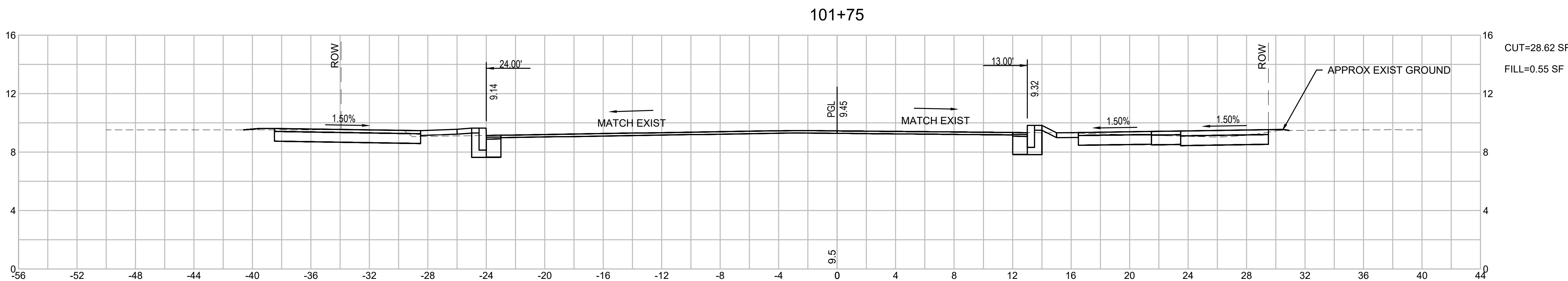
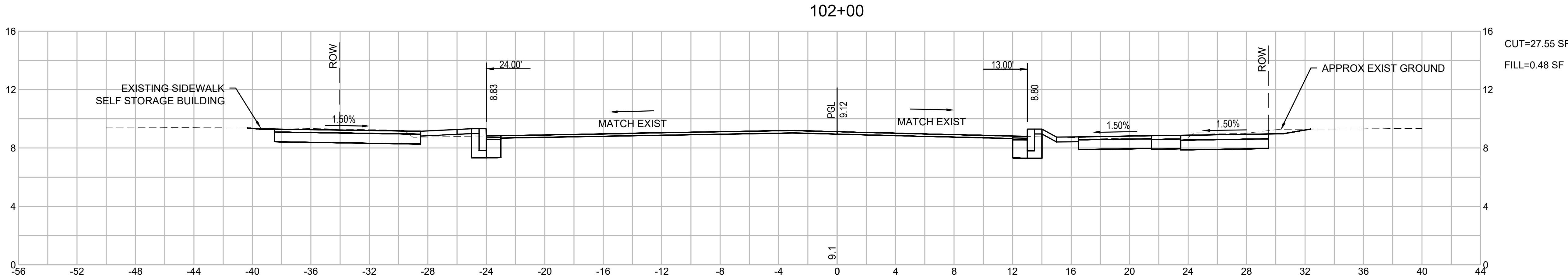
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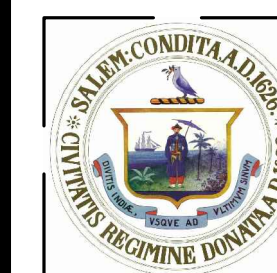
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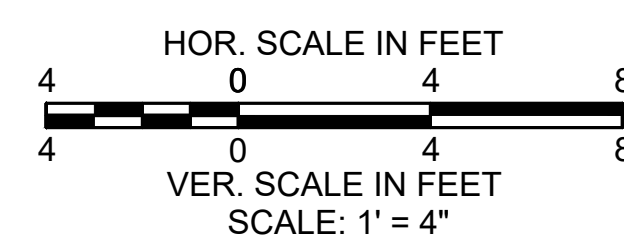
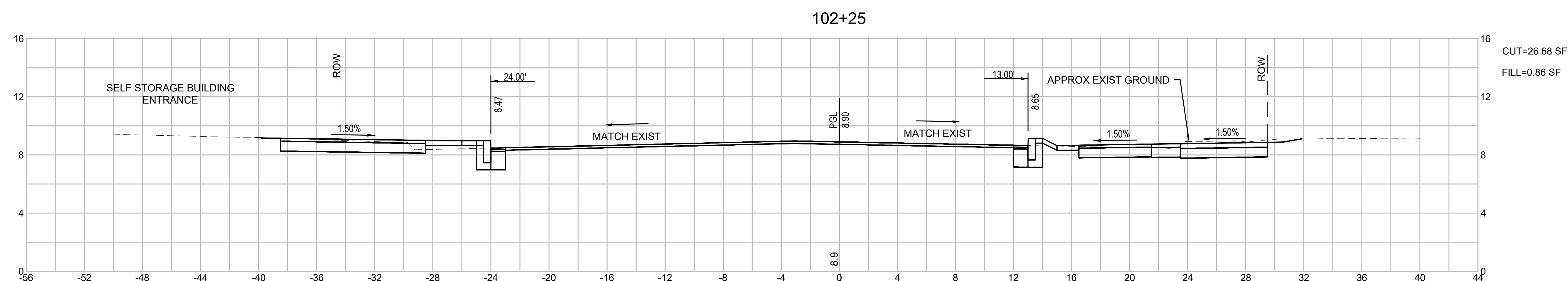
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Title
CROSS SECTIONS BRIDGE STREET
PART 4 OF 18

179410455 1" = 4'

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BRIDGE STREET RECONSTRUCTION PROJECT

Title

CROSS SECTIONS BRIDGE STREET
PART 5 OF 18

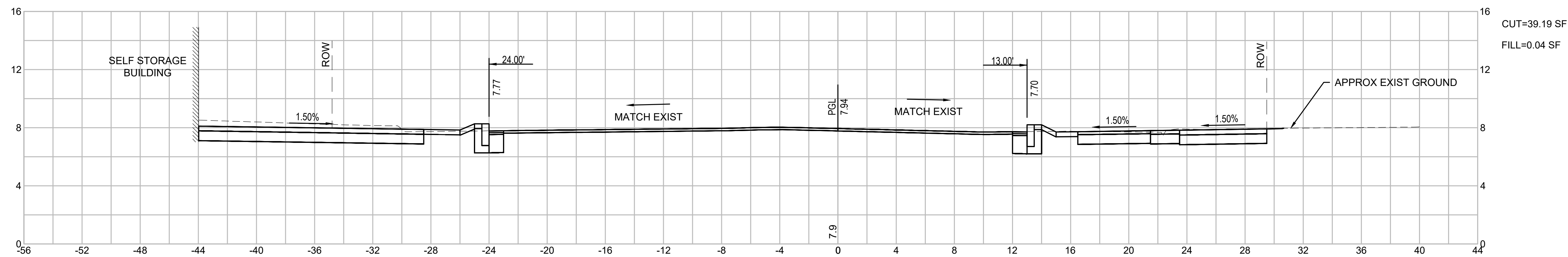
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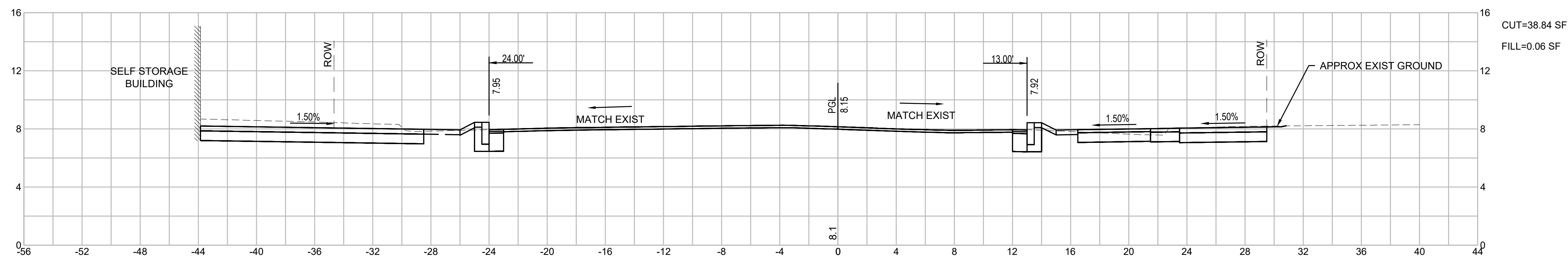
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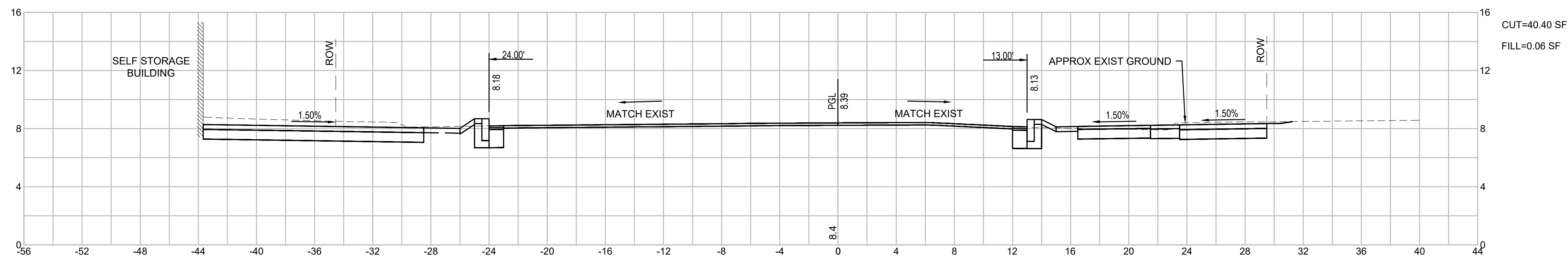
103+50



103+25



103+00



HOR. SCALE IN FEET
0 4 8
VER. SCALE IN FEET
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SCALE: 1" = 4'

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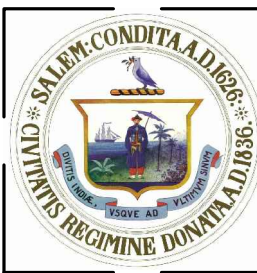
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BRIDGE STREET RECONSTRUCTION PROJECT

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CROSS SECTIONS BRIDGE STREET
PART 6 OF 18

179410455

1" = 4'

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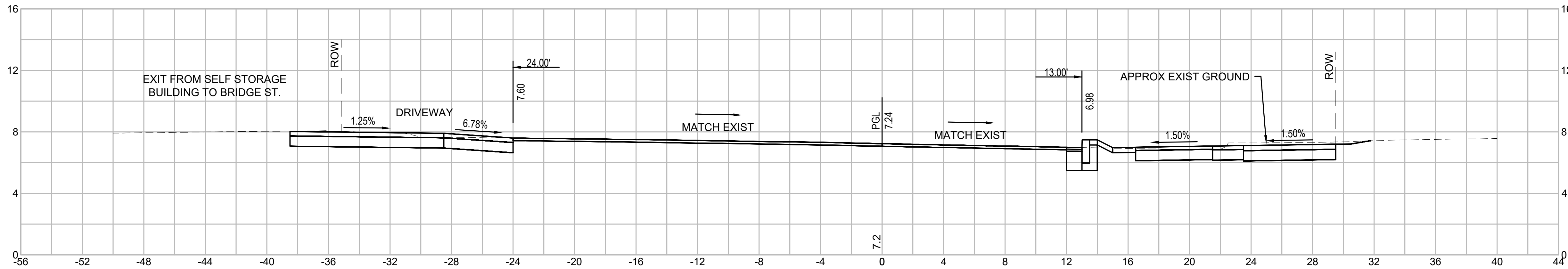
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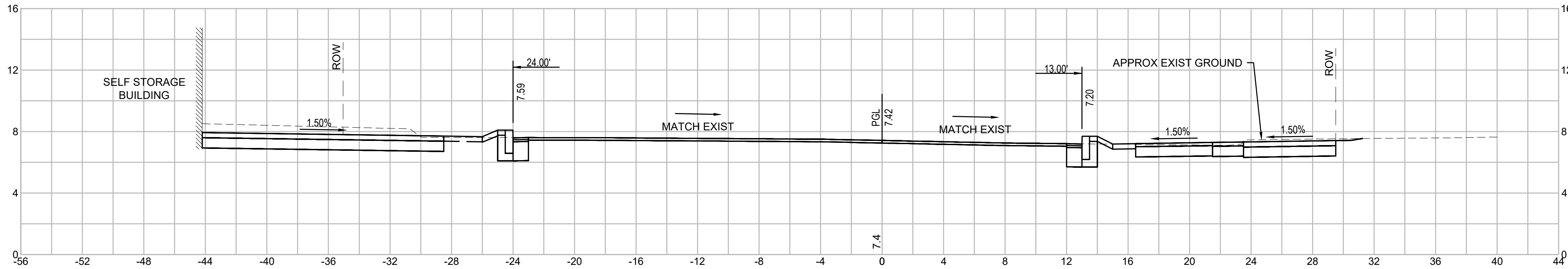
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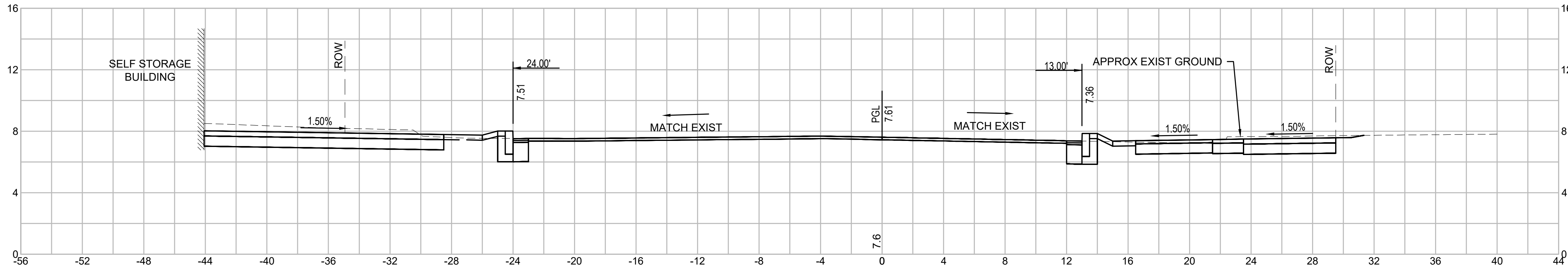
104+00



CUT=41.69 SF

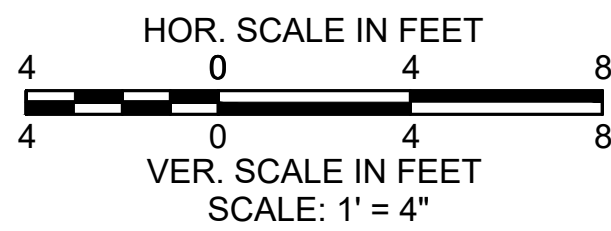
FILL=0.05 SF

103+75



CUT=39.60 SF

FILL=0.07 SF





105+00

CUT=37.95 SF

FILL=0.00 SF

CUT=36.21 SF

FILL=0.05 SF

CUT=35.33 SF

FILL=0.05 SF

104+75

104+50

HOR. SCALE IN FEET
4 0 4 8
VER. SCALE IN FEET
4 0 4 8
SCALE: 1" = 4'



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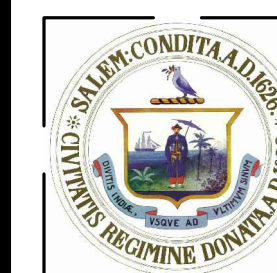
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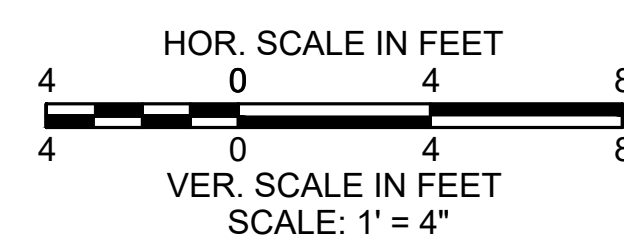
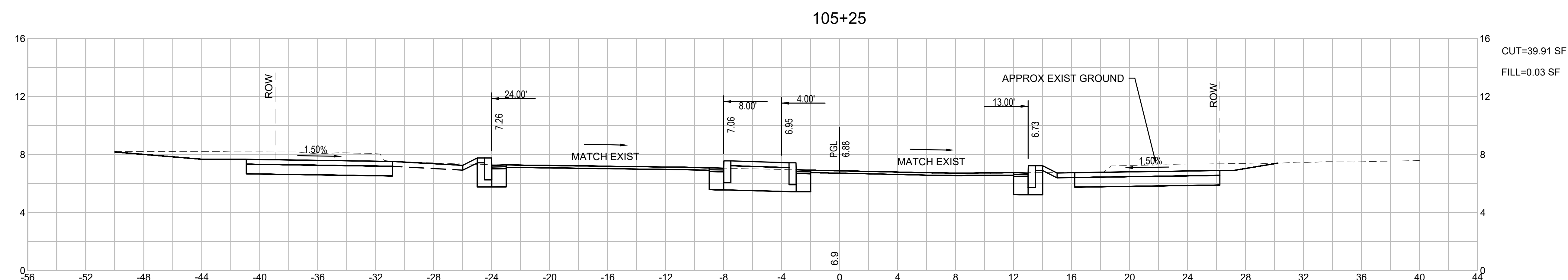


