

CITY OF SALEM

PAVEMENT MARKING & SIGNAGE IMPROVEMENTS

DERBY STREET FROM WEBB STREET TO FORT AVENUE

FORT AVENUE FROM DERBY STREET TO WINTER ISLAND ROAD



LOCUS PLAN
1"=250'

ALL WORK SHALL CONFORM TO THESE PLANS, THE MASSACHUSETTS DEPARTMENT OF TRANSPORTATION (MASSDOT) STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES DATED 1988, THE MASSDOT SUPPLEMENTAL SPECIFICATIONS DATED JUNE 15, 2012, THE MASSDOT INTERIM SUPPLEMENTAL SPECIFICATIONS DATE OCTOBER 17, 2014, THE MASSDOT 2016 CONSTRUCTION STANDARD DETAILS, THE MASSDOT 1968 STANDARD DRAWINGS FOR TRAFFIC SIGNALS AND HIGHWAY LIGHTING, THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, 2009 EDITION, AND ALL INTERIM REVISIONS PUBLISHED BY FEDERAL HIGHWAY ADMINISTRATION.

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PROFESSIONAL CERTIFICATION <div>I HEREBY CERTIFY THAT THESE PLANS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MASSACHUSETTS. LICENSE NO. 46632, EXPIRATION DATE: 06-30-2020.</div>		REUSE OF DOCUMENTS ALL DRAWINGS ARE INSTRUMENTS OF PROFESSIONAL SERVICE FOR THIS PROJECT. REUSE OR ALTERATION IS AT THE USER'S SOLE RISK.						<div><div>TOOLE</div>DESIGN</div> <div>2 OLIVER STREET, SUITE 305 BOSTON, MA 02109 PHONE: 617.619.9910 FAX: 301.927.2800 www.tooledesign.com</div>	SALEM DESIGN ASSESSMENTS CITY OF SALEM	PAVEMENT MARKING & SIGNAGE	PROJECT NO. 60154.04
		FORT AVENUE TWO-WAY SEPARATED BICYCLE FACILITY	DATE 8/29/2019								
			DRAWING NO. G-01								
			SHEET NO. 1 OF 10								


GENERAL NOTES

1. THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE FEDERAL, STATE, AND LOCAL LAWS, RULES, REGULATIONS AND SAFETY CODES IN THE CONSTRUCTION OF ALL IMPROVEMENTS. STRIPING WORK SHALL ALSO MEET ALL APPLICABLE REQUIREMENTS OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
2. BASE MAPPING WAS ESTABLISHED UTILIZING GIS DATA PROVIDED BY THE CITY OF SALEM.
3. EXISTING CONDITIONS MAY VARY FROM THOSE SHOWN ON THE PLANS. THE CONTRACTOR SHALL VERIFY EXISTING CONDITIONS AND ADJUST WORK PLAN ACCORDINGLY PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL REPORT ANY CONFLICTS BETWEEN EXISTING CONDITIONS AND PROPOSED FEATURES TO THE ENGINEER.
4. ALL PAVEMENT MARKINGS SHALL MEET MUTCD STANDARD SPECIFICATIONS.
5. IF PROPOSED PAVEMENT MARKINGS ARE SHOWN IN THE AREA OF EXISTING PAVEMENT MARKINGS, ERADICATE ANY CONFLICTING MARKINGS USING AN APPROVED NON-DESTRUCTIVE METHOD SUCH THAT NO TRACES OF EXISTING MARKINGS ARE VISIBLE. THE CONTRACTOR SHALL NOT INSTALL NEW PAVEMENT MARKINGS WITHOUT ERADICATING EXISTING PAVEMENT MARKINGS FIRST AND AS APPROVED BY THE TOWN OF ACTON.
6. THE CONTRACTOR SHALL TAKE ADEQUATE PRECAUTIONS TO PROTECT ALL WALKS, GRADING, SIDEWALKS, AND FEATURES ADJACENT TO THE LIMITS OF WORK. AS DIRECTED BY THE ENGINEER, AND AT THE CONTRACTOR’S OWN EXPENSE, THE CONTRACTOR SHALL REPAIR, REPLACE, OR OTHERWISE MAKE GOOD ANY DAMAGE CAUSED BY THE CONTRACTOR’S OPERATIONS.
7. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH ARE A RESULT OF THE CONTRACTOR’S ACTIVITIES OR METHOD OF CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ALL DAMAGED UTILITIES AND RESTORE SERVICE IN A TIMELY MANNER, PRIOR TO PROCEEDING WITH CONSTRUCTION, AT THE CONTRACTOR’S EXPENSE.
8. WHERE AN EXISTING UTILITY IS FOUND TO BE IN CONFLICT WITH THE PROPOSED WORK, THE LOCATION, ELEVATION AND SIZE OF THE UTILITY SHALL BE ACCURATELY DETERMINED WITHOUT DELAY BY THE CONTRACTOR, AND THE INFORMATION FURNISHED TO THE ENGINEER FOR RESOLUTION OF THE CONFLICT.
9. AREAS OUTSIDE THE LIMITS OF PROPOSED WORK DISTURBED BY THE CONTRACTOR’S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR TO THEIR ORIGINAL CONDITION AT THE CONTRACTORS EXPENSE.
10. THE TERM "PROPOSED" (PROP) MEANS WORK TO BE CONSTRUCTED USING NEW MATERIALS, REMOVING EXISTING MATERIALS IDENTIFIED AS "REMOVE AND STACK" (R&S), OR, WHERE APPLICABLE, RE-USING EXISTING MATERIALS IDENTIFIED AS "REMOVE AND RESET" (R&R).
11. THE TERM "MEET EX" MEANS TO MEET BOTH THE EXISTING HORIZONTAL ALIGNMENT AND ELEVATION.
12. ERADICATE ALL EXISTING ROADWAY EDGE LINE MARKINGS WITHIN PROJECT LIMITS.
13. STRIPED PARKING SPACES SHALL MAINTAIN A MINIMUM DISTANCE OF 4’ FROM ALL DRIVEWAYS UNLESS OTHERWISE NOTED. CENTER PARKING SPACES BETWEEN DRIVEWAYS WHERE APPLICABLE.
14. FLEXPOSTS TO BE CENTERED IN BUFFER SPACE UNLESS OTHERWISE NOTED.