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BRIDGE STREET RECONSTRUCTION PROJECT

Title
CROSS SECTIONS BRIDGE STREET
PART 8 OF 18

179410455	1" = 4'	
Project No.	Scale	
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2020/02/05 1:24 PM By: Sadtler, Chris

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File Name:	dwg_cross_sections_bridge.dwg	Dwn.	Chkd.	Dsgn.	YY.MM.DD
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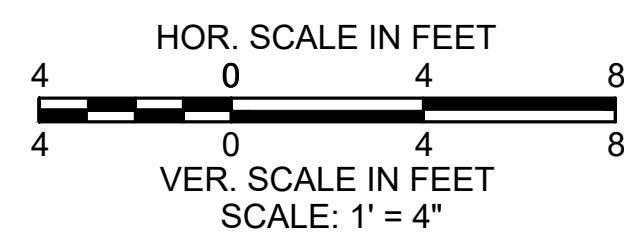
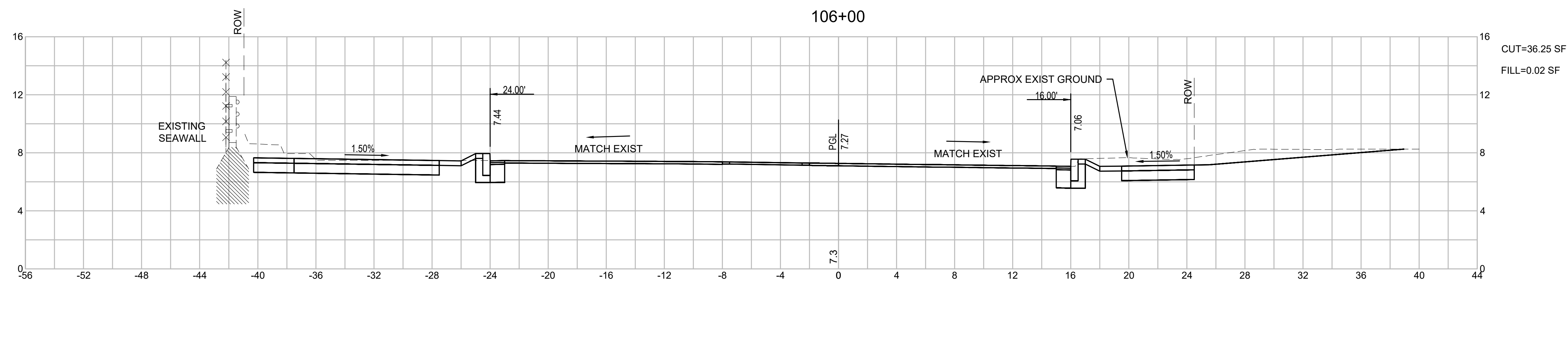
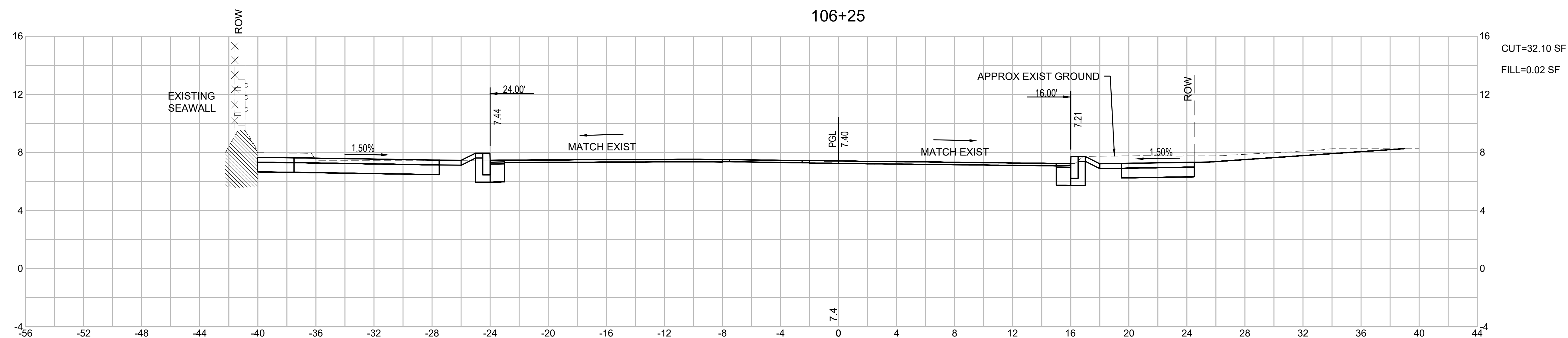
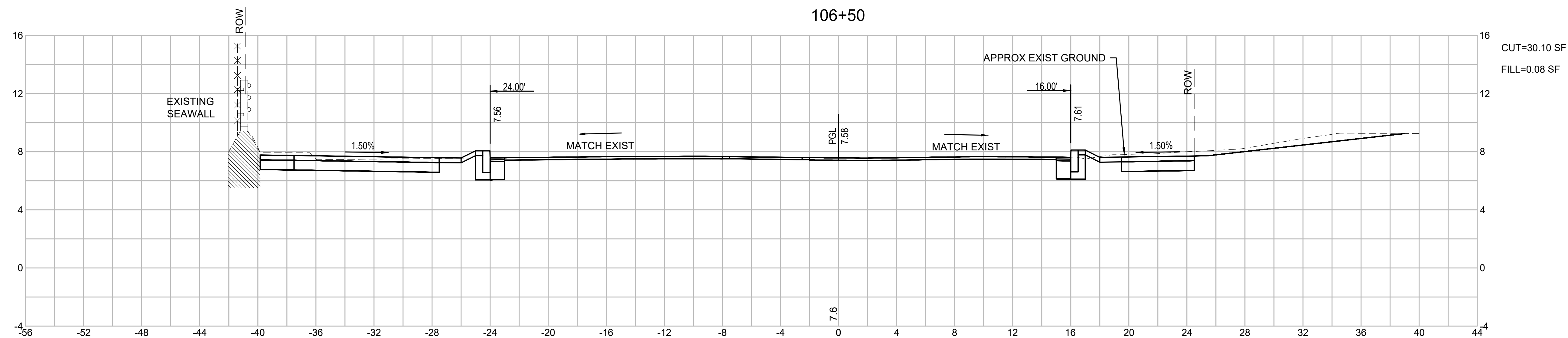


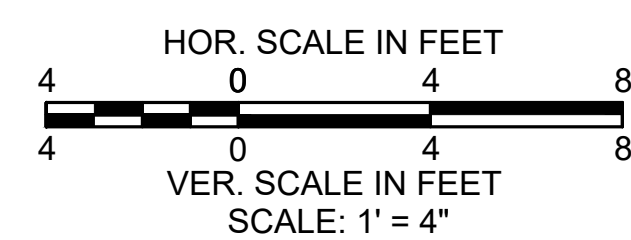
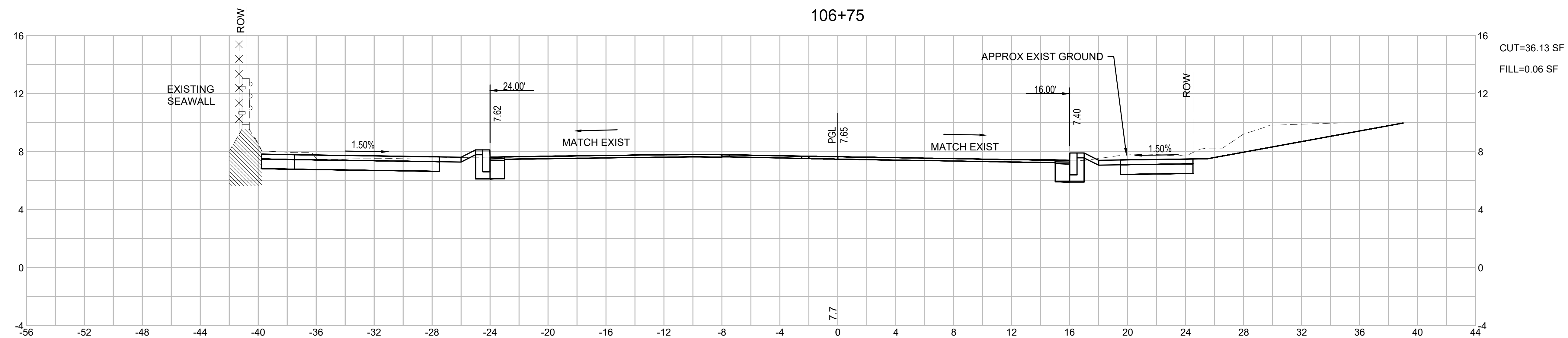
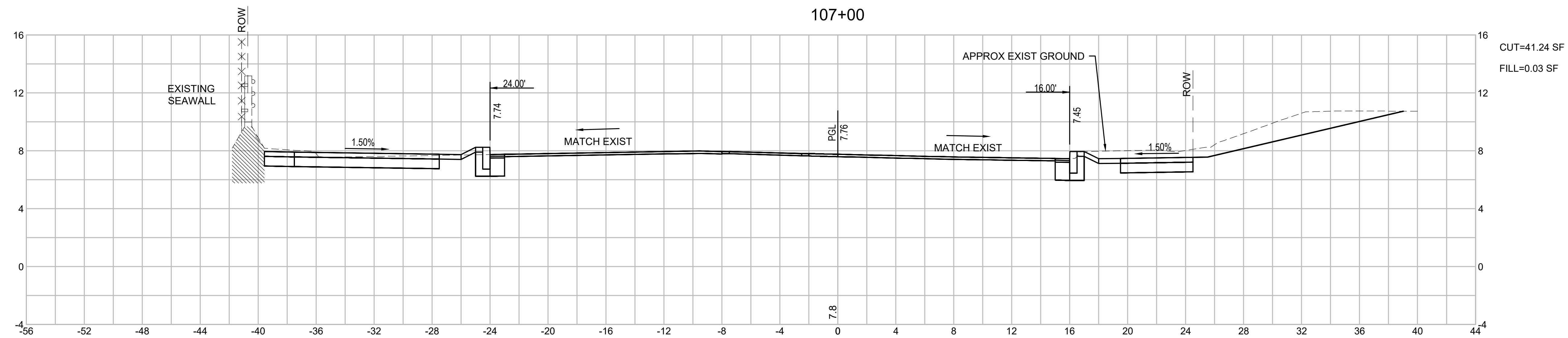
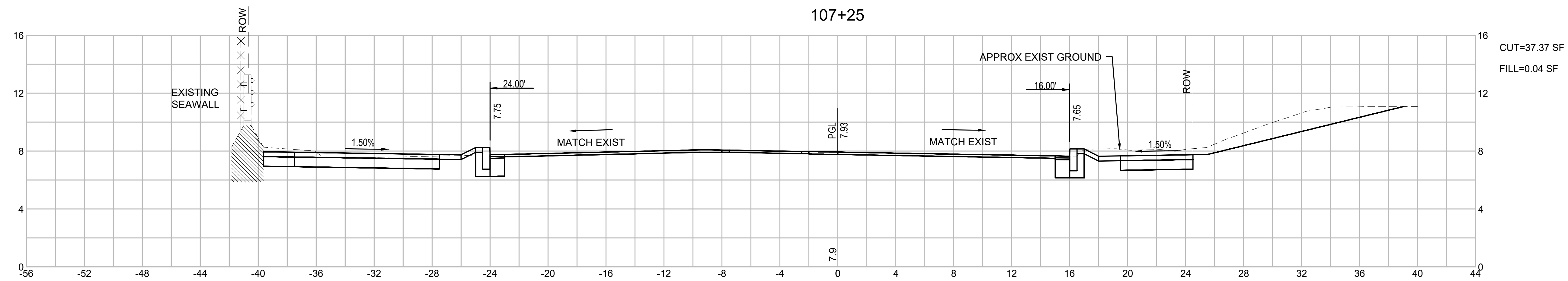
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Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title
CROSS SECTIONS BRIDGE STREET
PART 9 OF 18

179410455	1" = 4'
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BRIDGE STREET RECONSTRUCTION PROJECT

Title

CROSS SECTIONS BRIDGE STREET
PART 10 OF 18

179410455

$$1'' = 4'$$

Project No.

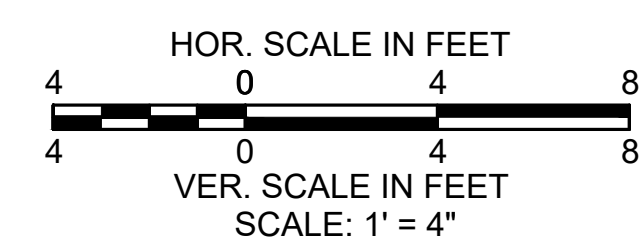
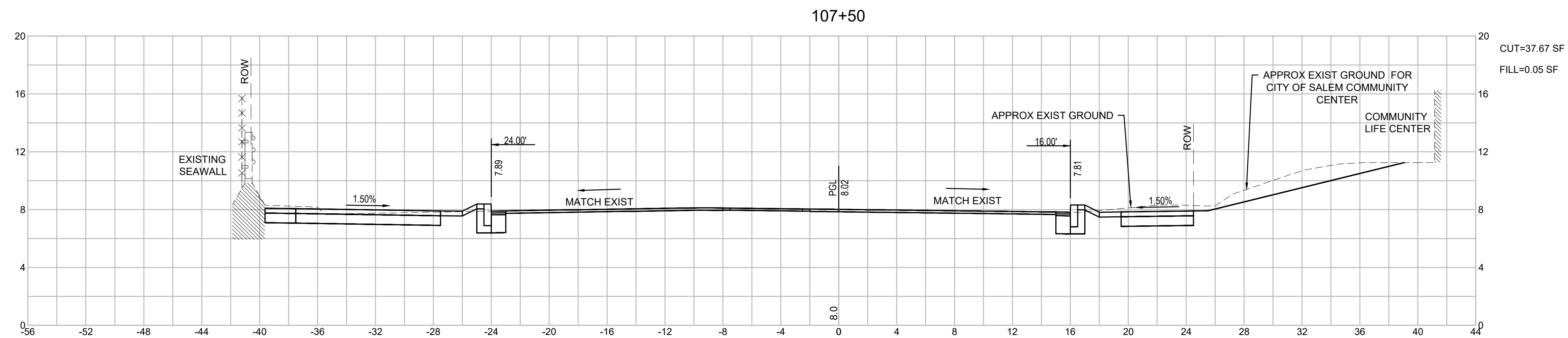
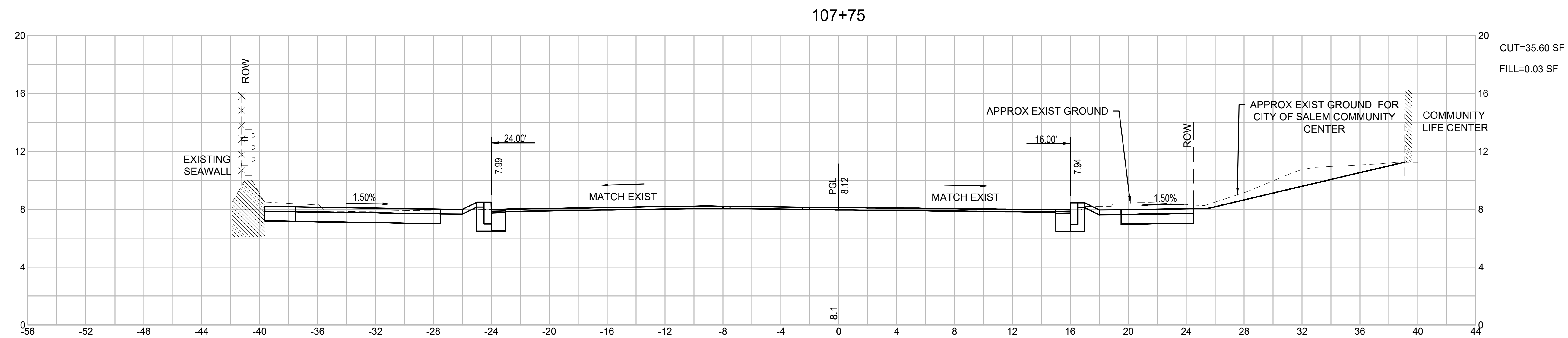
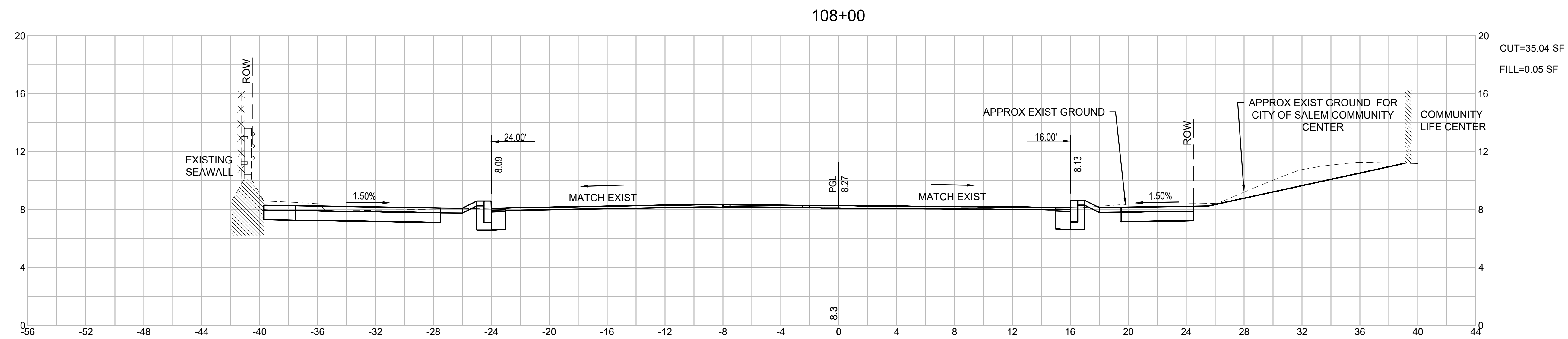
Scale

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BRIDGE STREET RECONSTRUCTION PROJECT

Title
CROSS SECTIONS BRIDGE STREET
PART 11 OF 18

179410455 1" = 4'

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BRIDGE STREET RECONSTRUCTION PROJECT

Title

CROSS SECTIONS BRIDGE STREET
PART 12 OF 18

179410455

1" = 4'

Project No.

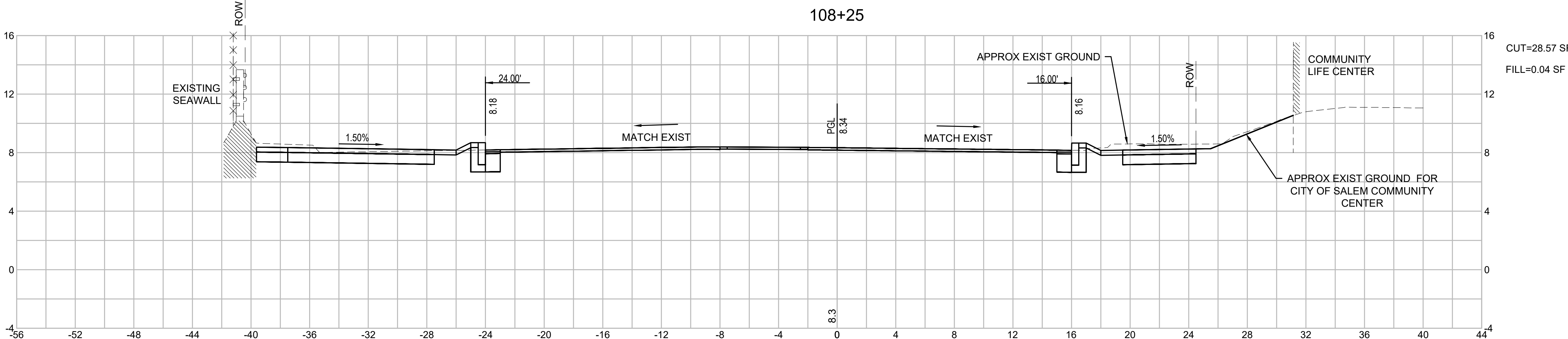
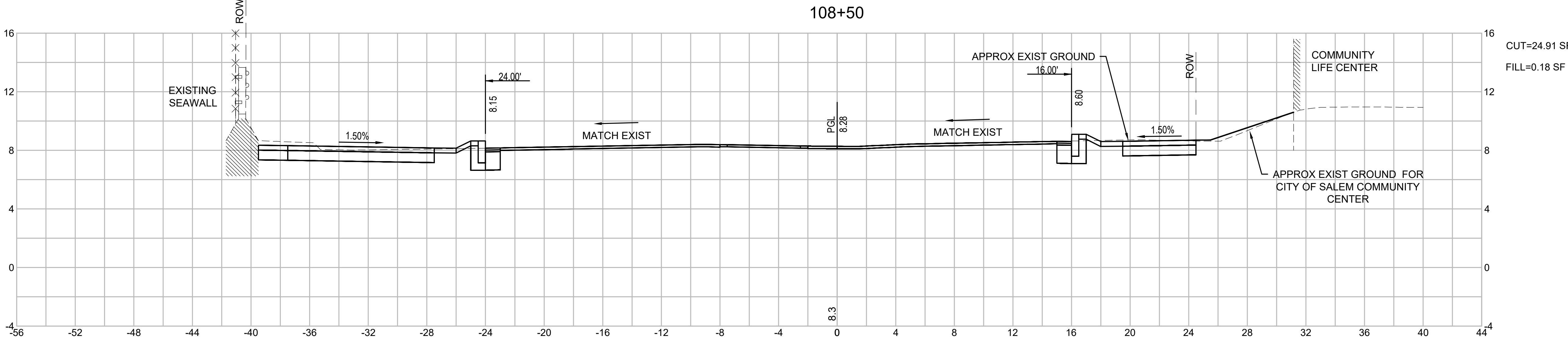
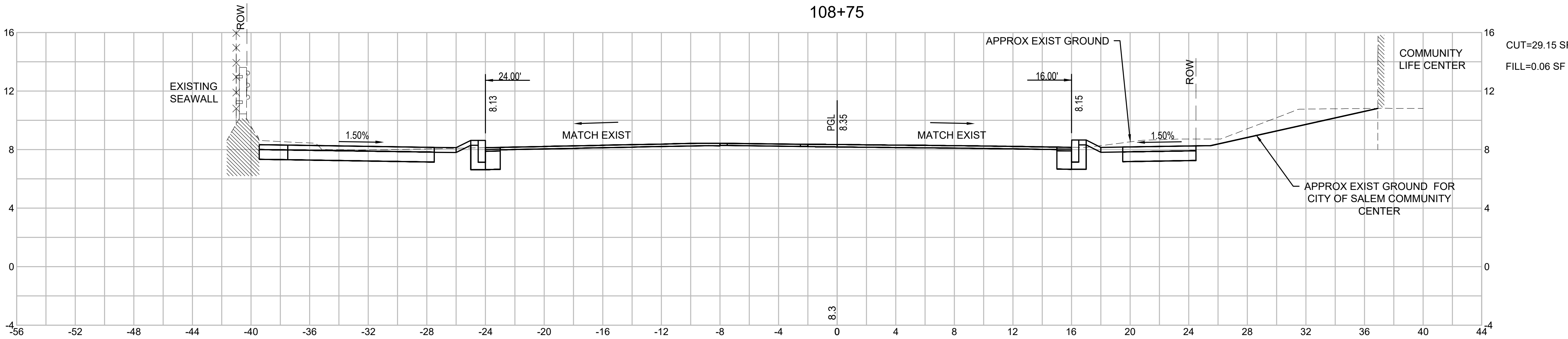
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HOR. SCALE IN FEET
0 4 8
VER. SCALE IN FEET
0 4 8
SCALE: 1" = 4"

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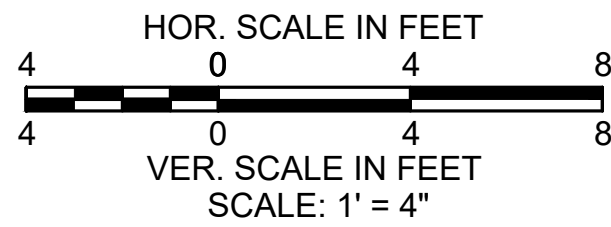
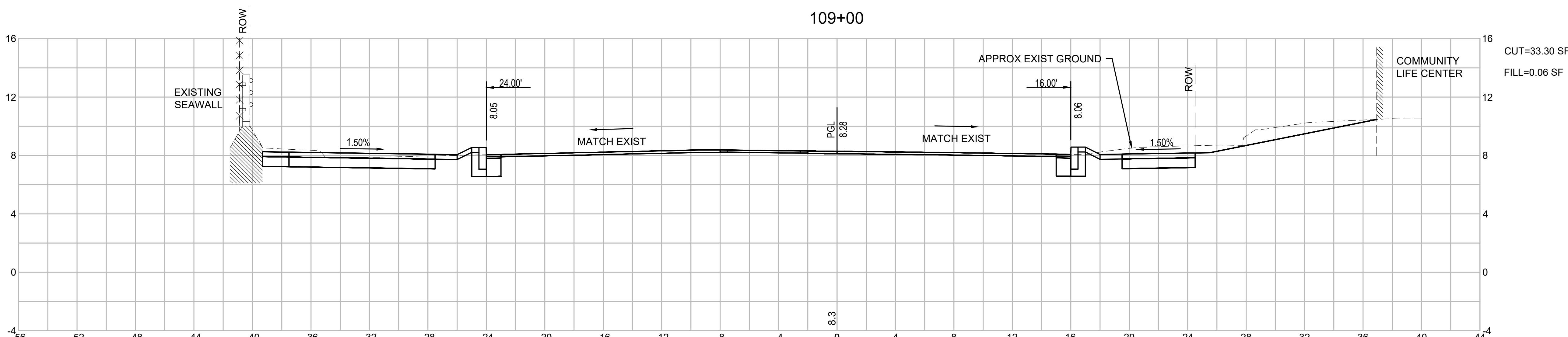
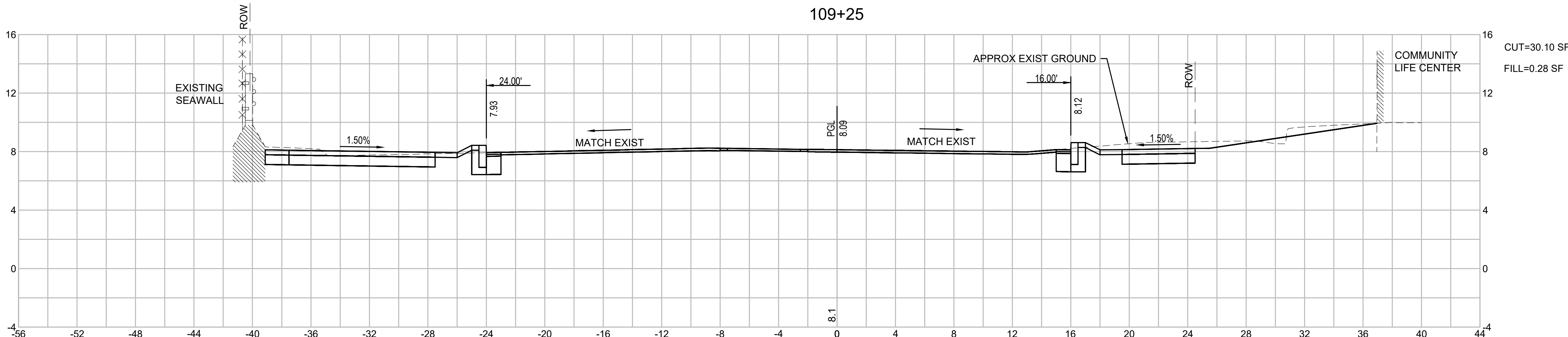
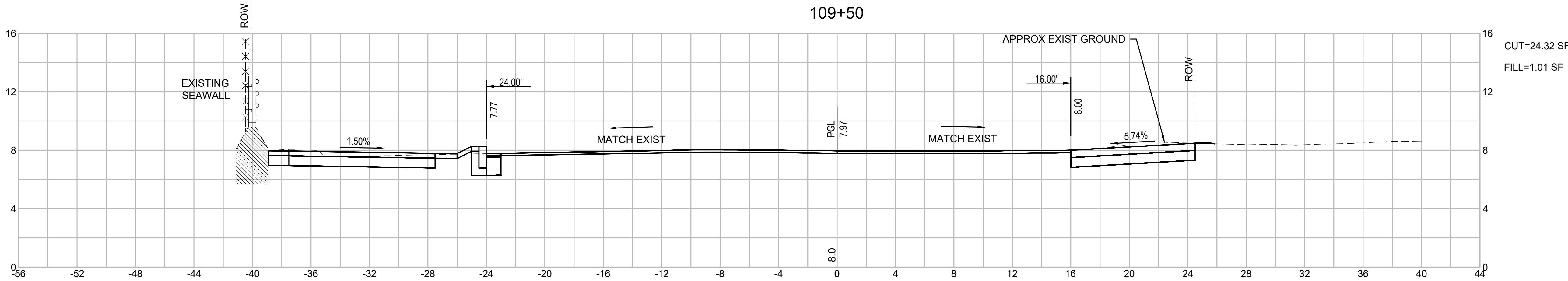


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BRIDGE STREET RECONSTRUCTION PROJECT

Title
CROSS SECTIONS BRIDGE
STREET PART 13 OF 18

179410455 1" = 4'
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BRIDGE STREET RECONSTRUCTION PROJECT

Title

CROSS SECTIONS BRIDGE STREET
PART 14 OF 18

179410455

1" = 4'

Project No.

Scale

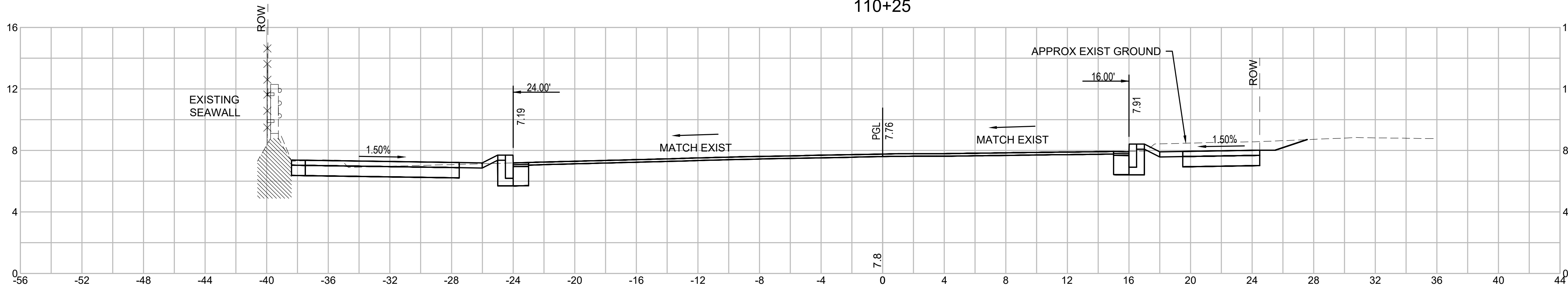
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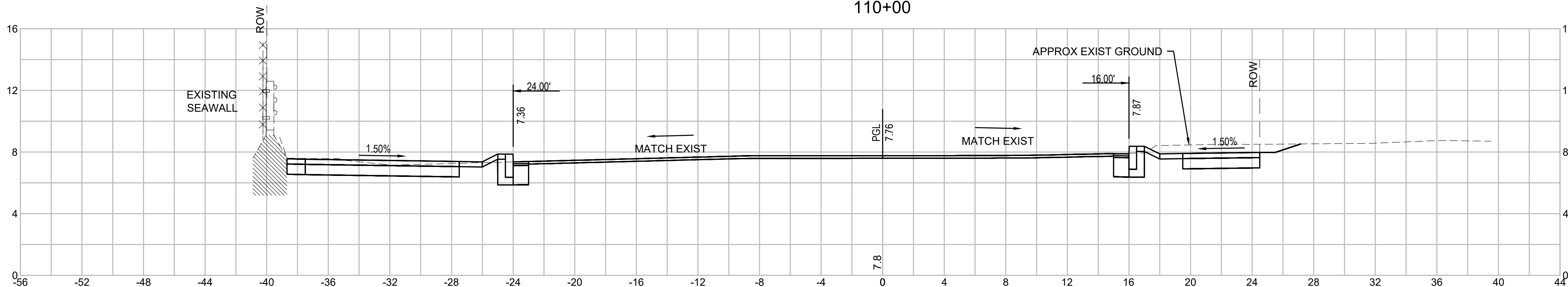
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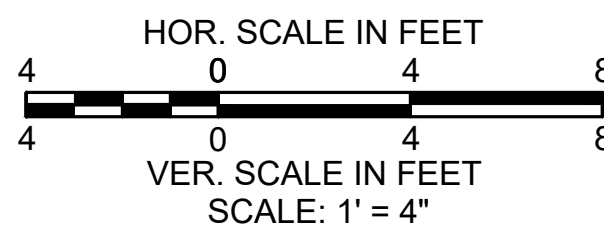
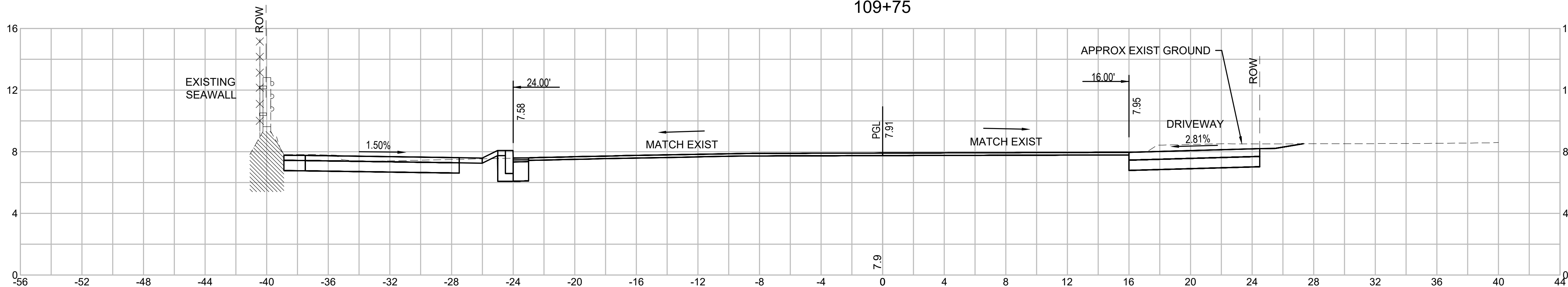
110+25