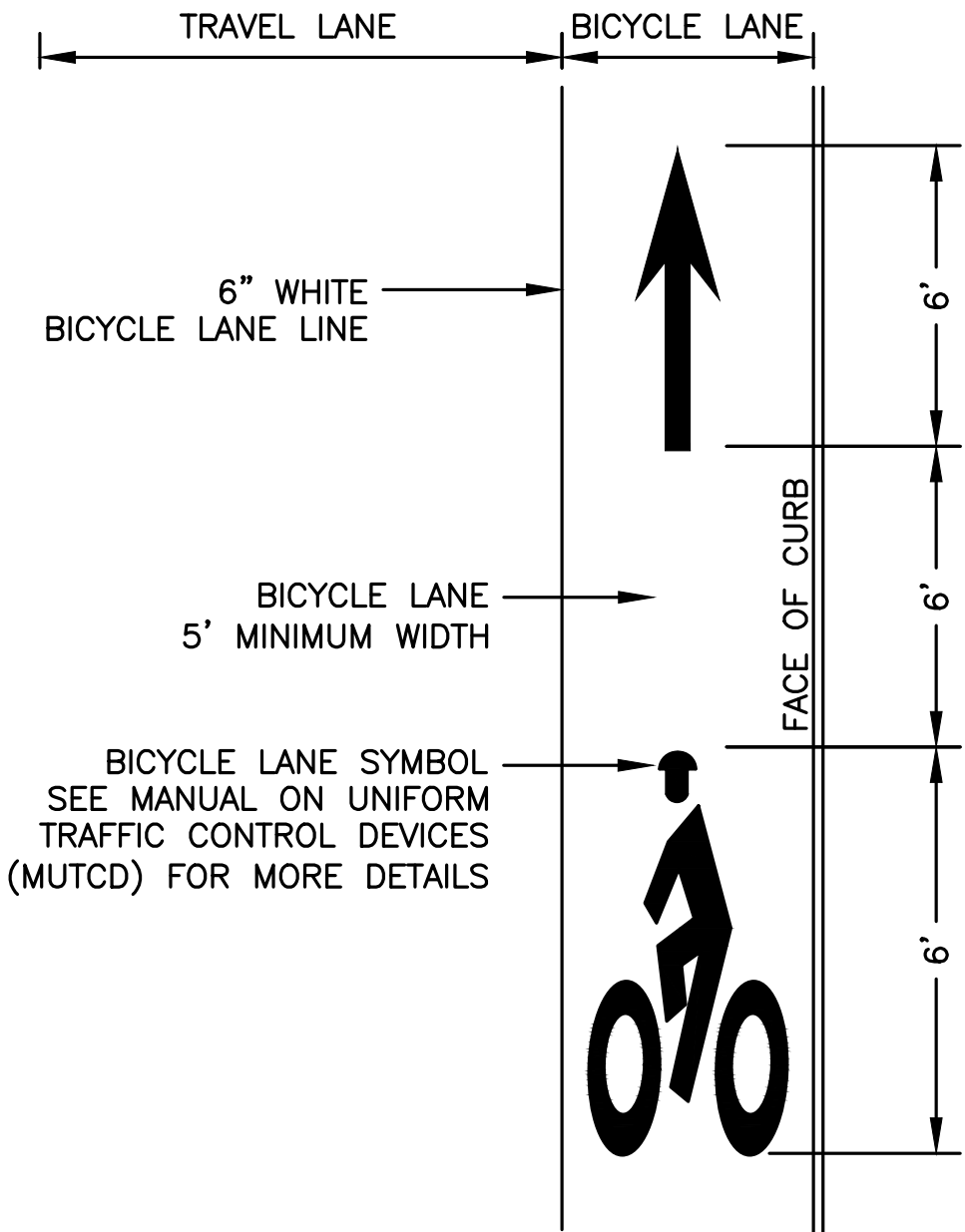
LEGEND AND ABBREVIATIONS

EXISTING	PROPOSED	
	--- --	LIMIT OF PAVEMENT MARKINGS
	BE →	BEGIN ERADICATION
	EE →	END ERADICATION
	ES →	ERADICATE SYMBOL
	EX	EXISTING
	MNT	MOUNT
	R&R	REMOVE AND RESET
	●	SIGN POST
	●	HYDRANT
	●	FLEX POST
	●	TRASH BARREL
	●	STREET LIGHT
	●	TRAFFIC SIGNAL
	●	PUSH BUTTON
	●	UTILITY POLE
	●	TREE
	●	DOUBLE HEAD METER
	●	SINGLE HEAD METER
	●	MULTI-SPACE METER
	●	ELECTRIC BOX
	●	BOLLARD
	●	BICYCLE RACK
	●	WHEELCHAIR RAMP
	●	FENCE
	●	WALL
	●	RAILROAD TRACKS
	●	BICYCLE LANE SYMBOL
	●	SHARED LANE MARKING SYMBOL
	SWEL	4" SOLID WHITE EDGE LINE
	SWLL	4" SOLID WHITE LANE LINE
	SWBLL	6" SOLID WHITE BICYCLE LANE LINE
	BWLL	4" BROKEN WHITE LANE LINE (10' MARK, 20' SPACE)
	DWLL	4" DOTTED WHITE LANE LINE (2' LINE, 4' SPACE)
	DWBLL	6" DOTTED WHITE BICYCLE LANE LINE (2' LINE, 4' SPACE)
	SYEL	4" SOLID YELLOW EDGE LINE
	SYCL	4" SOLID YELLOW CENTER LINE
	BYCL	4" BROKEN YELLOW CENTER LINE (10' MARK, 20' SPACE)
	DYCL	2-4" DOUBLE YELLOW CENTER LINES
	DDYCL	2-4" DOTTED DOUBLE YELLOW CENTER LINES (2' LINE, 4' SPACE)
	SDYCL	4" SINGLE DOTTED YELLOW CENTER LINES (2' LINE, 4' SPACE)
	SWCHL	8" SOLID WHITE CHANNELIZATION LINE
	SYCHL	8" SOLID YELLOW CHANNELIZATION LINE
	SL	12" MINIMUM WHITE STOP LINE (UNLESS OTHERWISE NOTED)
	CW	12" MINIMUM WHITE CROSSWALK LINES WITH 12" LONGITUDINAL LINES AT 4' O.C. (UNLESS OTHERWISE SHOWN)
		8" SWCHL OR SYCHL (10' O.C. AT 45' UNLESS OTHERWISE NOTED)
		WHITE PAVEMENT ARROW AND LEGEND
		COLORIZED PAVEMENT – MATERIAL AS SPECIFIED ON PLANS
		NEW OR RELOCATED SIGN

100% DESIGN SUBMISSION

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		<table><tr><td>DSGN</td><td>WH</td><td rowspan="3"></td><td rowspan="3"></td><td rowspan="3"></td><td rowspan="3"></td></tr><tr><td>DR</td><td>WH</td></tr><tr><td>CHK</td><td>LG</td></tr><tr><td>APVD</td><td>JD</td><td>NO.</td><td>DATE</td><td>REVISION</td><td>BY</td><td>APVD</td></tr></table>								DSGN	WH					DR	WH	CHK	LG	APVD	JD	NO.	DATE	REVISION	BY	APVD	DRAWING NO. G-02	
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						SHEET NO. 2 OF 10																						