110+00



109+75





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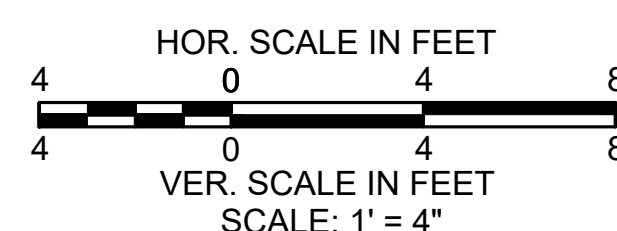
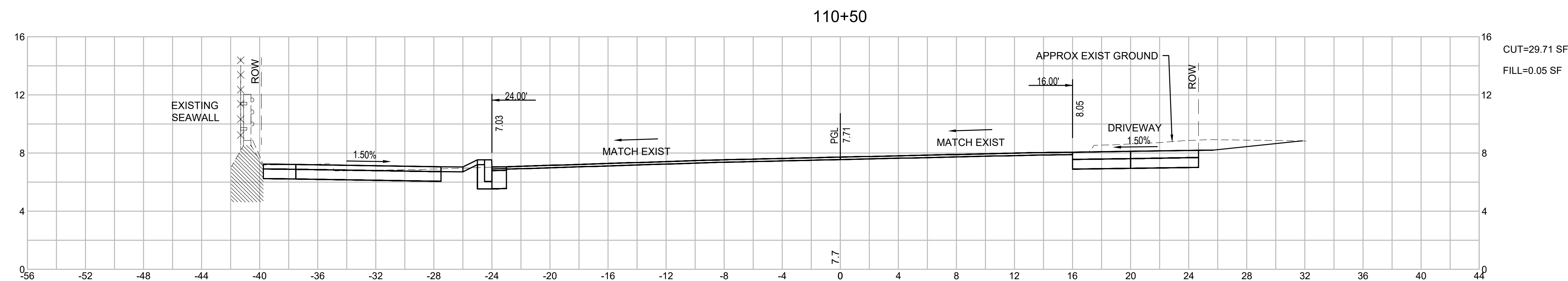
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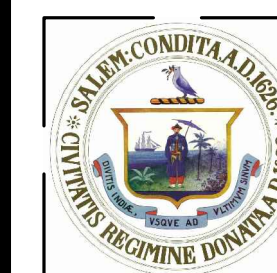
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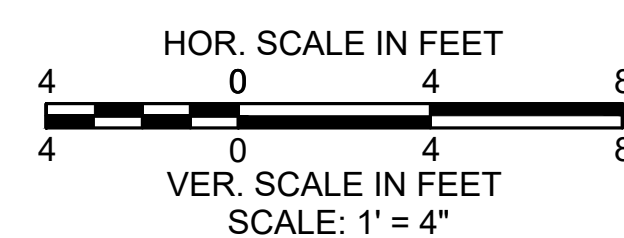
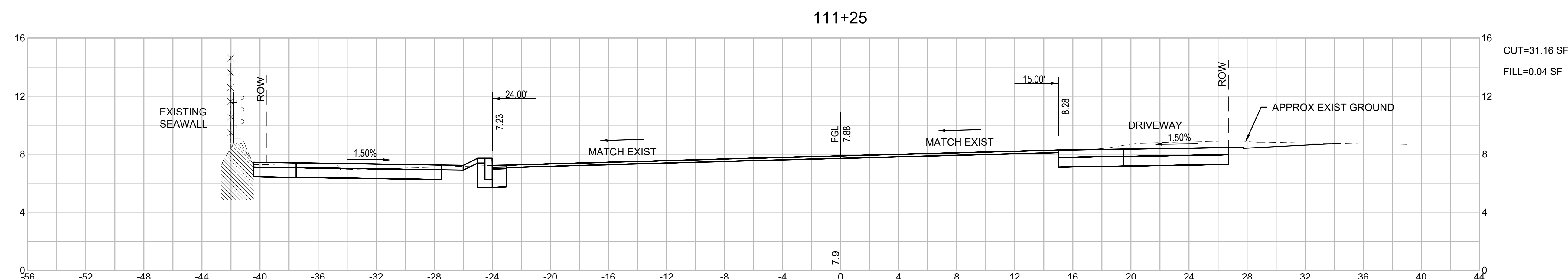
CROSS SECTIONS BRIDGE STREET
PART 16 OF 18

179410455 1" = 4'

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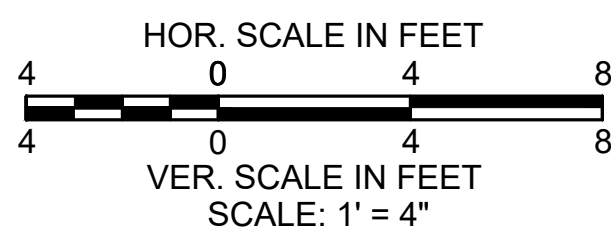
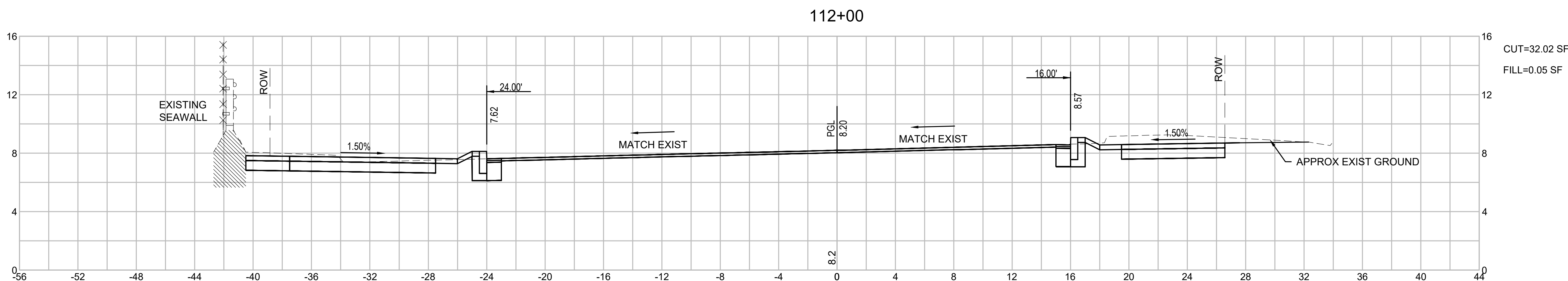
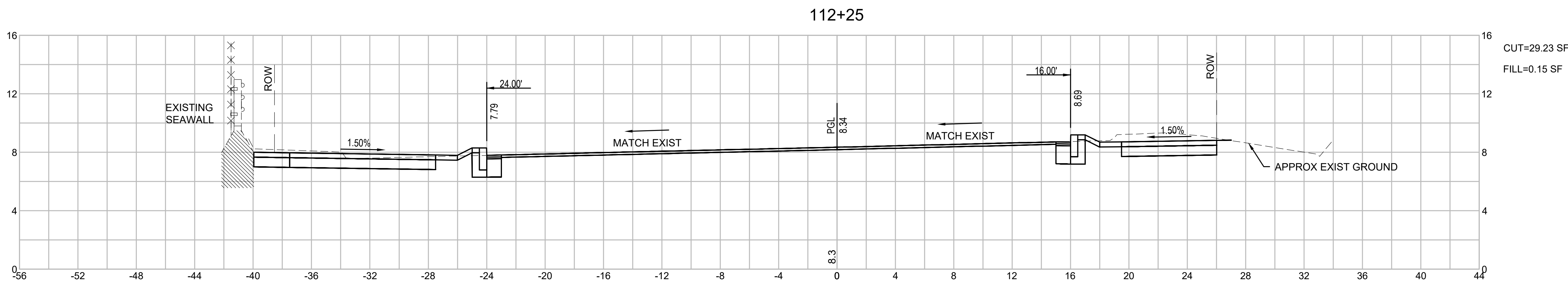
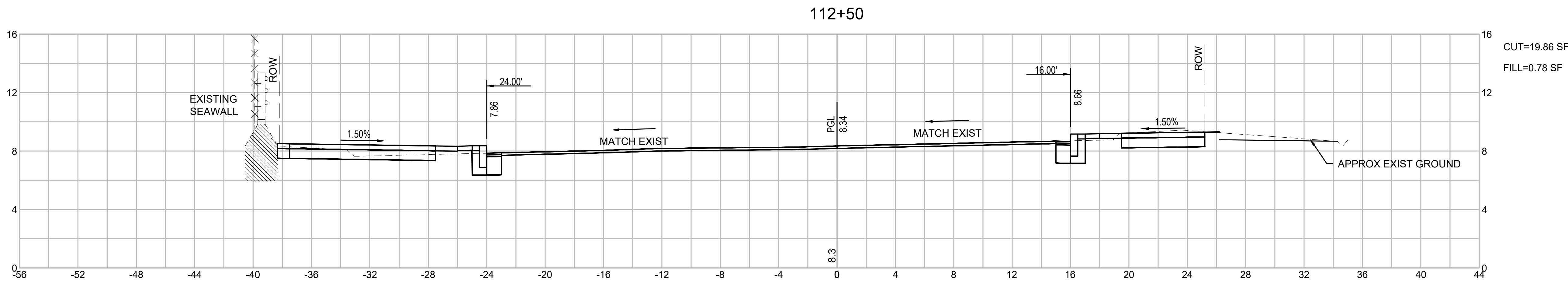
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CROSS SECTIONS BRIDGE STREET
PART 17 OF 18

179410455	1" = 4'
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113+00

CUT=17.61 SF
FILL=0.10 SF

16
12
8
4
0

56 -52 -48 -44 -40 -36 -32 -28 -24 -20 -16 -12 -8 -4 0 4 8 12 16 20 24 28 32 36 40 44

0.61%
36.90'
8.11'
MATCH EXIST
PGL 8.54
8.5
MATCH EXIST
16.05'
8.67'
1.51%
ROW
APPROX EXIST GROUND

112+75

CUT=18.20 SF
FILL=0.24 SF

16.00'

8.59

1.50%

ROW

APPROX EXIST GROUND

MATCH EXIST

PGL 8.42

8.4

1.51%

8.13

26.46'

8

4

0

56

52

48

44

40

36

32

28

24

20

16

12

8

4

0

0

4

8

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16

20

24

28

32

36

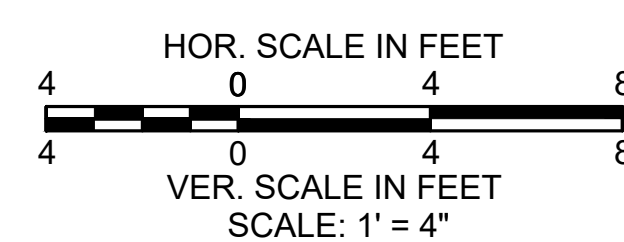
40

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BRIDGE STREET RECONSTRUCTION PROJECT

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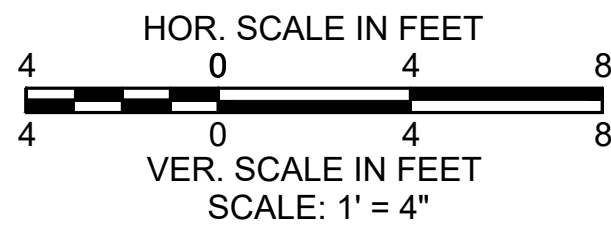
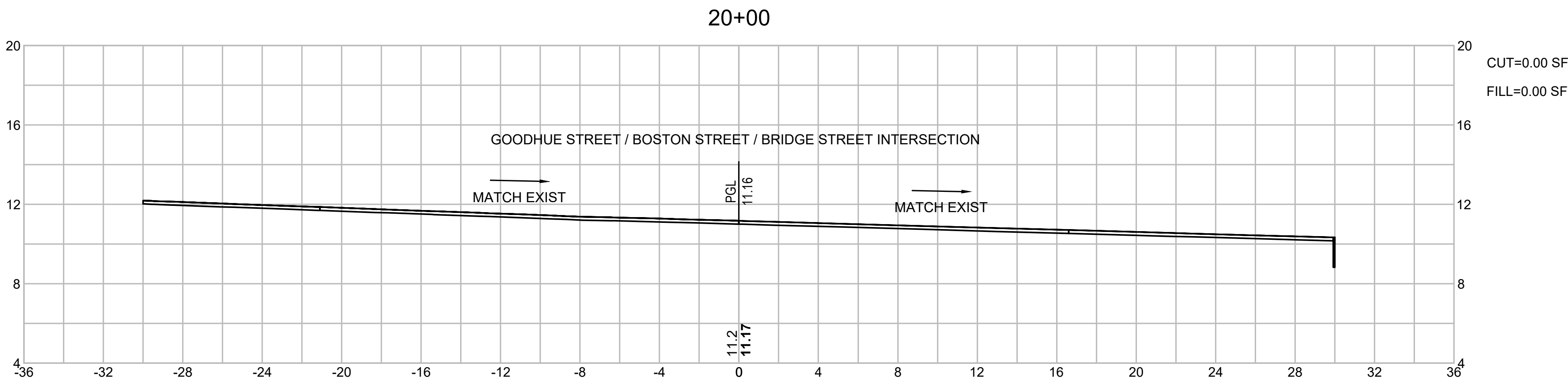
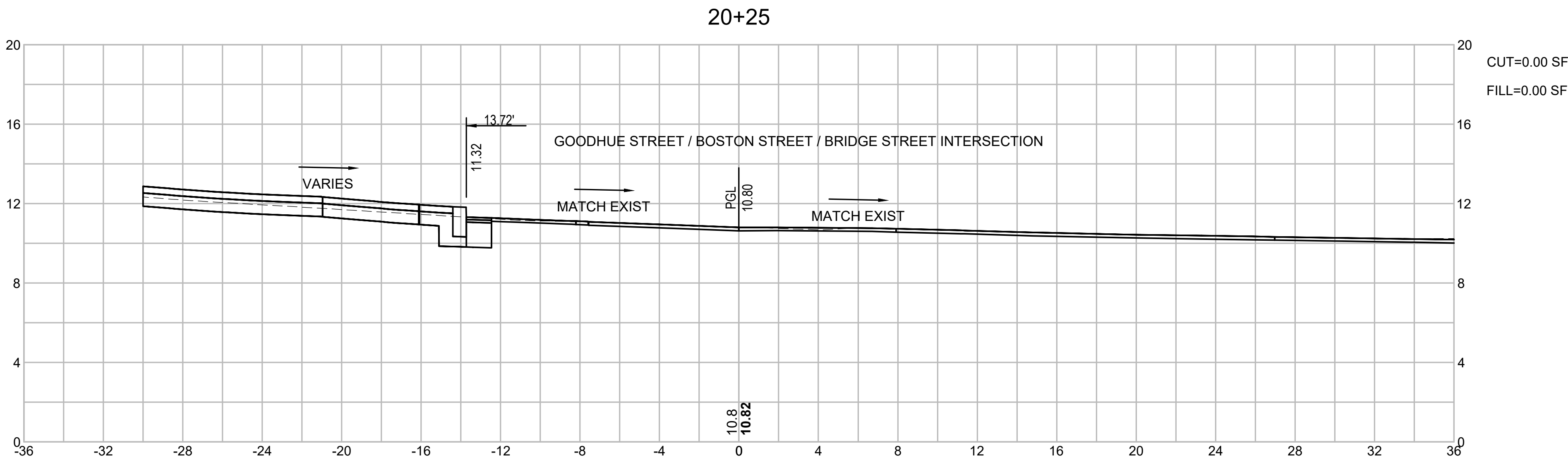
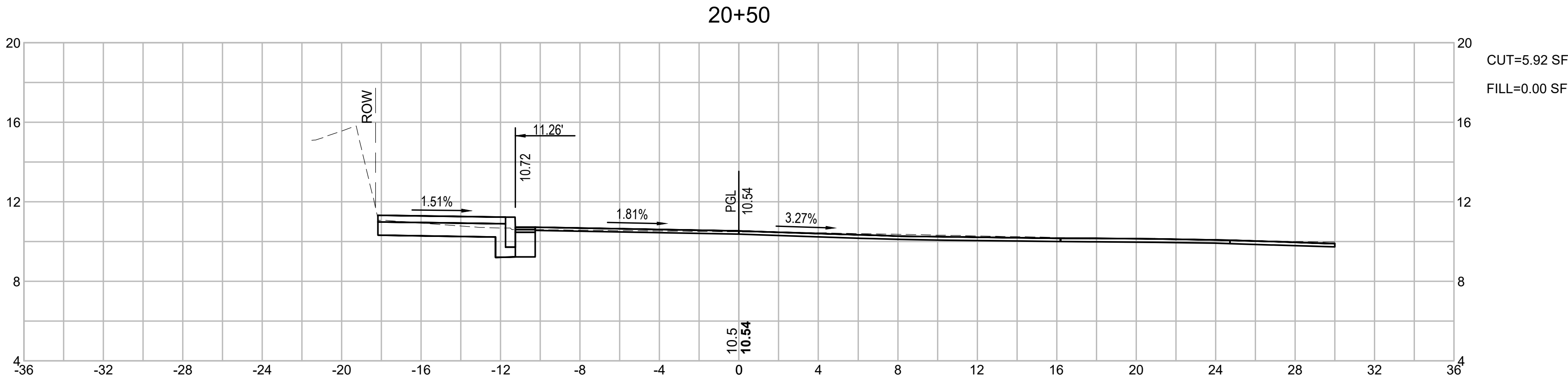
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BRIDGE STREET RECONSTRUCTION PROJECT

Title

CROSS SECTIONS GOODHUE STREET
PART 1 OF 8

179410455	1" = 4'
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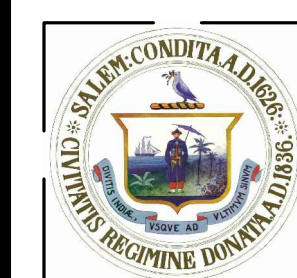
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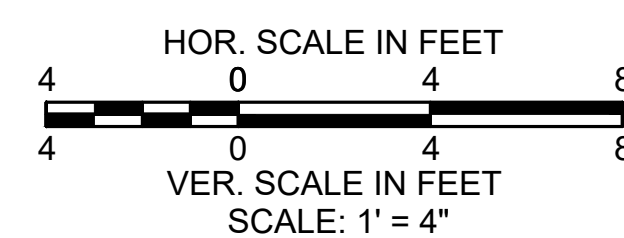
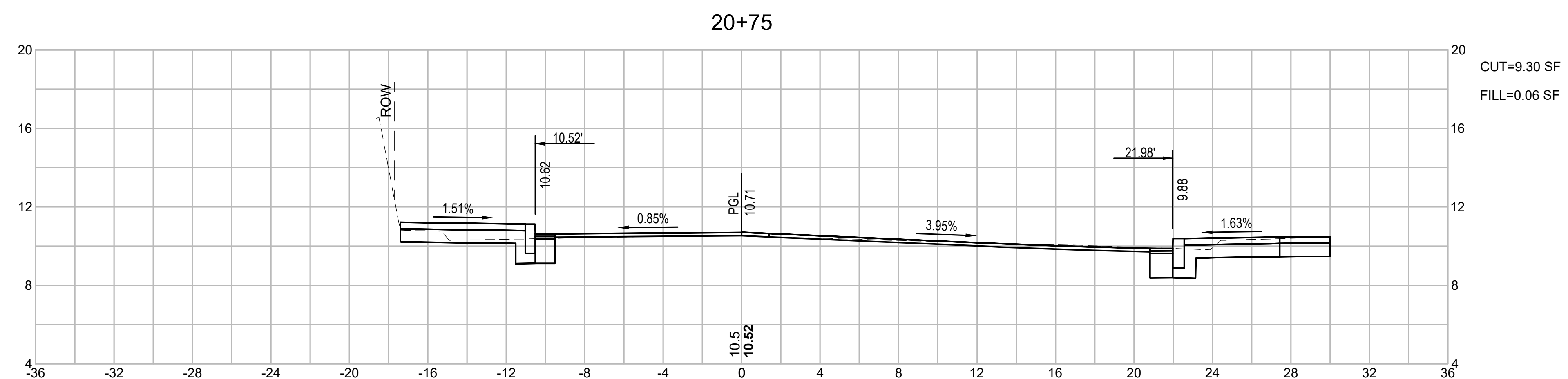
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CROSS SECTIONS GOODHUE STREET
PART 2 OF 8

179410455 1" = 4'

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2009/02/05 1:31 PM Bill Seidler, Chris

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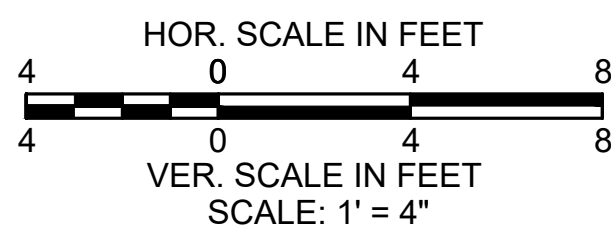
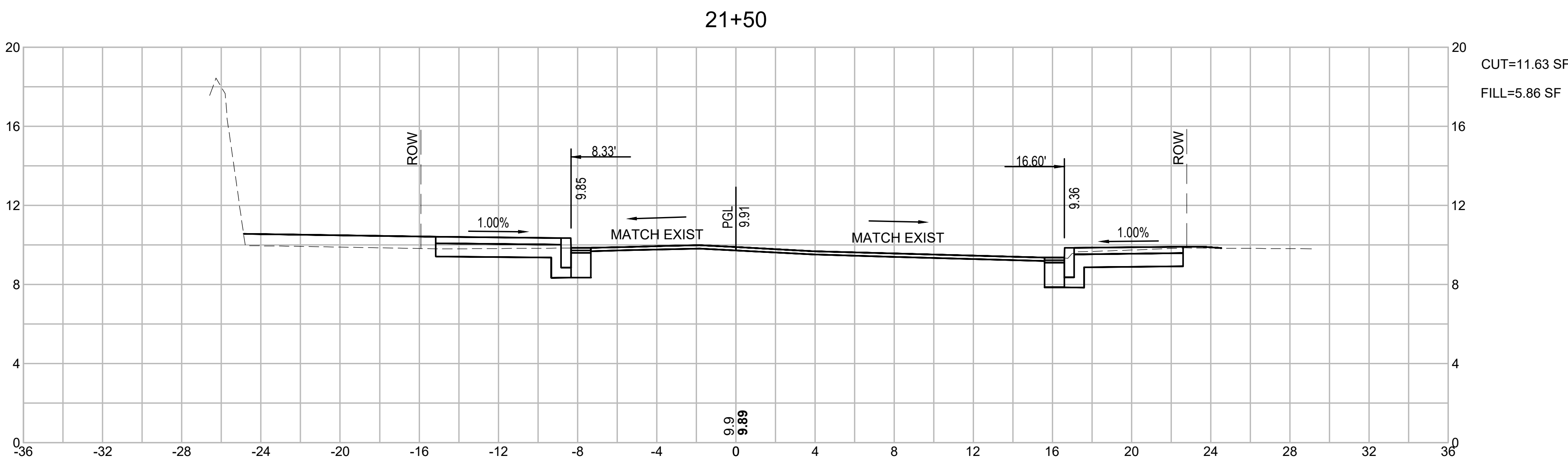
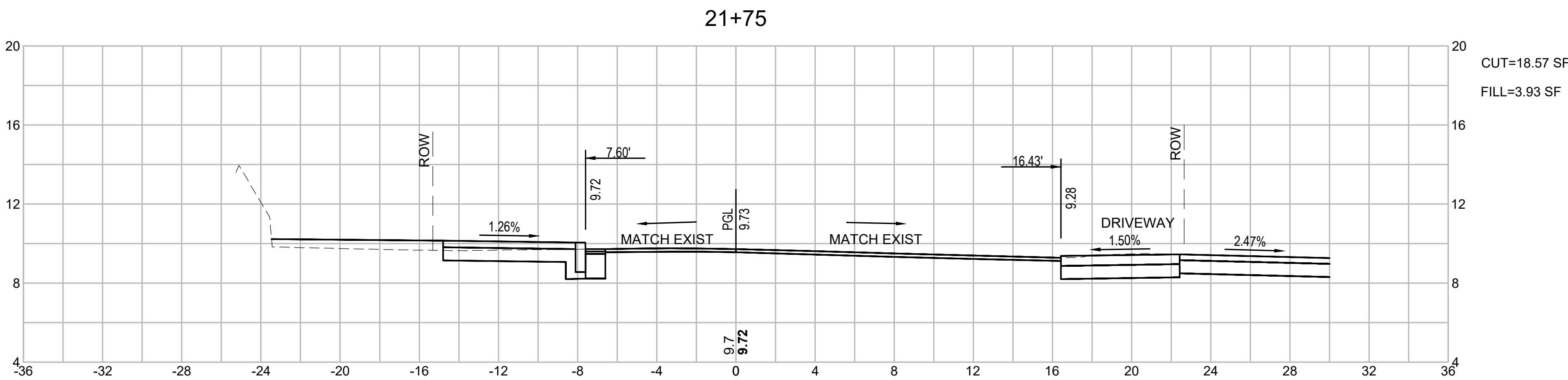
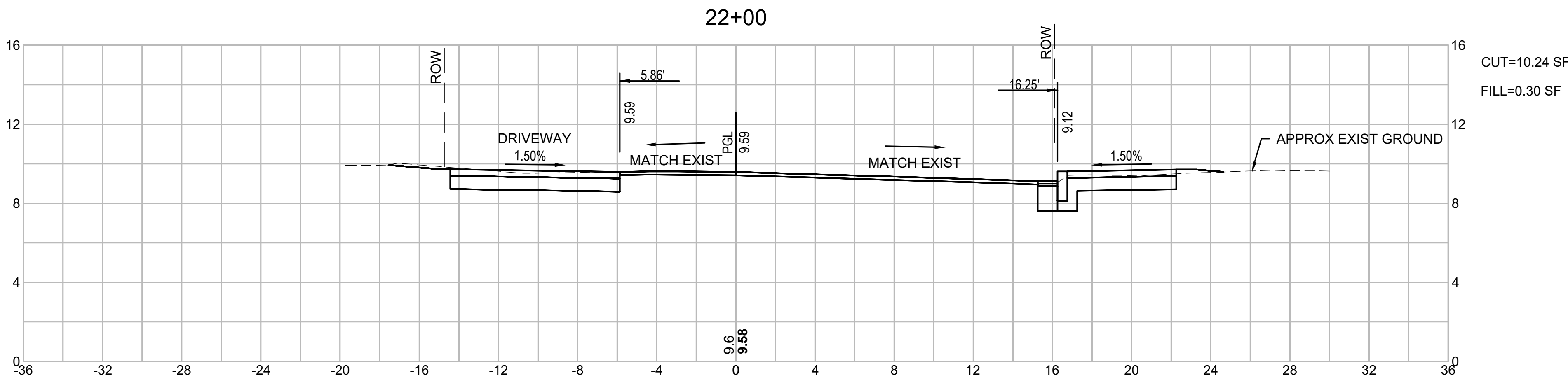
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CROSS SECTIONS GOODHUE STREET
PART 3 OF 8

179410455	1" = 4'
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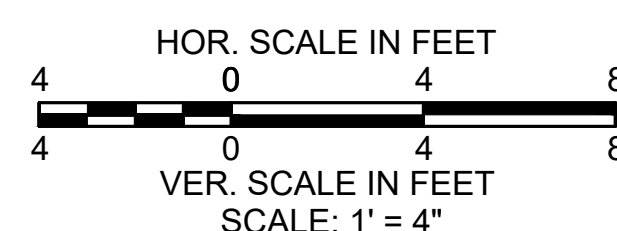
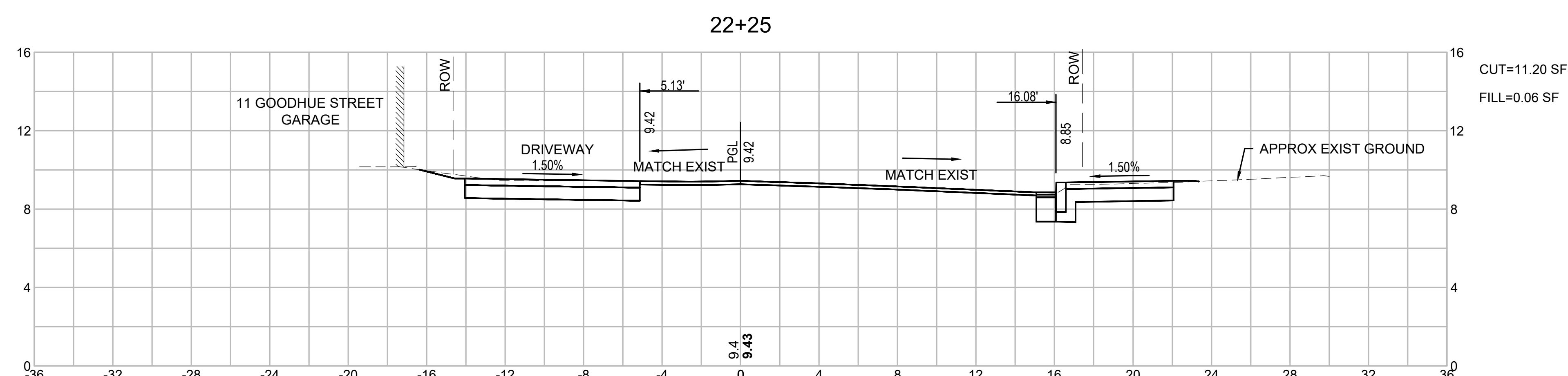
File Name:	dwg_cross_sections_goodhue.dwg	Down	Chkd	Done	YY.MM.DD
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Client/Project