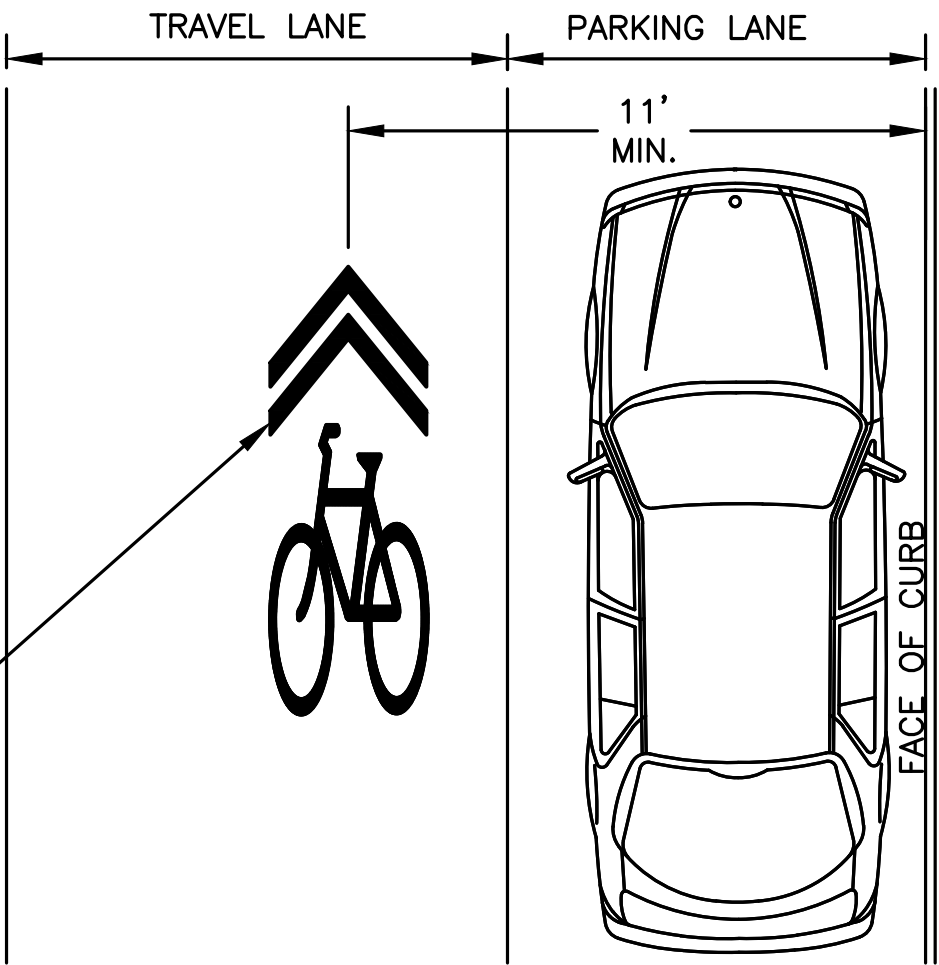
- BICYCLE LANE STRIPING NOTES:
- DO NOT PLACE SYMBOLS ON LANE LINES.
 - PLACE BICYCLE LANE SYMBOLS WITHIN CENTER OF BICYCLE LANE.
 - SEE PLAN SHEETS FOR SYMBOL LOCATIONS.
 - TRAVEL AND PARKING LANE WIDTHS ARE AS SHOWN ON THE DRAWINGS.
 - DO NOT PLACE SYMBOLS WITHIN DASHED LINES.