CROSS SECTIONS GOODHUE STREET
PART 4 OF 8

Project No.	Scale
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File Name:	dwg_cross_sections_goodhue.dwg	Dwn.	Chkd.	Dsgn.	YY.MM.DD
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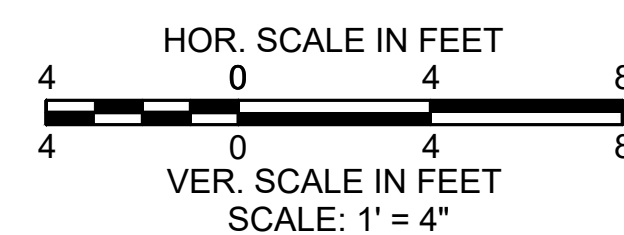
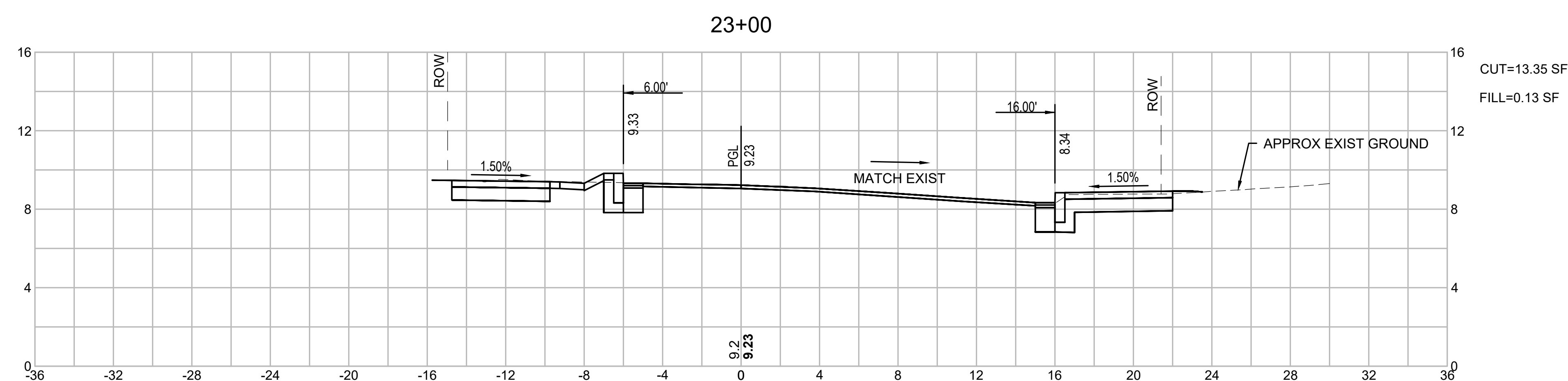
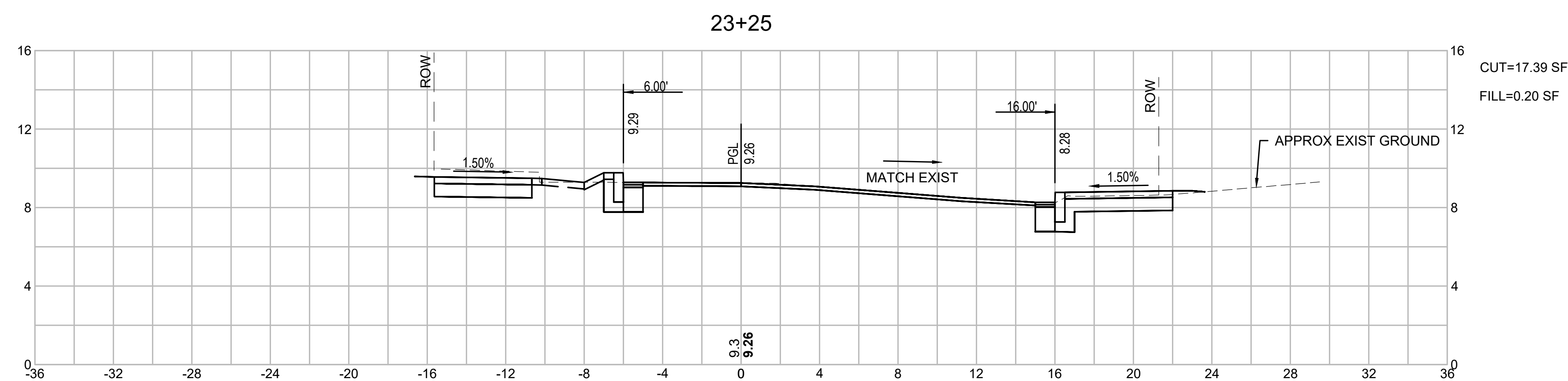
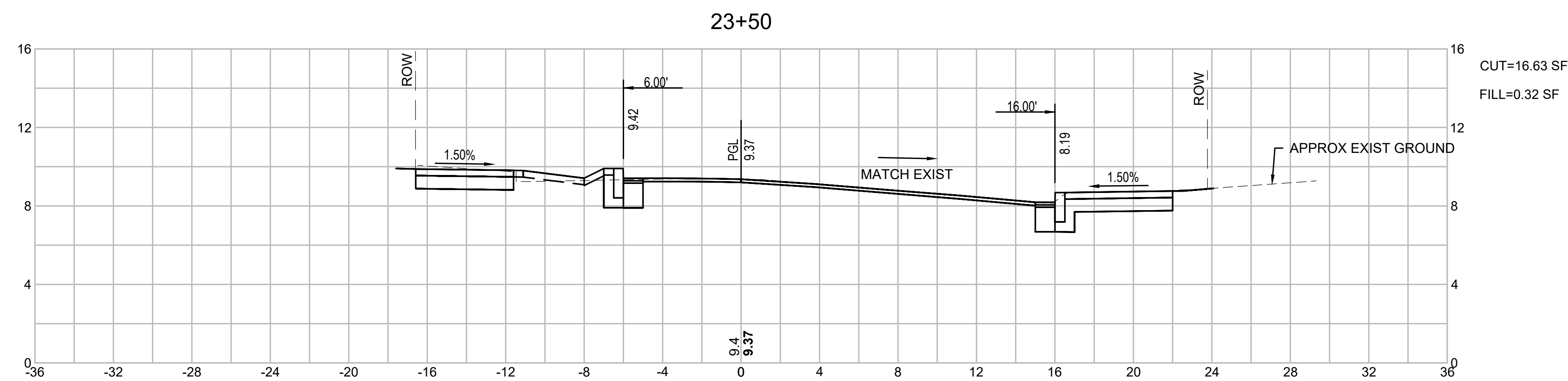


BRIDGE STREET RECONSTRUCTION PROJECT

179410455 1" = 4'

Project No.	Scale
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Drawing No.	Sheet	Revision
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2020/02/05 1:33 PM By: Sodtler, Chris

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By Appd. YY.MM.DD

File Name: dwg_cross_sections_goodhue.dwg

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Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title

CROSS SECTIONS GOODHUE STREET
PART 6 OF 8

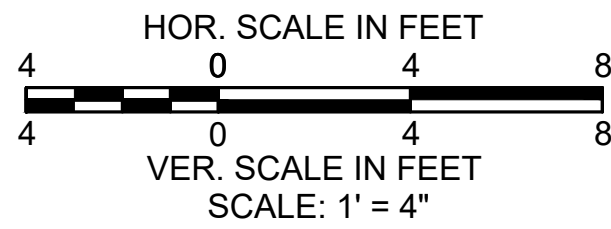
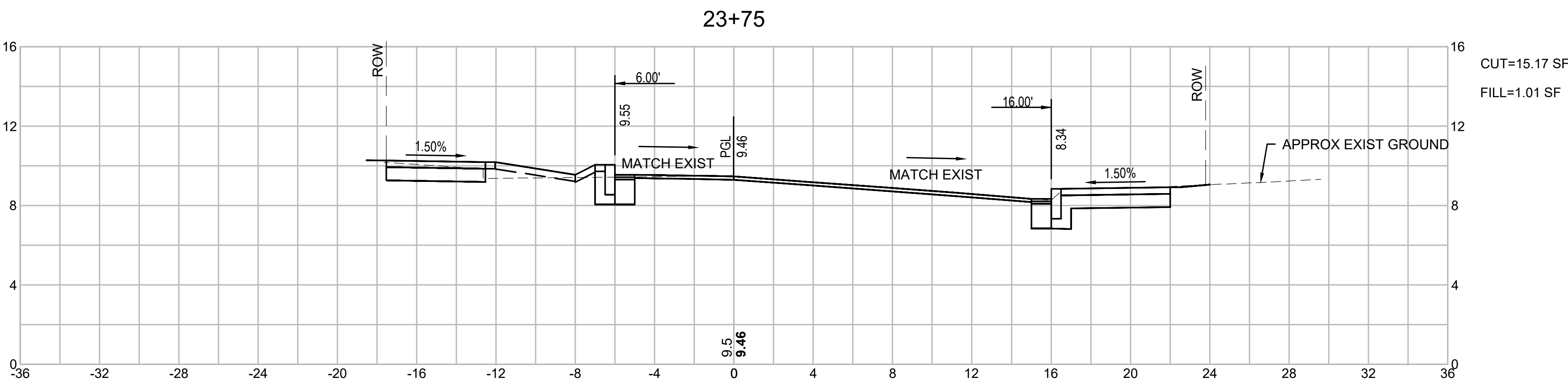
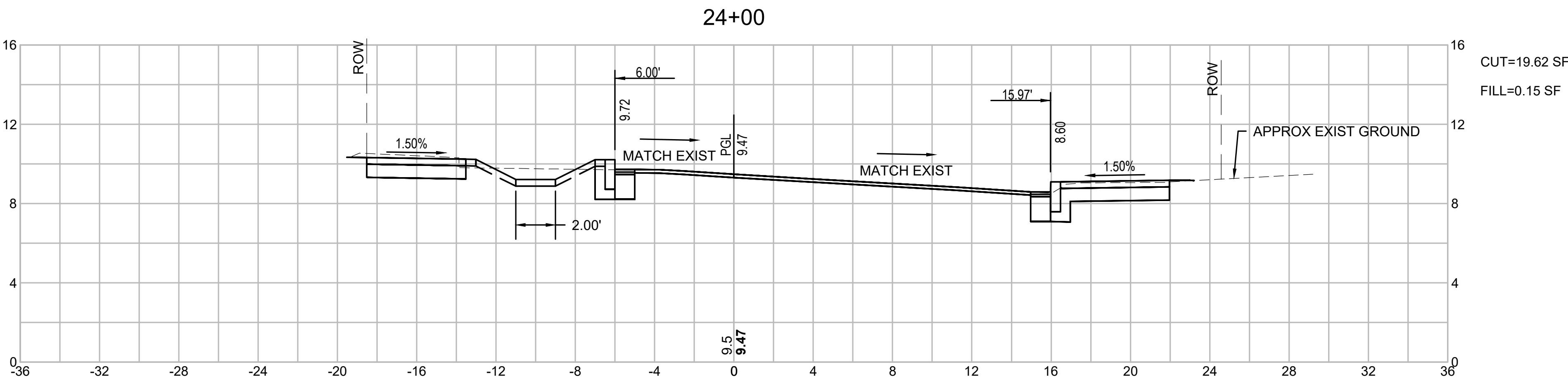
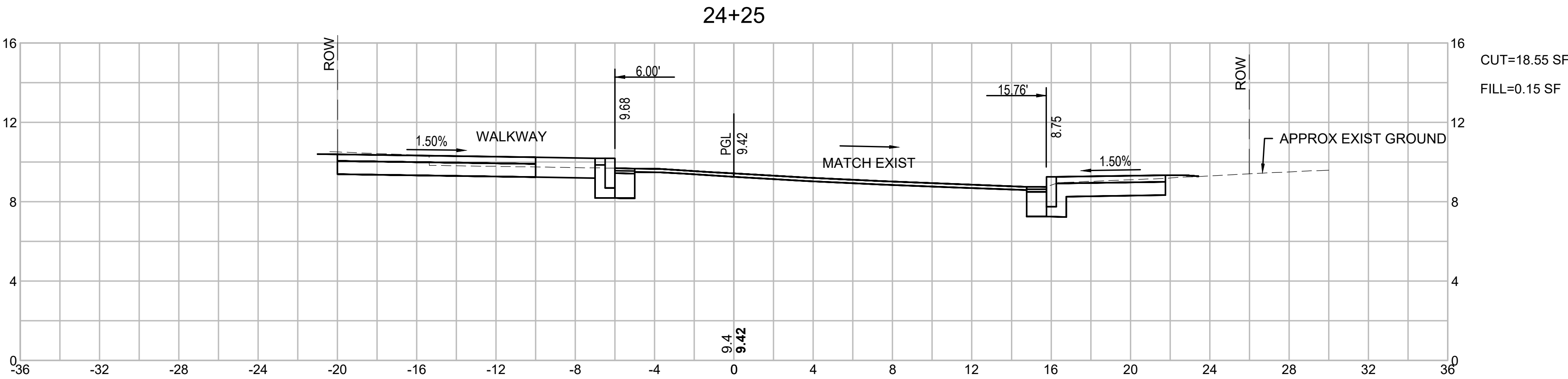
179410455

Project No.

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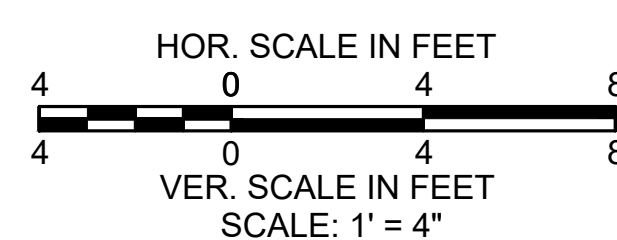
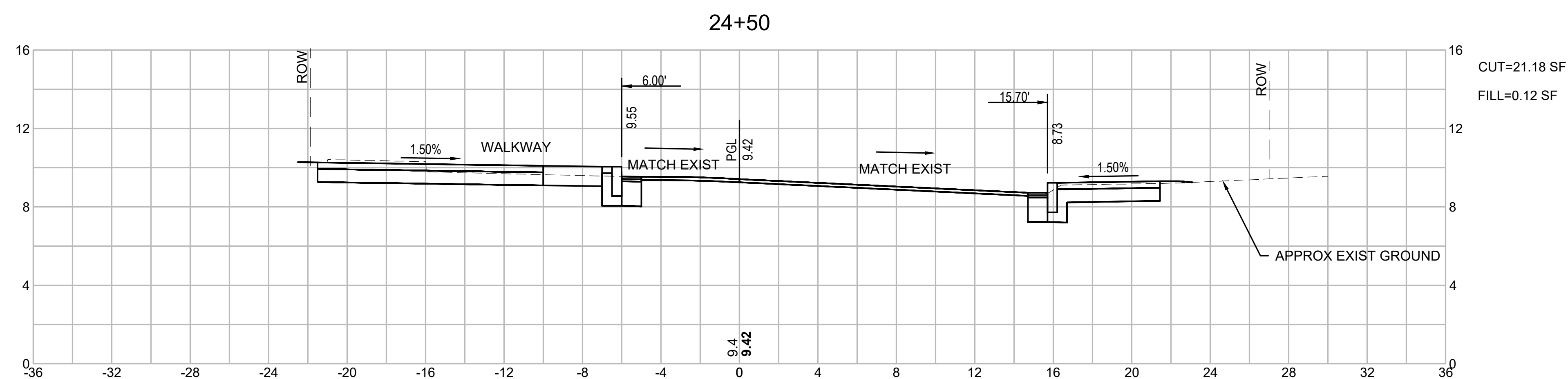
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Legend
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BRIDGE STREET RECONSTRUCTION PROJECT

179410455	1" = 4'	
Project No.	Scale	
	104 of 113	
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J:\179410455\transportation\drawing\dwg_cross_sections_goodhue.dwg
2020/02/05 1:35 PM By: Sodtler, Chris

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YY.MM.DD

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BRIDGE STREET RECONSTRUCTION PROJECT

Title

CROSS SECTIONS GOODHUE STREET
PART 8 OF 8

179410455

1" = 4'

Project No.

Scale

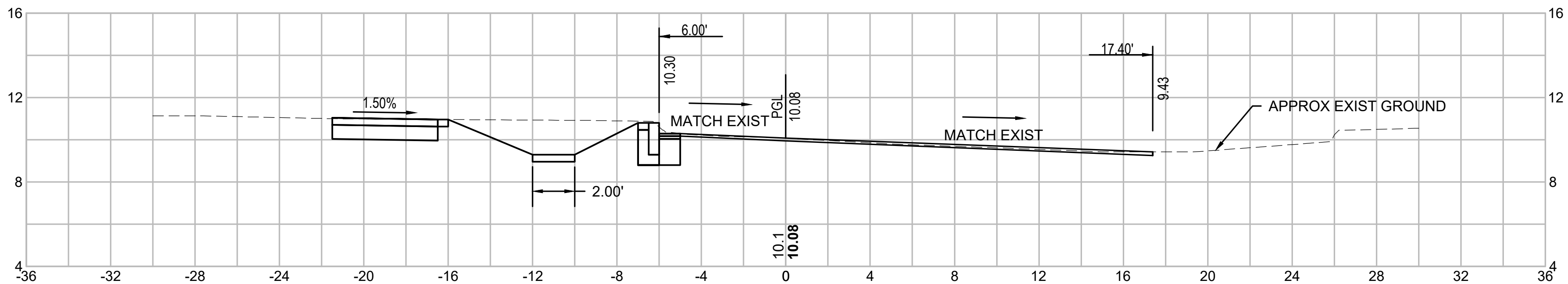
105 of 113

Drawing No.

Sheet

Revision

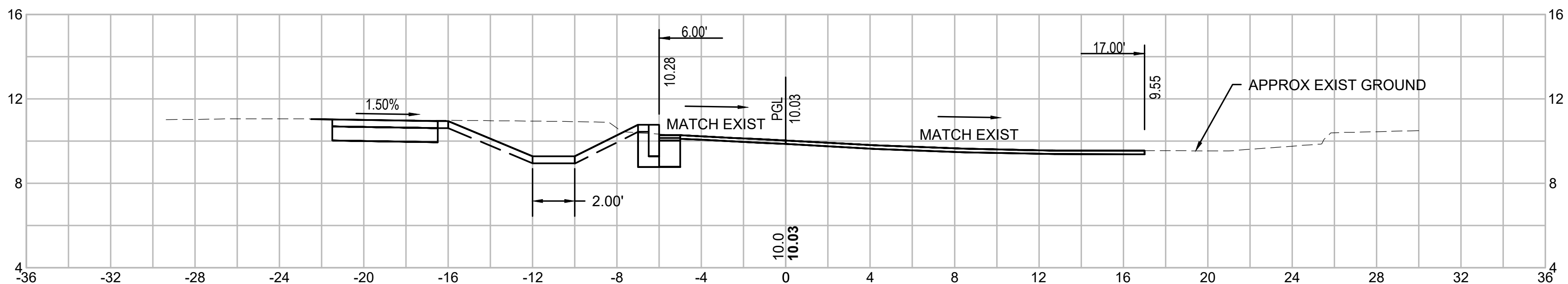
25+55.94



CUT=20.88 SF

FILL=0.04 SF

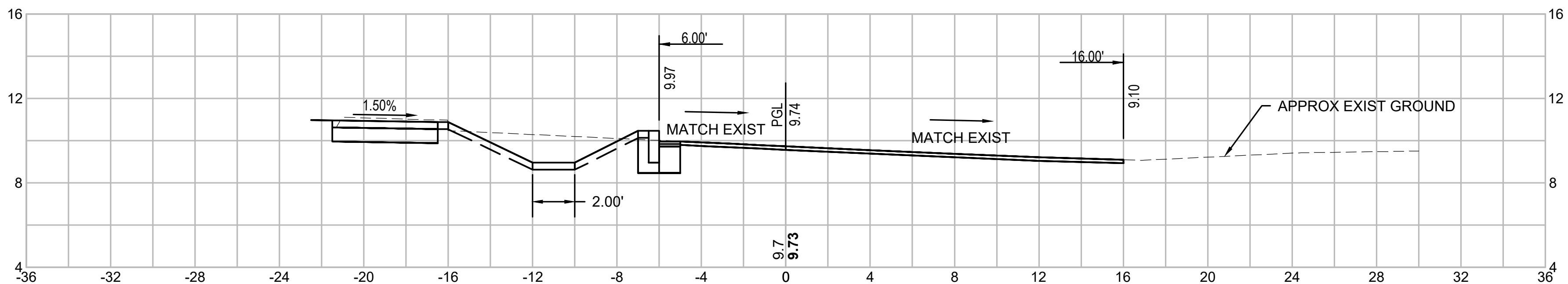
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CUT=20.35 SF

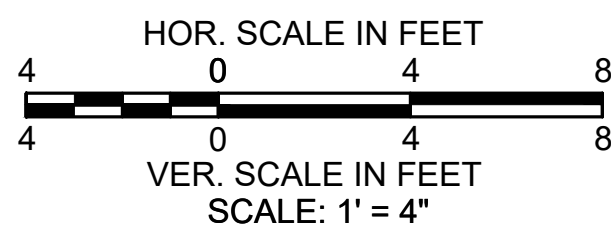
FILL=0.00 SF

25+25



CUT=17.44 SF

FILL=0.01 SF



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File Name: dwg_cross_sections_flnt.dwg

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Client/Project

BRIDGE STREET RECONSTRUCTION PROJECT

Title

CROSS SECTIONS NORTH FLINT STREET
PART 1 OF 2

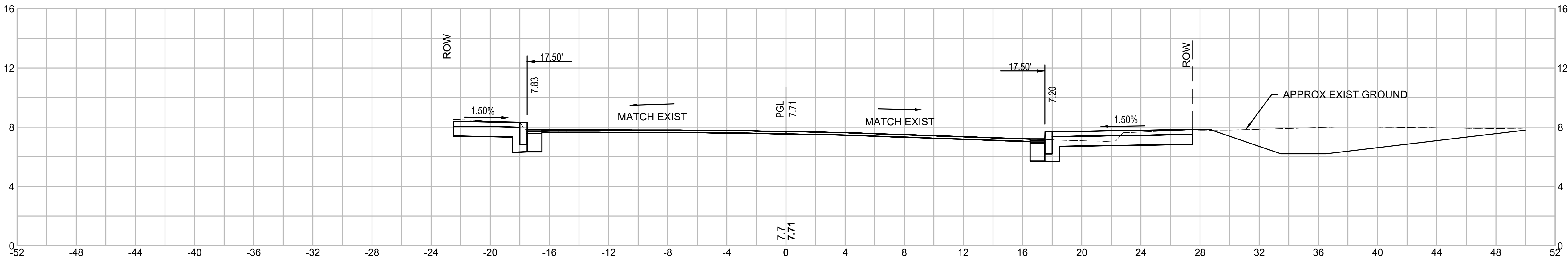
179410455 1" = 4'

Project No. Scale

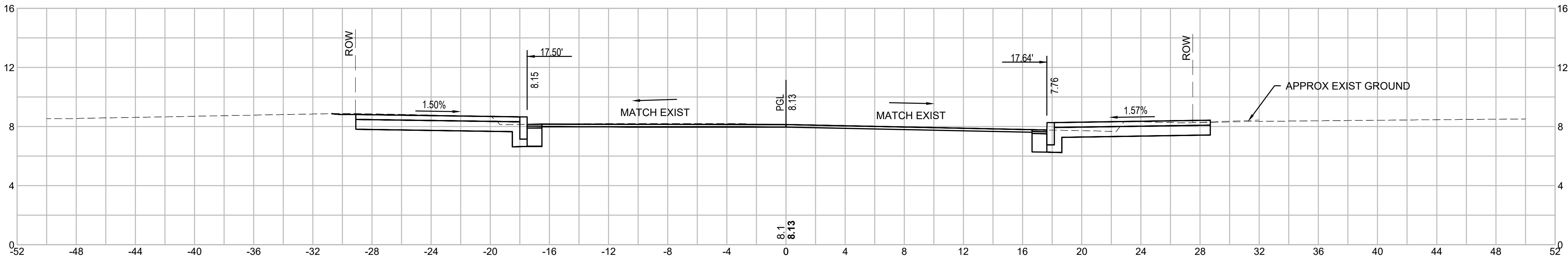
106 of 113

Drawing No. Sheet Revision

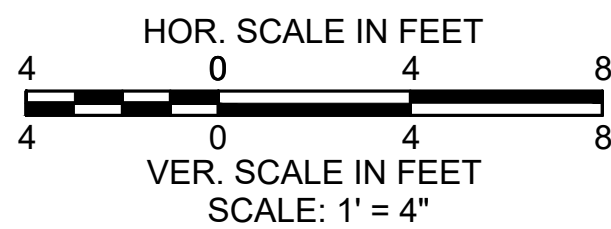
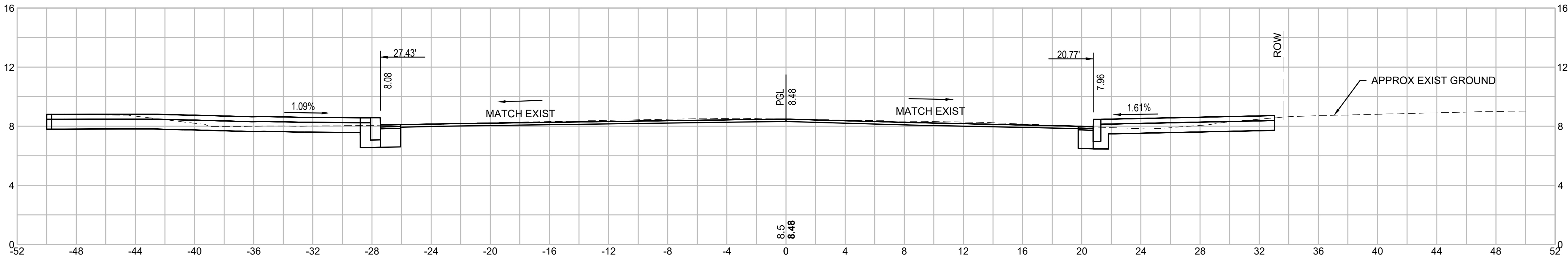
201+00



200+75



200+50



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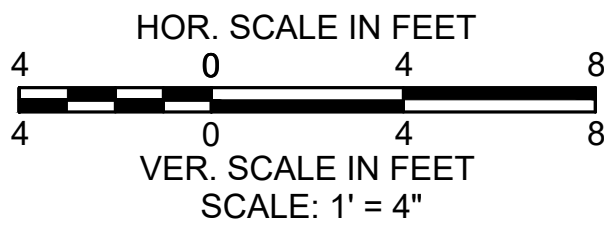
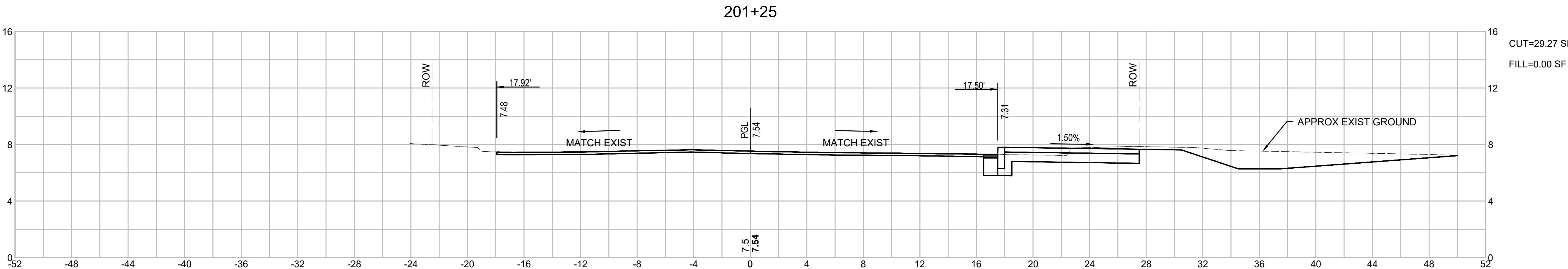
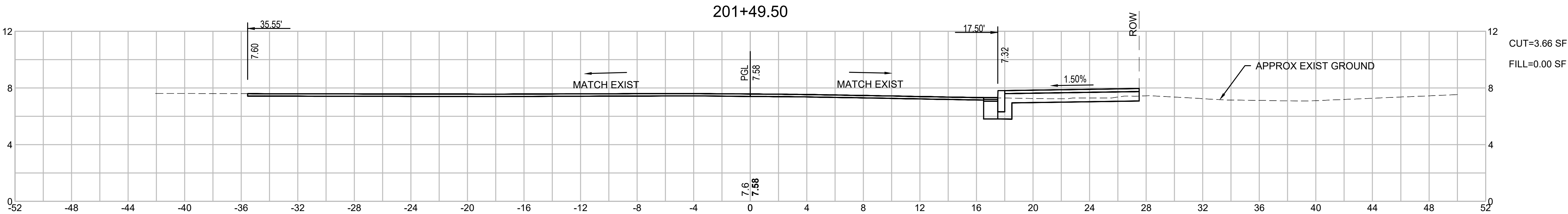
Consultants

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BRIDGE STREET RECONSTRUCTION PROJECT

Title

CROSS SECTIONS NORTH FLINT STREET
PART 2 OF 2

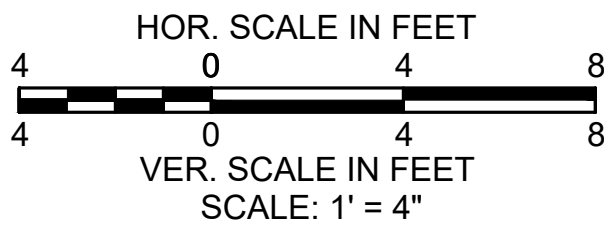
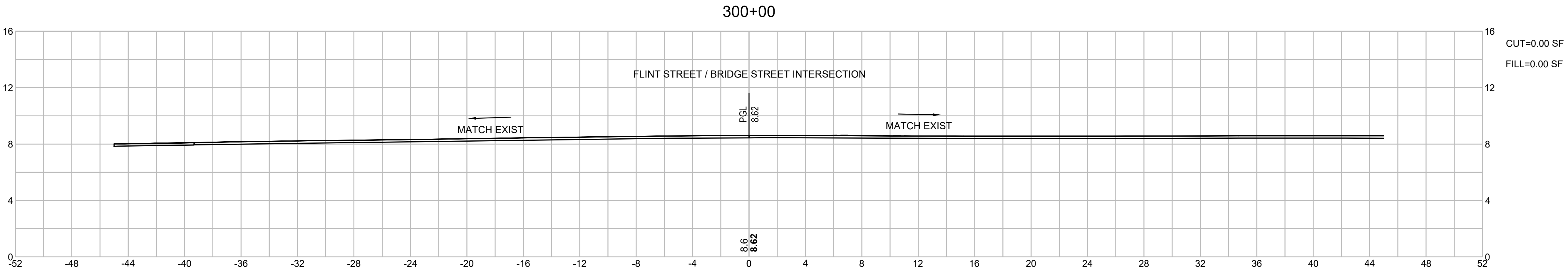
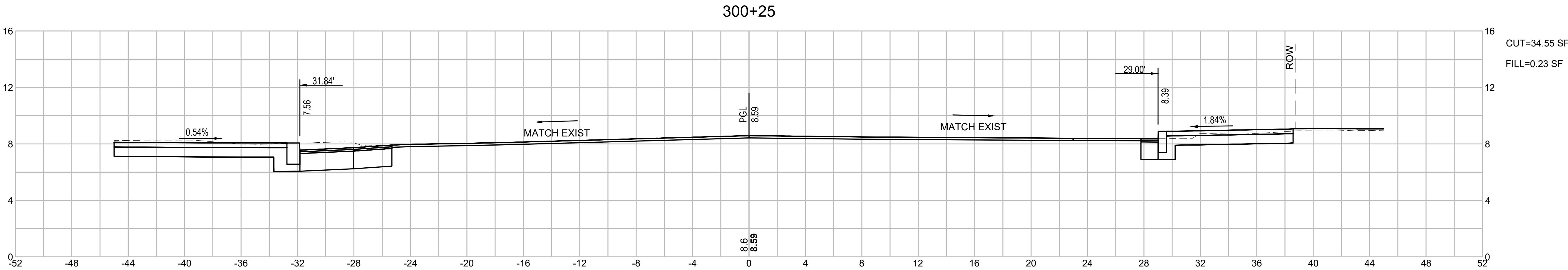
179410455 1" = 4'

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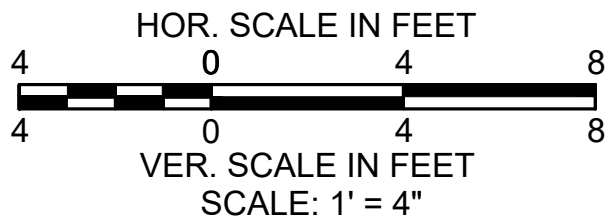
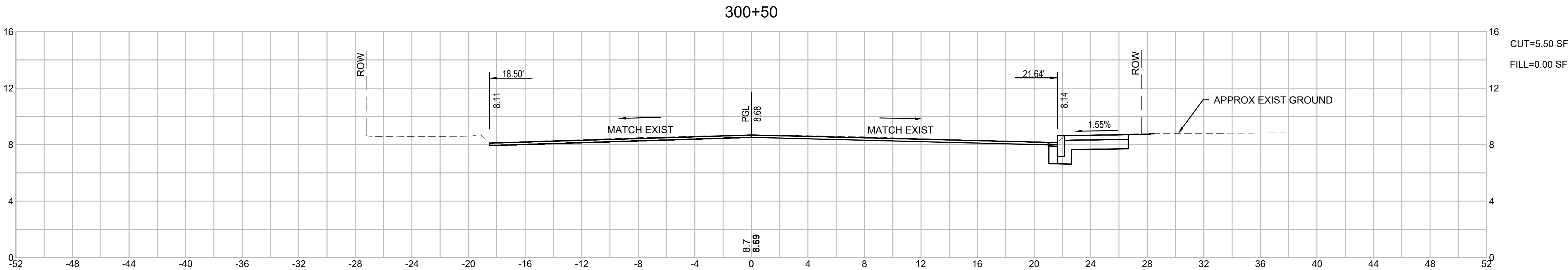
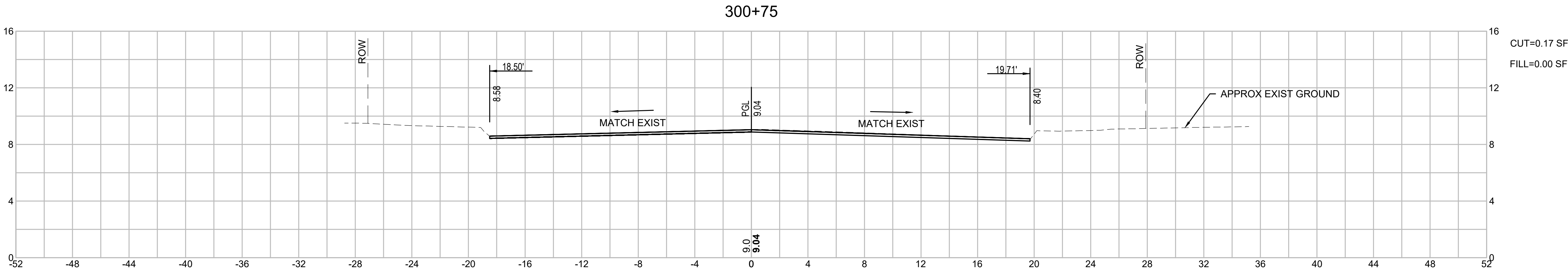
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BRIDGE STREET RECONSTRUCTION PROJECT

Title

CROSS SECTIONS SOUTH FLINT STREET
PART 2 OF 2

179410455	1" = 4'
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BRIDGE STREET RECONSTRUCTION PROJECT

Title

CROSS SECTIONS SHARED-USE PATH
PART 1 OF 4

179410455

1" = 4'

Project No.