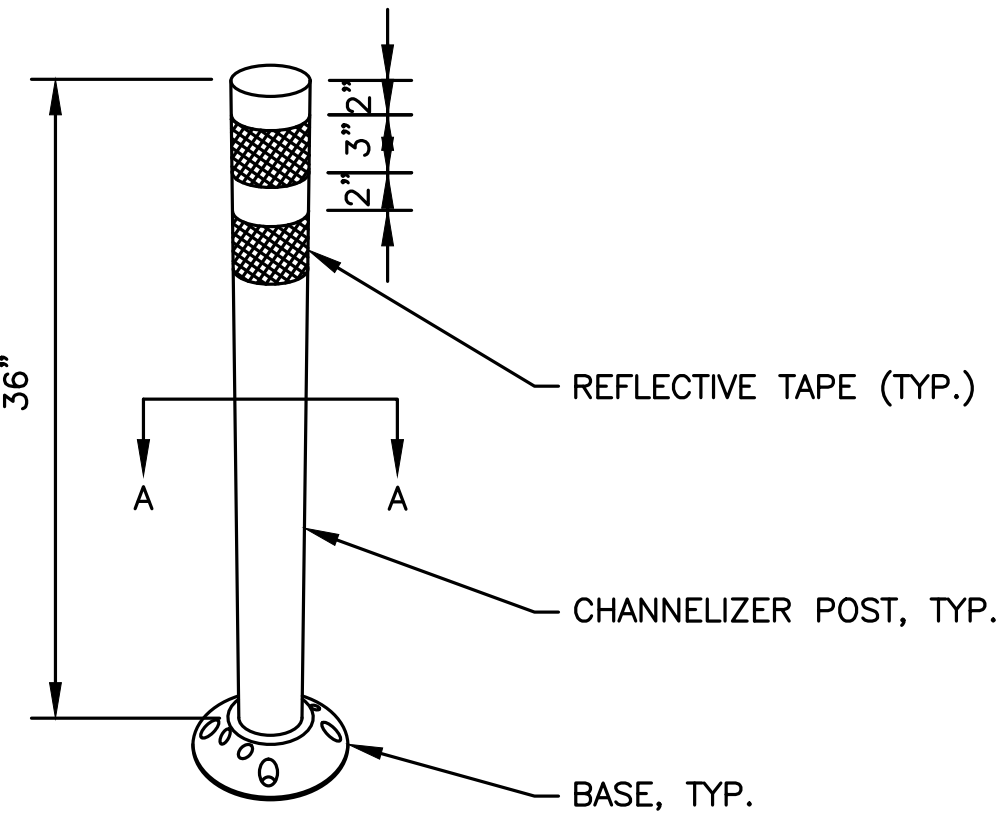
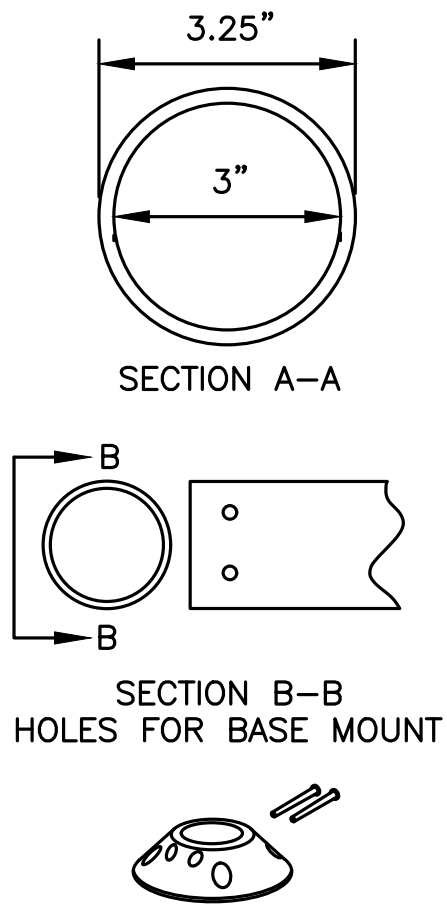
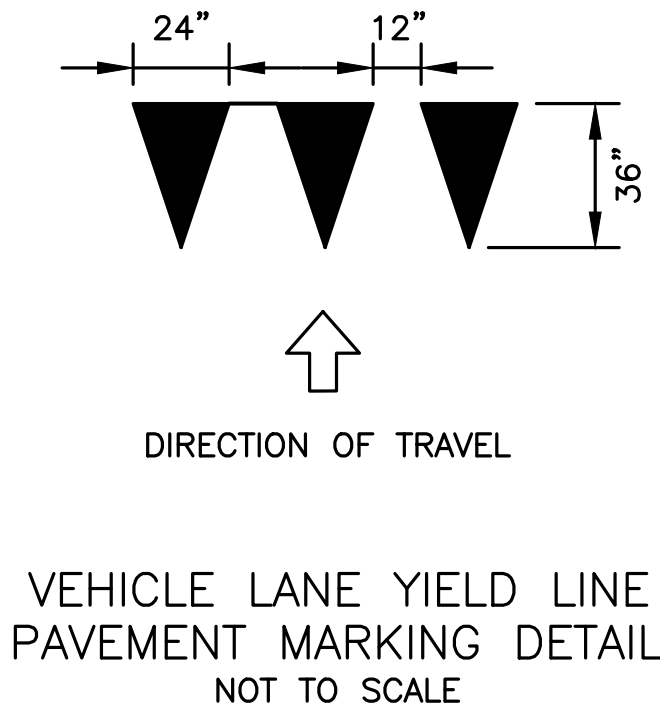
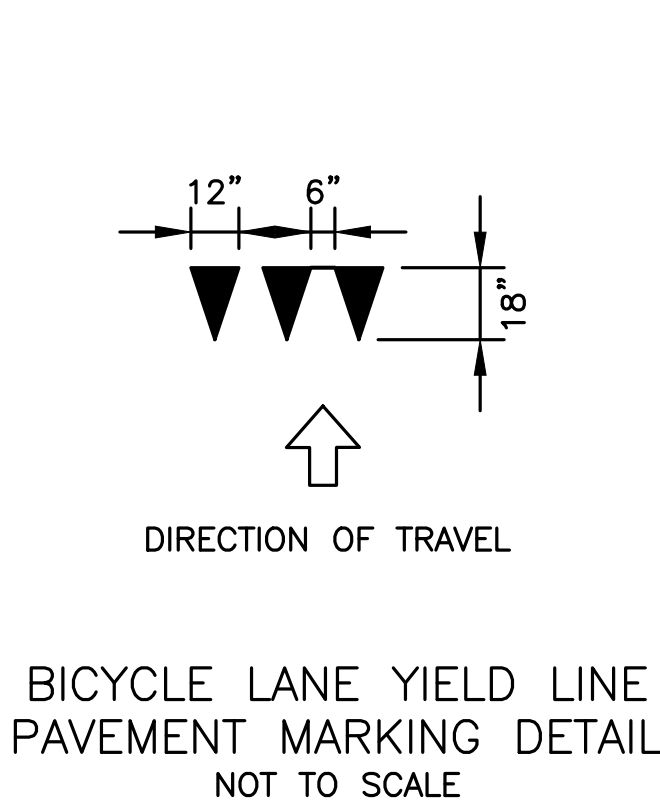
TYPICAL BICYCLE LANE STRIPING ADJACENT TO CURB
NOT TO SCALE

- SHARED LANE MARKING SYMBOL NOTES:
- IN LOCATIONS WITH ON-STREET PARKING, SHARED LANE MARKING SYMBOLS SHALL BE PLACED SO THAT THEIR CENTERS ARE A MINIMUM OF 11 FEET FROM THE ADJACENT FACE OF CURB UNLESS OTHERWISE NOTED ON THE PLANS.
 - IN LOCATIONS WITHOUT PARKING, SHARED LANE MARKING SYMBOLS SHALL BE PLACED SO THAT THEIR CENTERS ARE A MINIMUM OF 4 FEET FROM THE ADJACENT FACE OF CURB UNLESS OTHERWISE NOTED ON THE PLANS.
 - IF LOCATED IN A TURN LANE, SHARED LANE MARKING SYMBOLS SHALL BE CENTERED IN THE LANE UNLESS OTHERWISE NOTED ON THE PLANS.
 - DO NOT PLACE SYMBOLS ON LANE LINES.
 - SEE PLANS FOR SYMBOL LOCATIONS.

SHARED LANE MARKING
SEE MANUAL ON UNIFORM
TRAFFIC CONTROL DEVICES
(MUTCD) FOR MORE DETAILS

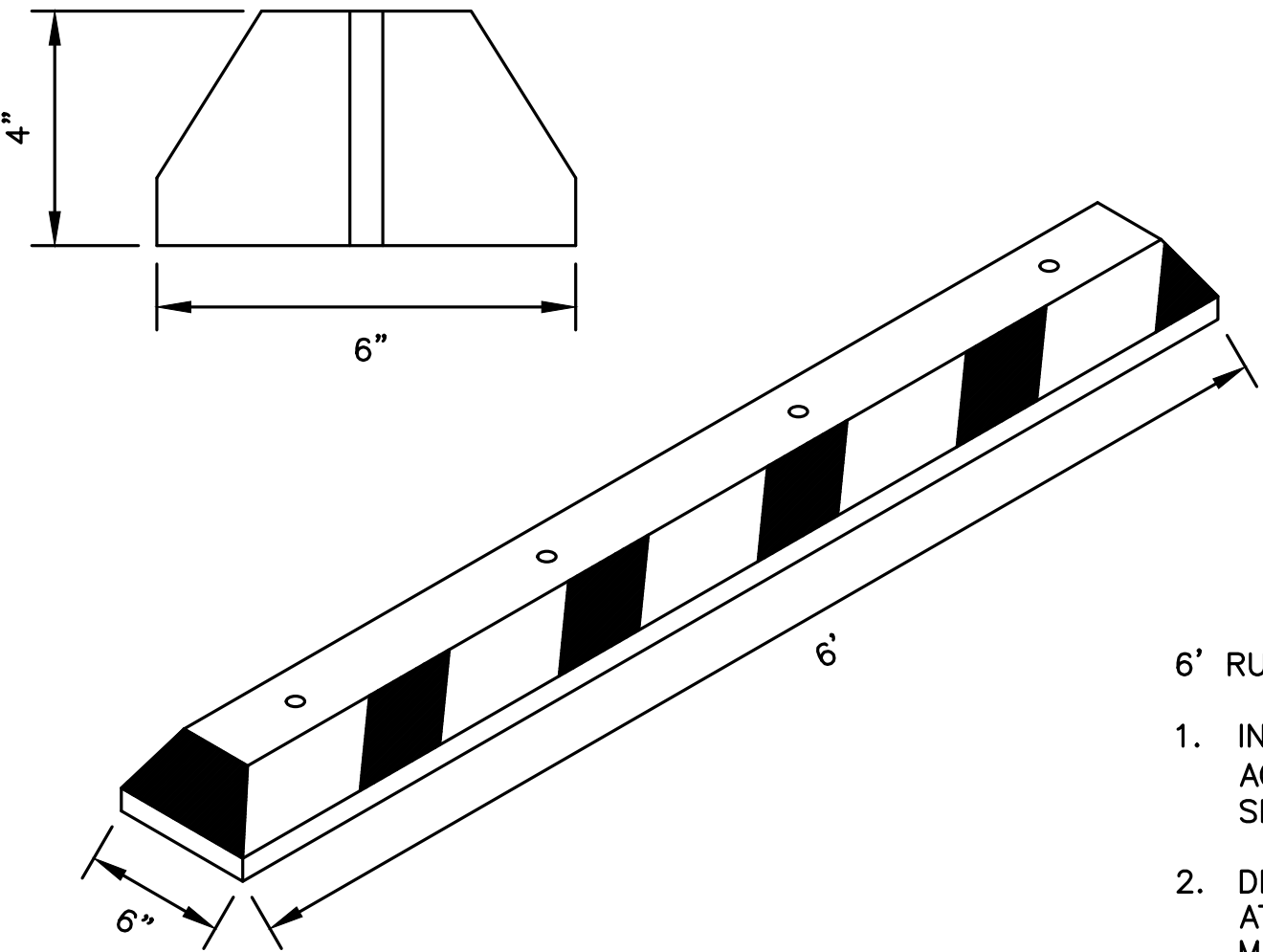


SHARED LANE MARKING SYMBOL
PLACEMENT ADJACENT TO PARKING
NOT TO SCALE



- FLEXPOST NOTES:
- INSTALLATION TO BE COMPLETED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.
 - DIMENSIONS, MATERIALS, AND ATTACHMENTS MAY VARY BETWEEN MANUFACTURERS.
 - COLOR OF POST SHALL MATCH COLOR OF APPLICABLE LANE LINE.

36" SURFACE MOUNT
(BOLTED) FLEXPOST DETAIL
NOT TO SCALE

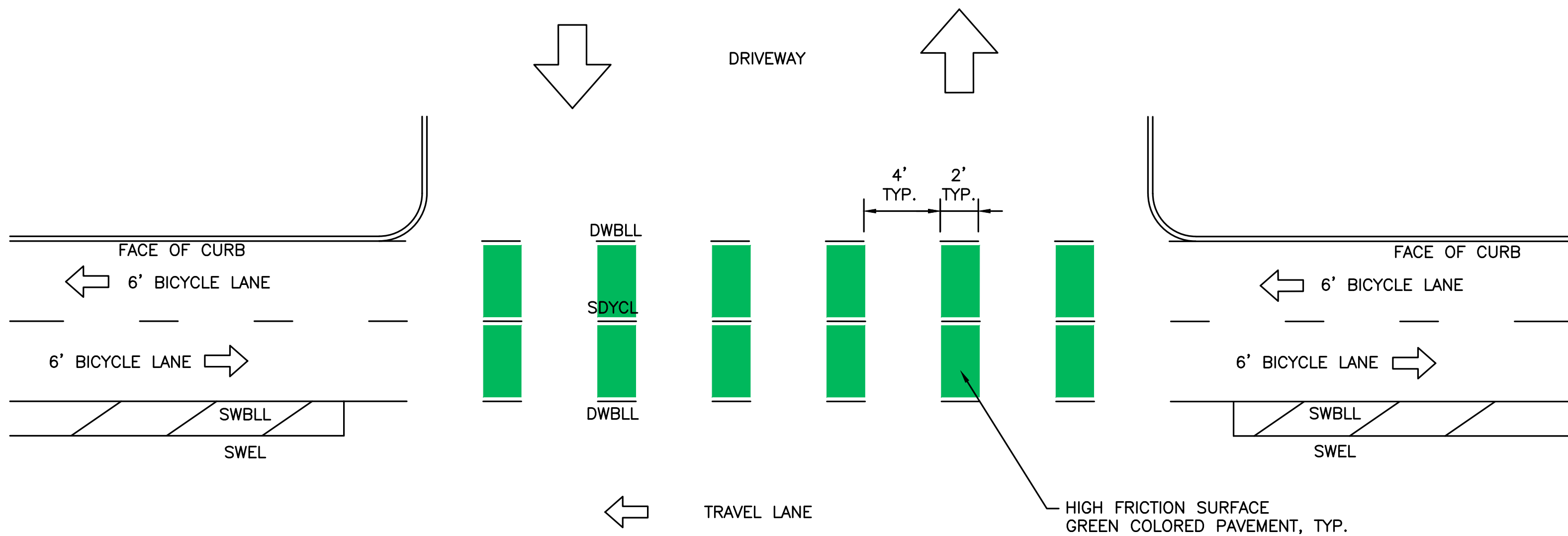


- 6' RUBBER PARKING STOP NOTES:
- INSTALLATION TO BE COMPLETED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.
 - DIMENSIONS, MATERIALS, AND ATTACHMENTS MAY VARY BETWEEN MANUFACTURERS.