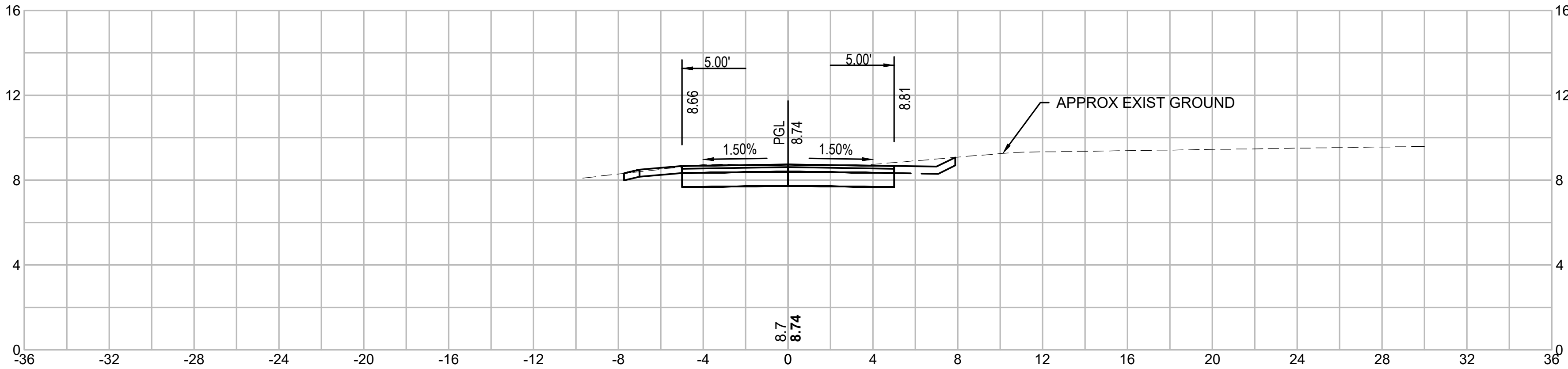
Scale

Drawing No.

Sheet

Revision

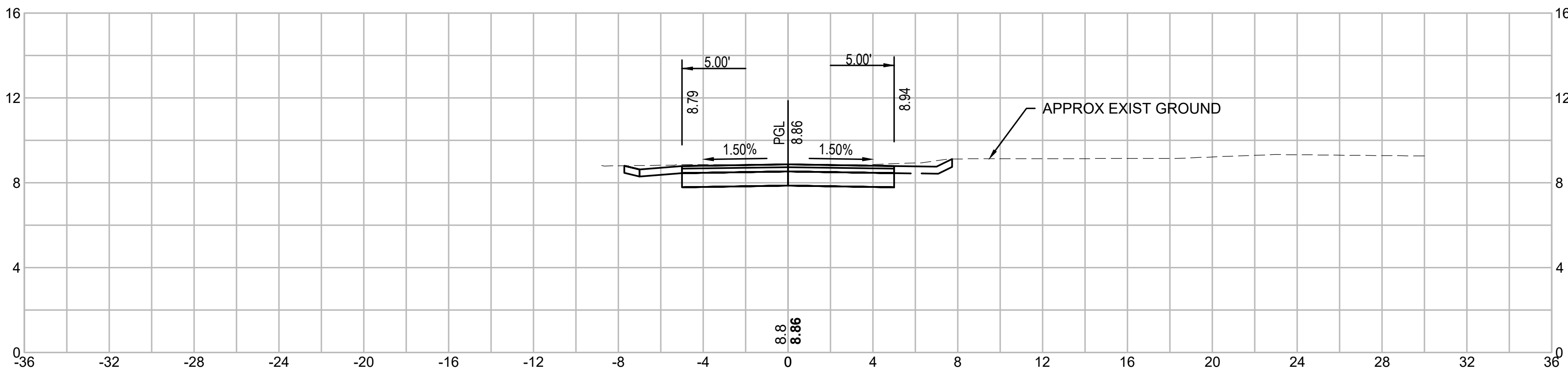
30+50



CUT=12.62 SF

FILL=0.00 SF

30+25



CUT=12.85 SF

FILL=0.00 SF

HOR. SCALE IN FEET



VER. SCALE IN FEET

SCALE: 1" = 4"



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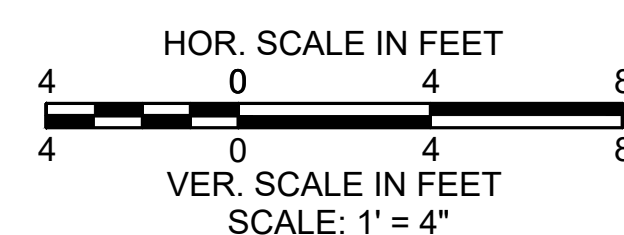
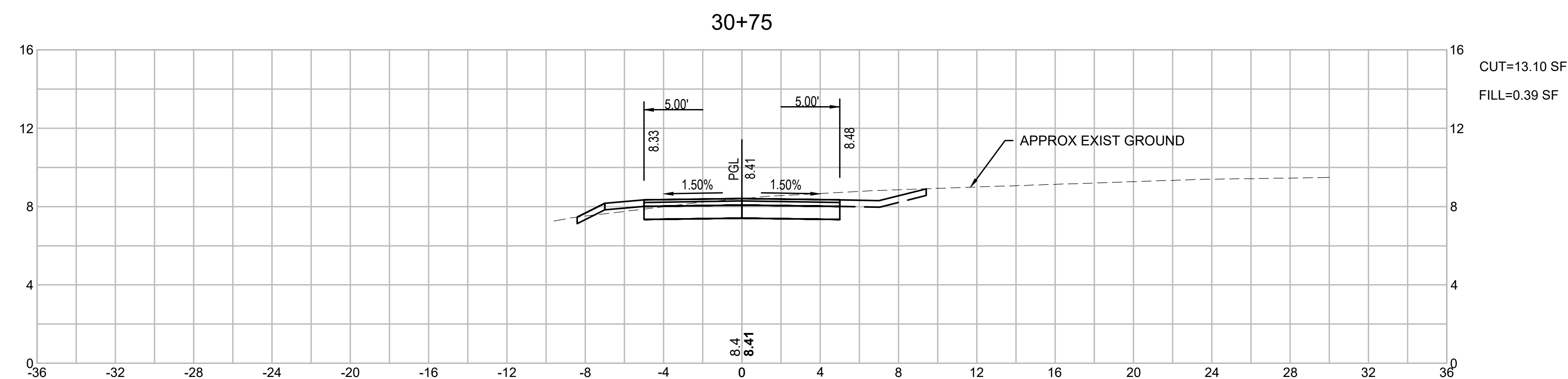
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BRIDGE STREET RECONSTRUCTION PROJECT

179410455	1" = 4'	
Project No.	Scale	
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Drawing No.	Sheet	Revision



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BRIDGE STREET RECONSTRUCTION PROJECT

Title

CROSS SECTIONS SHARED-USE PATH
PART 3 OF 4

179410455

Project No.

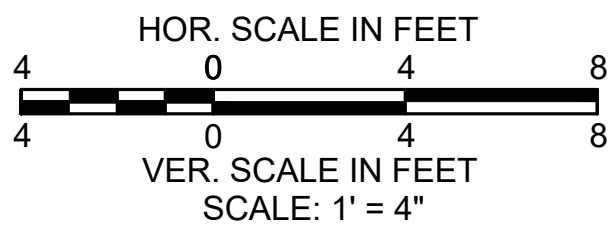
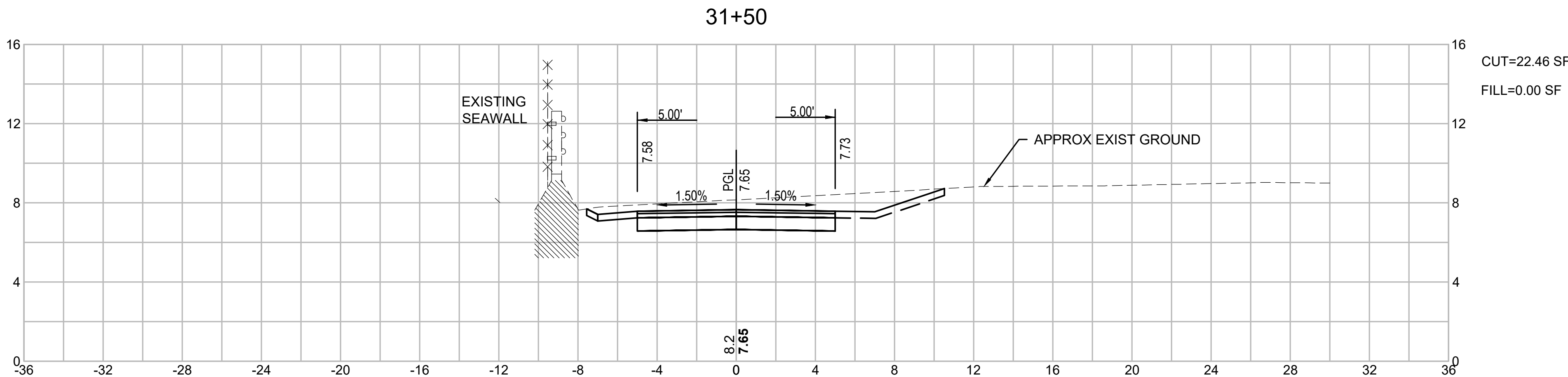
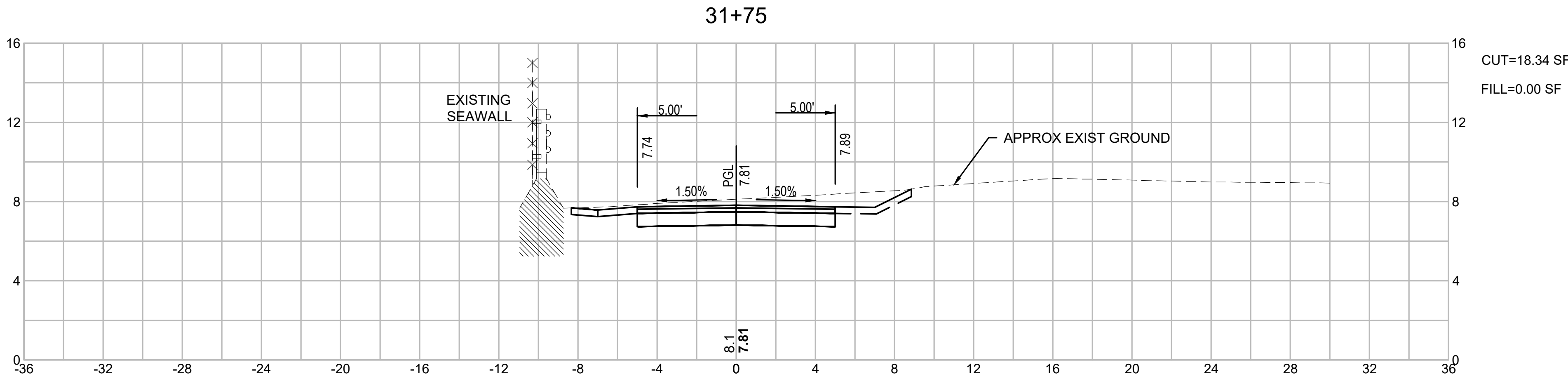
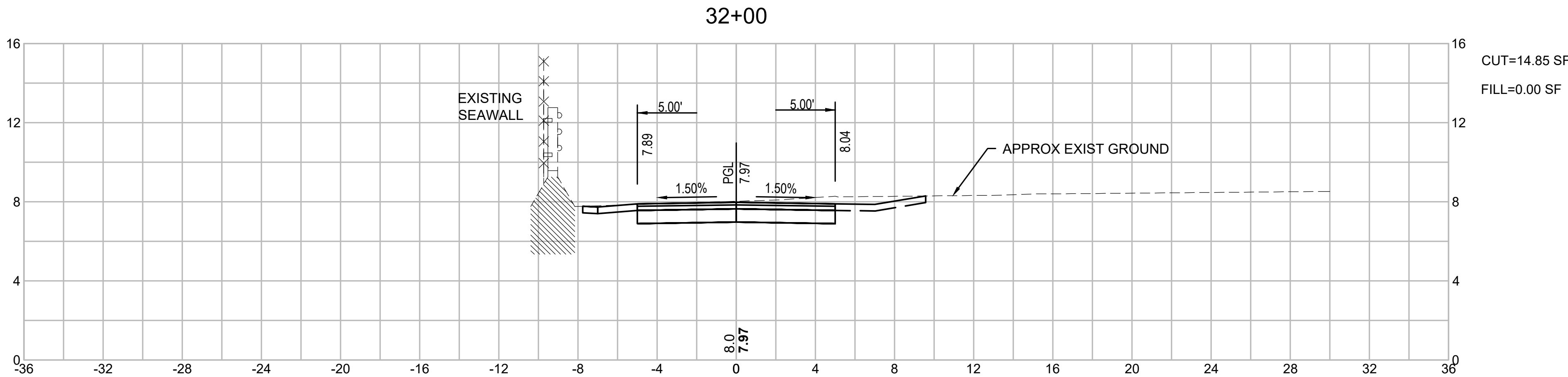
Scale

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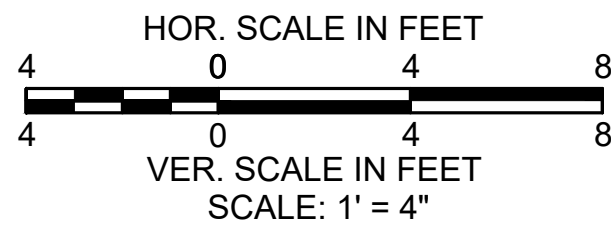
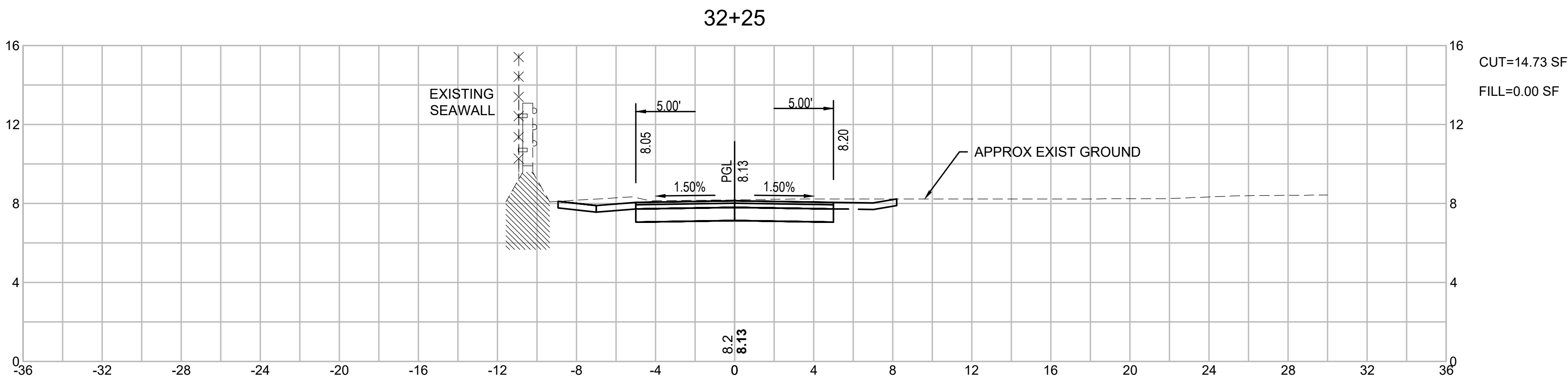
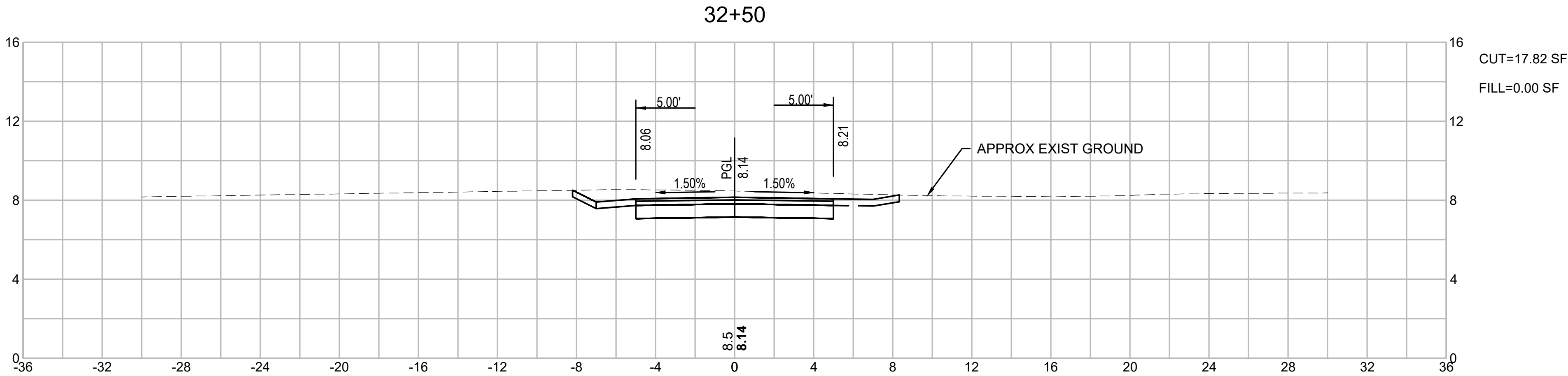
Consultants

Legend

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Notes

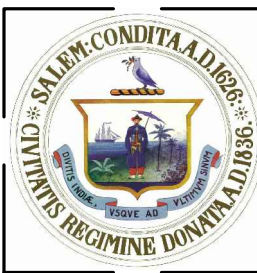
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BRIDGE STREET RECONSTRUCTION PROJECT

Title

CROSS SECTIONS SHARED-USE PATH
PART 4 OF 4

179410455	1" = 4'
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