6' RUBBER PARKING STOP
NOT TO SCALE

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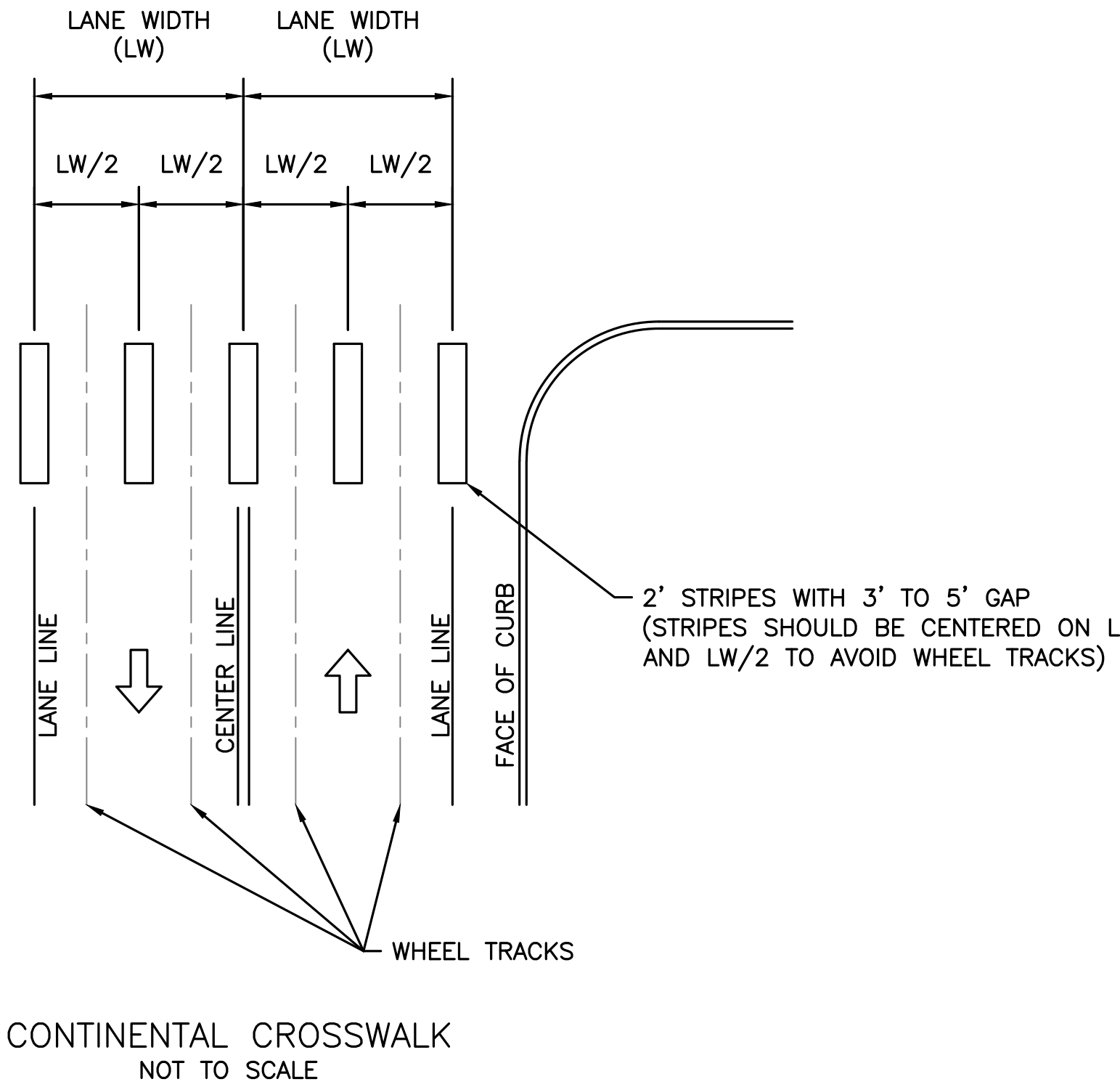
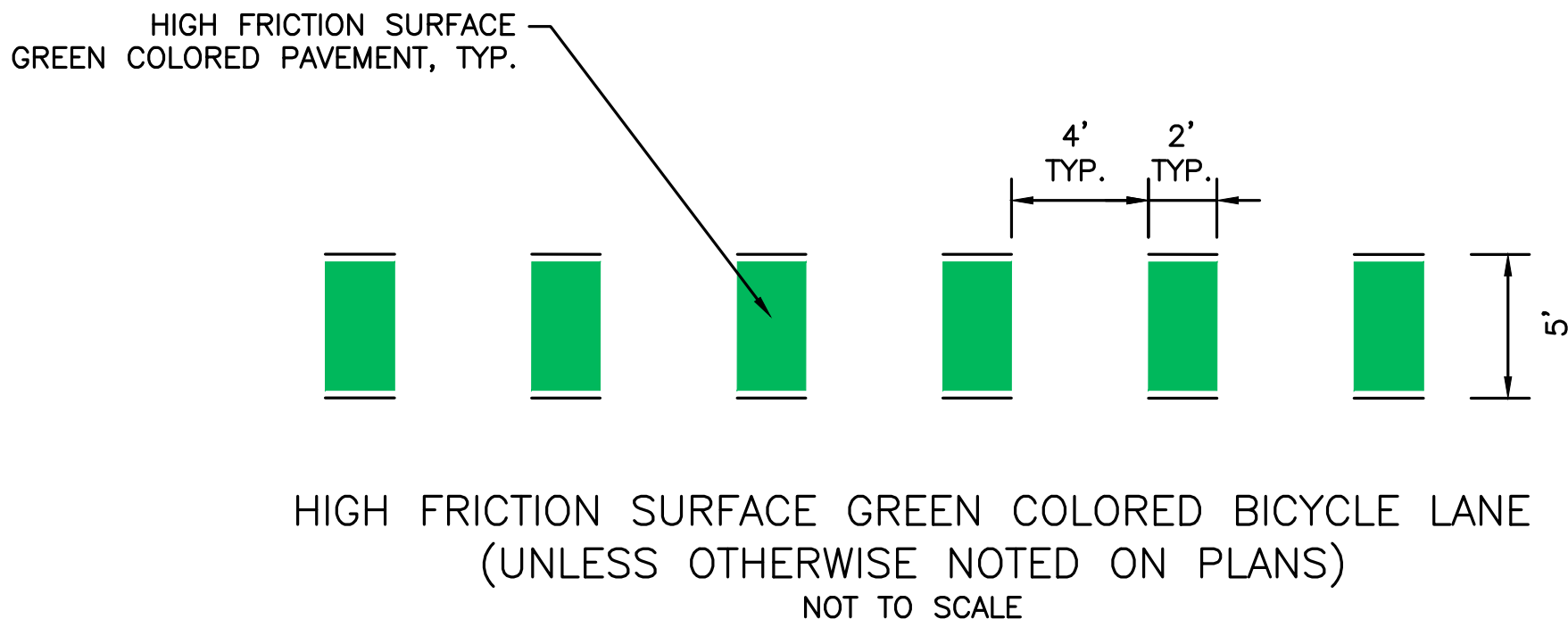
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DRIVEWAY TREATMENT NOTES:

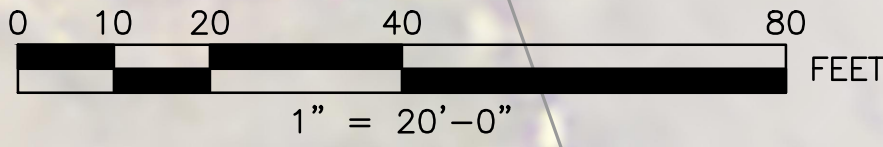
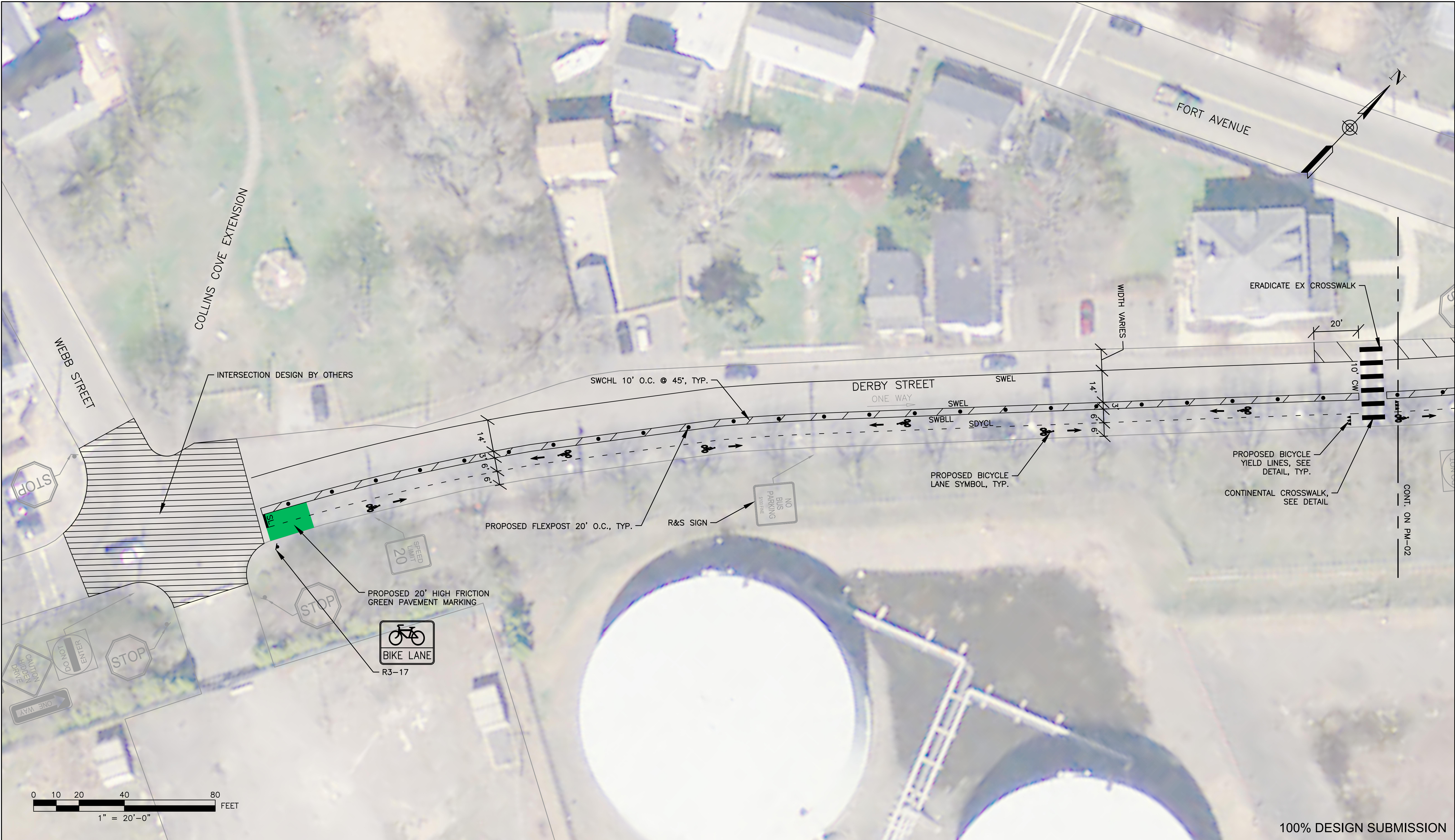
1. HIGH FRICTION GREEN COLORED PAVEMENT MAY BE USED WHERE MOTOR VEHICLE TRAFFIC CAN TURN RIGHT ACROSS THE BICYCLE LANE. SEE PLANS FOR LOCATIONS.
2. INSTALL HIGH FRICTION GREEN COLORED PAVEMENT BETWEEN 2' DOTTED WHITE LANE LINES AS SHOWN ON PLANS.
3. INSTALL HIGH FRICTION GREEN COLORED PAVEMENT SO THAT CROSSWALK MARKINGS ARE NOT DISTURBED.
4. TRAVEL LANE WIDTHS ARE AS SHOWN ON PLANS.

TYPICAL DRIVEWAY TREATMENT
(UNLESS OTHERWISE NOTED ON PLANS)
NOT TO SCALE



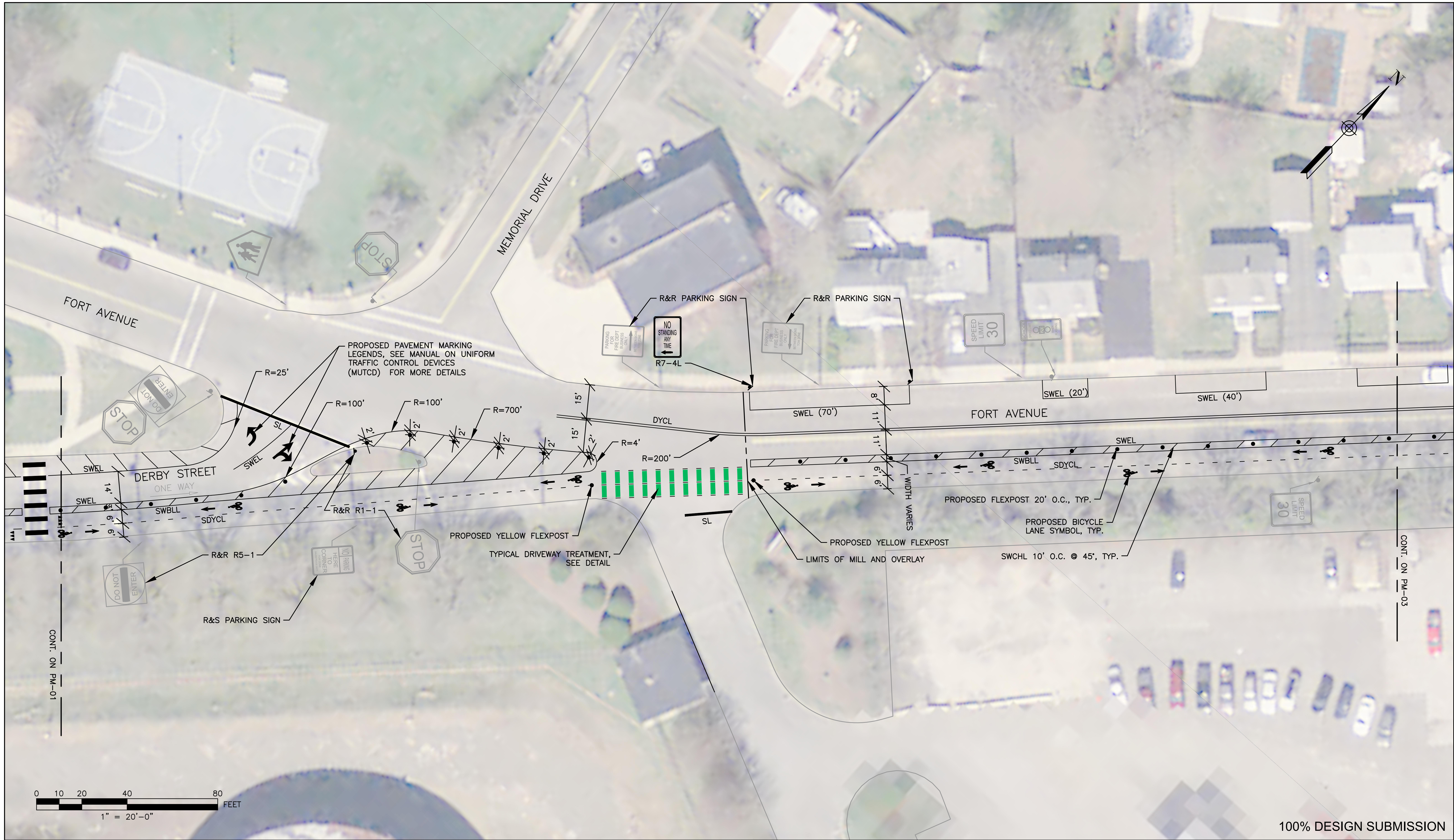
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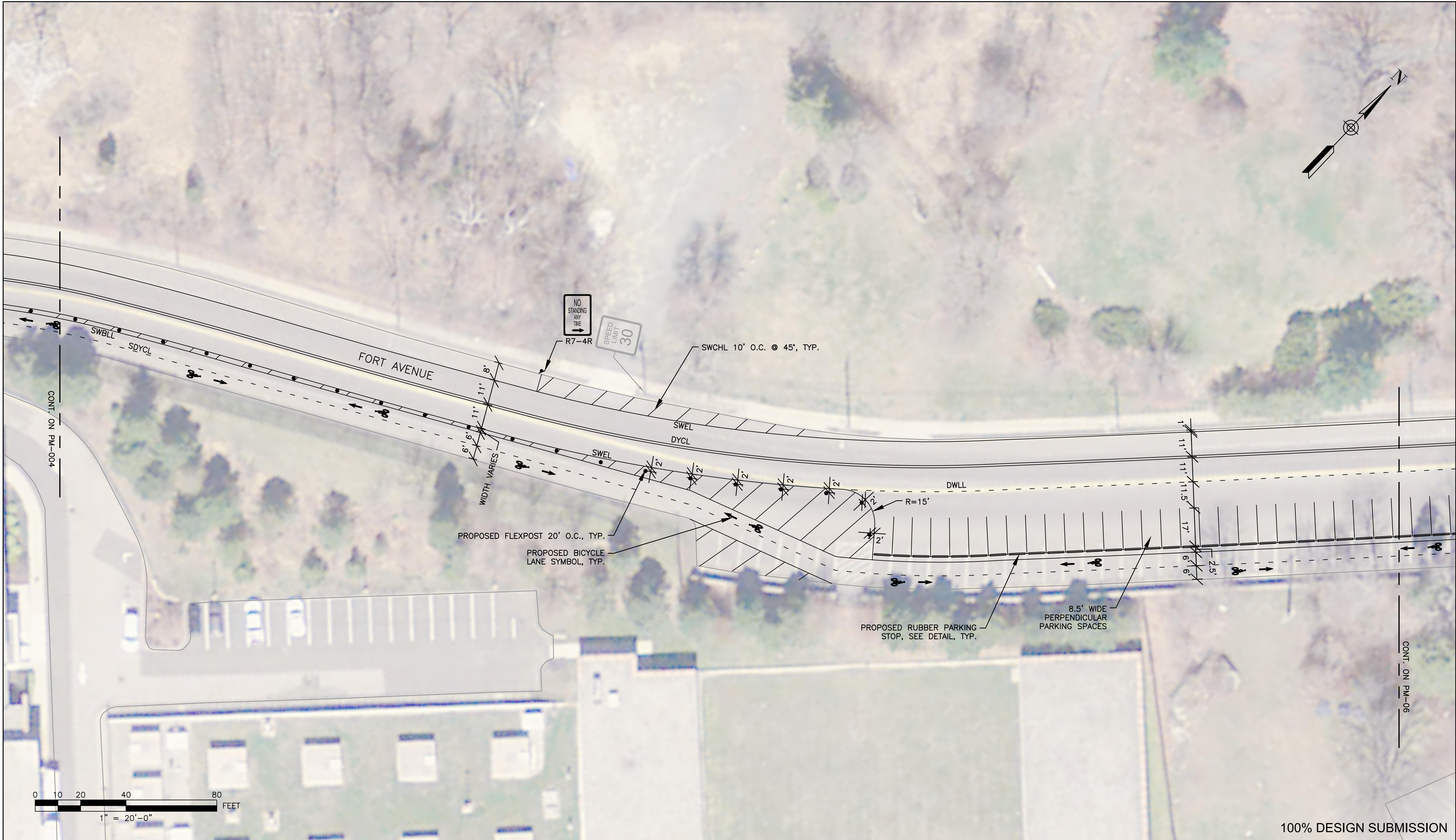
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